



May 8, 2020
Email Delivery

John Ruschke, P.E. Borough Engineer
Chatham Township
111 Ridgedale Avenue
Florham Park, NJ 07932

**RE: PUBLIC SERVICE ELECTRIC AND GAS COMPANY (PSE&G)
SEGMENT 1: ROSELAND TO BRANCHBURG
PSE&G TRANSMISSION RIGHT-OF-WAY (ROW)
CHATHAM TOWNSHIP, MORRIS COUNTY, NEW JERSEY
BOARD OF ADJUSTMENT MEMORANDUM - RESPONSE**

Dear Mr. Ruschke:

In response to comments received in Mott MacDonald letter dated May 8, 2020, we offer the following responses:

Comment #1: Approvals of applications submitted by PSE&G to all outside agencies including, but not limited to, the New Jersey Department of Environmental Protection (NJDEP) and Morris County Soil Conservation District (MCSCD) should be provided when obtained. Site plans should be revised to address all conditions of approvals from outside agencies as required. Anticipated NJDEP permits include, but are not limited to, freshwater wetlands, flood hazard area and right to discharge stormwater.

Response: Proofs of submission have been enclosed. Proof of approval will be forthcoming upon receipt.

Comment #2: The site plans include disturbances of riparian zones as described in Township Code Section 30-147. An exception to proposed riparian zone disturbances shall be granted by the Board upon proof, by virtue of submission of appropriate maps, drawings, reports and testimony, that the disturbance protects public health, safety or welfare; provides an environmental benefit; prevents extraordinary hardship on the property owner peculiar to the property; or prevents extraordinary hardship, provided the hardship was not created by the property owner, that would not permit a minimum economically viable use of the property based upon reasonable investment.

Response: PSE&G will be prepared to provide testimony that the disturbance protects the public health, safety, and welfare as PSE&G, as an electric provider, provides an inherently beneficial service to the residents.

Comment #3: Copy of Determination of No Hazard to Air Navigation Letters and Notice of Presumed Hazard Letters from FAA referenced in the letter from PSE&G should be provided.

Response: Enclosed please find copies of the letters issued by the FAA.

Comment #4: The site plans include new tower construction in several lots that are wholly or partially within an area of special flood hazard established in Township Code Section 30-119. As such, a Development Permit is required for site plan approval, per Township Code Section 30-124.

Response: Per the ordinance the locations of flood hazard areas are shown on the provided Site Plans. PSE&G will complete a Development Permit application immediately to comply.

Comment #5: Given that the proposed project includes several towers located in environmentally sensitive areas, an Avian Protection Plan should be provided as recommended by the U.S. Fish and Wildlife Service. The Avian Protection Plan should verify that the project is consistent with the Avian Power Line Interaction Committee's Suggested Practices for Avian Protection on Power Lines.

Response: Some of the transmission project located within Chatham Township is also located within the USFWS Great Swamp Refuge. PSE&G has been working with the Refuge since the initiation of Project planning. All mitigation components that are noted within the Assessment will be complied with.

Comment #6: Per Township Code Section 22-4.3.b, no public utility, except as authorized by the Vegetation Management Rules (N.J.A.C. 14:5-9.1), shall perform, or authorize the performance of, any maintenance work on any overhead wires, underground pipes or conduits, wherein any pruning, defacing, scarring or removal of trees will occur, without having previously applied for, and obtained, approval from the Township Land Use Office. The Applicant should clarify if any tree removal not authorized under N.J.A.C. 14:5-9.1 is proposed. If so, this tree removal should be documented/quantified on the plans, and replacement trees should be included on the plans in accordance with Township Code Section 22.

Response: No tree cutting, except as authorized by N.J.A.C. 14:5-9.1, is proposed. PSE&G will be prepared to provide testimony in this regard.

Comment #7: Lot widths at setback lines noted on Sheet 2 of the plans within the R-1 Zone should be noted as pre-existing, non-conforming.

Response: Our site plan will be updated accordingly.

Comment #8: The existing lattice tower heights shown on Sheet 2 of the plans are inconsistent with the heights indicated in the project description letter and should be checked/revised as necessary.

Response: PSE&G performed a survey of the existing towers and the heights have been updated on Page 2 of the site plan and in the project description letter.

Comment #9: The Applicant should give testimony for explanation as to why the seven proposed monopoles without existing adjacent monopoles are substantially higher than the other

fifteen proposed monopoles.

Response: Testimony will be provided at the hearing

Comment #10: All existing gas pipeline easements (Transcontinental Gas Co., Spectra Energy) located within the PSE&G right-of-way should be shown on the plans. Proposed disturbances within gas pipeline easements should be coordinated with the gas pipeline owners.

Response: All pipeline easements are located on the site plans. PSE&G has coordinated with the respective gas pipelines and has planned the proposed temporary disturbances with their support.

Comment #11: The plans should include notation that there is no change in existing stormwater runoff patterns either during or after the project. Temporary measures (such as berms/swales across access drives) are to be installed during construction in accordance with New Jersey SESC standards as necessary to maintain runoff patterns, and all proposed disturbance is to be restored in a manner such that no changes to existing topography result from the project.

Response: Enclosed please find the SESC drawings that have been submitted to the Morris County Soil Conservation District and are currently under review.

Comment #12: Approval from the Morris County engineering department should be obtained for proposed construction access drives from Shunpike Road and Southern Boulevard.

Response: PSE&G will apply to the Morris County Engineering Department. Morris County Planning Department exempted the Project in April 2020.

Comment #13: The project appears to include disturbance of steep slopes (15% slope or greater) in some areas. Steep slope disturbances should be delineated/quantified on the plans and identified as conforming/non-conforming to Township Code Section 30-96.24 for the following parcels:

- a. Block 102.08, Lot 37
- b. Block 105, Lot 5
- c. Block 102.11, Lot 3
- d. Block 18, Lot 94

For lots containing greater than 1,000 square feet of overall steep slope disturbance, all silt fence shall be "super" silt fence as defined in standards for SESC in New Jersey as promulgated by the NJDA State Soil Conservation Committee for consistency with Township Code Section 30-96.20.c.4(t).

Response: No grading is proposed. PSE&G will update the Site Plan to highlight areas of steep slope greater than 15% per township ordinance. Per the ordinance this PSE&G transmission project falls under e.2 "...New disturbance necessary to protect public health, safety or welfare, such as necessary linear development with no feasible alternative;..."

Comment #14: Swamp Road should be identified on Sheet No. 8 of the plans with gravel limits delineated.

Response: The site plan will be updated accordingly.

Comment #15: A construction detail for the proposed air bridge on Block 14901, Lots 1-3 should be provided on the plans.

Response: A detail of the proposed air bridge is provided on Sheet 122 of the Soil Erosion and Sediment Control Plans.

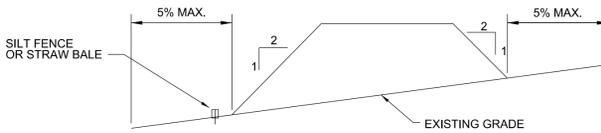
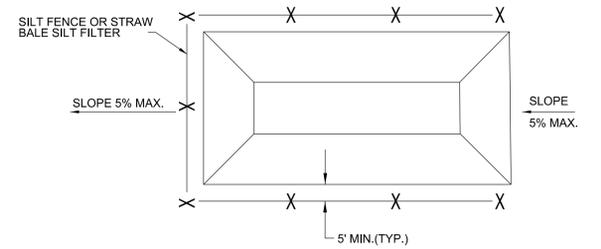
If you have any questions or require additional information, please feel free to contact me at 732-685-7274 or by email brandi.bartolomeo@pseg.com.

Sincerely,



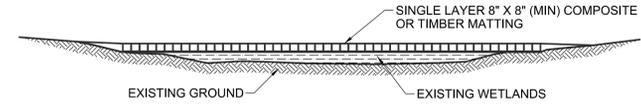
Brandi Bartolomeo
Manager, Major Permits and Field Services
PSE&G –Projects and Construction

cc: Everton Scott, Regional Public Affairs Manager, PSE&G



- NOTES:
1. PLACE STOCKPILES AT LOCATIONS SHOWN ON PLAN.
 2. STOCKPILE TO BE SEEDED WITH A TEMPORARY SEED MIXTURE IF PERIOD OF EXPOSURE IS EXPECTED TO EXCEED 20 DAYS.
 3. SIDE SLOPES SHALL NOT EXCEED 1' VERTICAL TO 2' HORIZONTAL.
 4. MAXIMUM STOCKPILE HEIGHT NOT TO EXCEED 15'.
 5. SEE MANAGEMENT OF HIGH ACID PRODUCING SOILS NOTES BELOW FOR METHOD OF STOCKPILING AND PROTECTING TEMPORARILY STOCKPILED HIGH ACID PRODUCING SOILS.

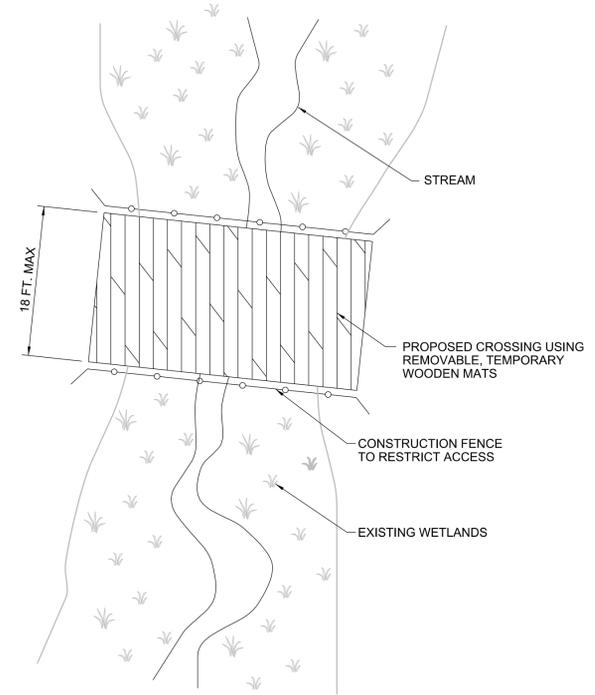
TEMPORARY TOPSOIL AND/OR EXCAVATED SOIL STOCKPILE
N.T.S.



WETLAND MATTING
N.T.S.

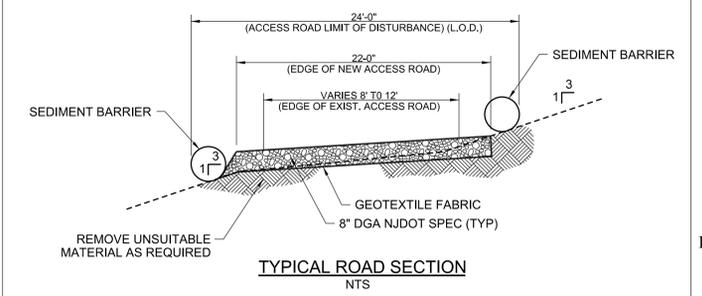
- NOTES:
1. CONSTRUCT ROADS AND PADS BY STACKING ADDITIONAL MATS AS NEEDED TO REDUCE THE SLOPE OF THE PAD SURFACE TO ALLOW SAFE OPERATION OF CONSTRUCTION EQUIPMENT.
 2. INSPECT MAT ROADS ON A DAILY BASIS. REPAIR DAMAGED MATS BEFORE ANY SUBSEQUENT USE.
 3. REMOVE THE TEMPORARY ROAD AND/OR PAD AS SOON AS IT IS NO LONGER NEEDED.
 4. ALL MATS SHALL BE CLEAN AND FREE OF ALL DEBRIS PRIOR TO ARRIVAL ON SITE.
 5. IF FLOWING WATER IS PRESENT, MATS SHALL BE STACKED WITH GAPS BETWEEN LOWER LAYERS TO ALLOW CONTINUOUS FLOW OF WATER THROUGH MATTED SECTION OF ACCESS.
 6. PERIODICALLY REMOVE SEDIMENT THAT IS FORCED UP ONTO MATS.
 7. WETLAND MATTING TYPE SHALL BE COMPOSITE OR WOOD.

TYPICAL TEMPORARY CONSTRUCTION MAT ROAD & CONSTRUCTION PAD DETAIL
N.T.S.

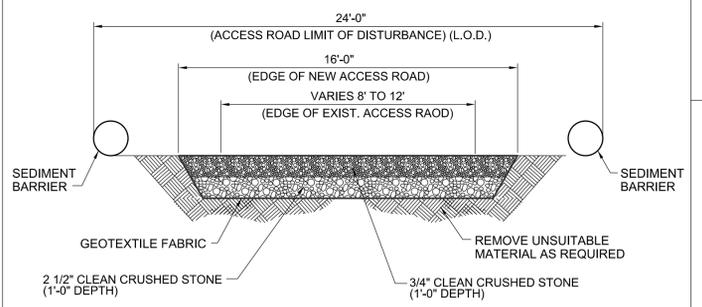


STANDARD CONSTRUCTION DETAIL TEMPORARY STREAM/WETLANDS CROSSING
N.T.S.

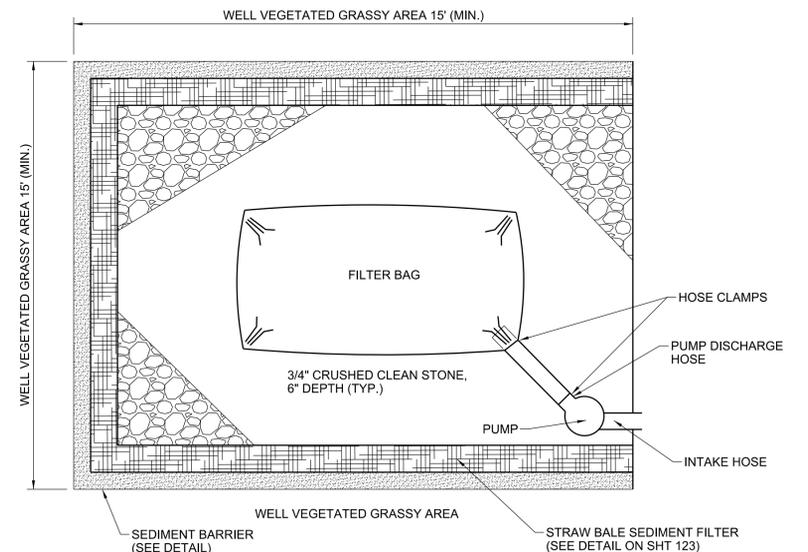
- TYPICAL WETLAND/STREAM CROSSING NOTES:
1. WETLANDS AND STREAMS HAVE BEEN DELINEATED IN ACCORDANCE WITH THE FEDERAL MANUAL FOR IDENTIFYING AND DELINEATING JURISDICTIONAL WETLANDS DATED 1989.
 2. THE SITE OF A WETLAND AND/OR STREAM CROSSING SHALL BE STABILIZED BY ANY APPROPRIATE MEANS, INCLUDING BUT NOT LIMITED TO USING REMOVABLE, TEMPORARY MATS, PADS OR OTHER SIMILAR DEVICES TO ENSURE MINIMIZATION OF IMPACT ON THE WETLAND ECOLOGY.
 3. FOLLOWING REMOVAL OF MATS, OVER SEEDING OF DISTURBED WETLANDS IS TO BE COMPLETED IN ACCORDANCE WITH SEEDING AND MULCHING SPECIFICATIONS.



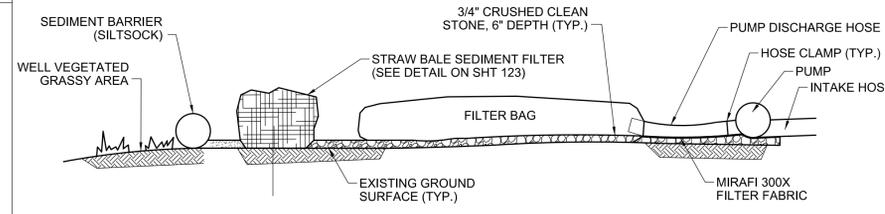
TYPICAL ROAD SECTION
NTS



PERMANENT ROAD SECTION
NTS



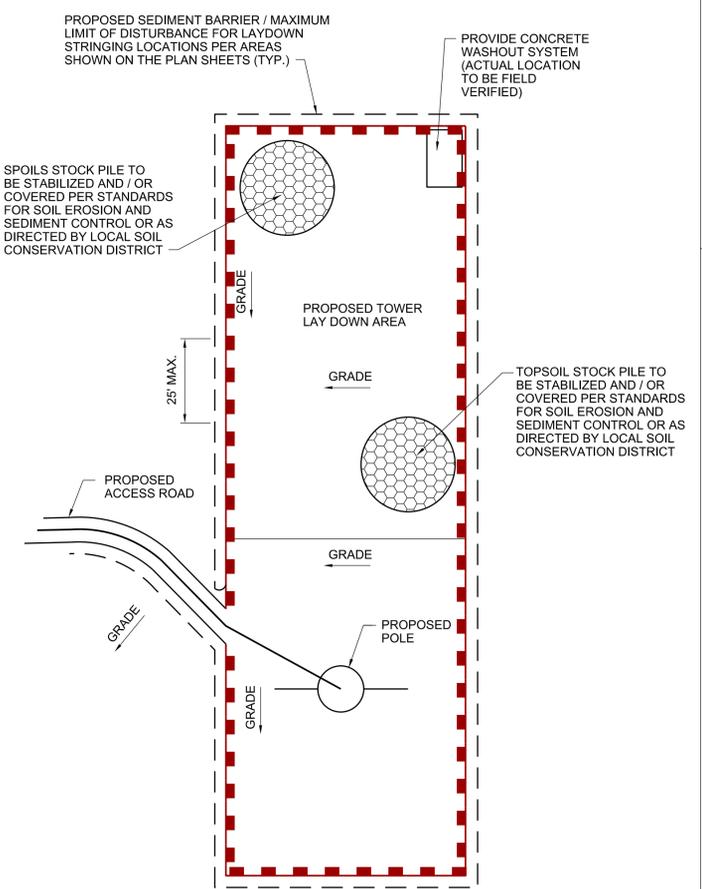
PLAN VIEW



ELEVATION VIEW

- NOTES:
1. SEDIMENT CONTROL BAG MUST BE LOCATED AWAY FROM RECEIVING WATER AND / OR CONSTRUCTION ACTIVITIES.
 2. BAG MUST BE DISPOSED OFF ACCORDING TO MANUFACTURER'S INSTRUCTIONS. BAGS MAY NOT BE REUSED.
 3. DISCHARGE FROM SEDIMENT CONTROL BAG / DEWATERING FILTER SHALL BE MAINTAINED AS SHEET FLOW IN AN UPLAND AREA WITHIN THE LOD.

SEDIMENT CONTROL BAG / DEWATERING FILTER INSTALLATION
NTS



- NOTES:
1. LAYOUT SHOWN IS A SUGGESTED LAYOUT. CONDITIONS IN THE FIELD WILL VARY AND DICTATE PAD LAYOUT.
 2. IF GRADING IS REQUIRED, CONTRACTOR TO SUBMIT PROPOSED GRADING PLAN TO COMPANY FOR APPROVAL.
 3. STOCKPILES ARE NOT TO BE LOCATED WITHIN 50 FT OF A FLOOD PLAIN, SLOPE, ROADWAY OR DRAINAGE FACILITY.

LAY DOWN AREA SEDIMENT CONTROL/LIMIT OF DISTURBANCE
NTS



| NO. | DESCRIPTION | DATE | APPROVED |
|-----|--|------------|----------|
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG |
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG |

| REVISIONS | | APPROVED/TITLE | |
|-----------|----------|----------------|------------|
| SCALE | NTS | CHECKED | J. FINK |
| DATE | 11/07/18 | DRAWN BY | J. GROSS |
| LOCATION: | RPV | DISCIPLINE | CIVIL |
| SIZE | ARCH D | DWG. NO. | TO-737087 |
| | | REV | 1 |
| | | SHEET | 120 OF 127 |

VEGETATIVE STANDARDS FOR SOIL STABILIZATION

| SOIL PREPARATION | APPLICATION RATES | |
|---|------------------------|------------------------|
| | PERMANENT COVER * | TEMPORARY COVER |
| FERTILIZER (10 - 20 - 10 OR EQUAL) | 11 LB'S./ 1000 SQ.FT. | 11 LB'S./ 1000 SQ.FT. |
| SLOW RELEASE NITROGEN (38 - 0 - 0 OR EQUIV.) | 7 LB'S./ 1000 SQ.FT. | NOT REQUIRED |
| LIMESTONE (50% CALCIUM PLUS MAGNESIUM OXIDES) | 180 LB'S./ 1000 SQ.FT. | 135 LB'S./ 1000 SQ.FT. |

* SEED OR SOD

TEMPORARY VEGETATIVE STABILIZATION GRASSES, SEEDING RATES, DATES, AND DEPTH.
(REFERENCE STANDARDS FOR SOIL EROSION & SEDIMENT CONTROL IN NEW JERSEY, SECTION 7)

| SEED SELECTIONS | SEEDING RATE (POUNDS) | | OPTIMUM SEEDING DATE BASED ON PLANT HARDINESS ZONE | | | OPTIMUM SEED DEPTH (INCHES) |
|---------------------------------|-----------------------|------------------|--|-----------------------|------------------------|-----------------------------|
| | PER ACRE | PER 1000 SQ. FT. | ZONE 5b, 6a | ZONE 6b | ZONE 7a, b | |
| COOL SEASON GRASS | | | | | | |
| 1. PERENNIAL RYEGRASS | 100 | 1.0 | 3/15-6/1 8/1-9/15 | 3/1-5/15 8/15-10/1 | 2/15-5/1 8/15-10/15 | 0.5 |
| 2. SPRING OATS | 86 | 2.0 | 3/15-6/1 8/1-9/15 | 3/1-5/15 8/15-10/1 | 2/15-5/1 8/15-10/15 | 1.0 |
| 3. WINTER BARLEY | 96 | 2.2 | 8/1-9/15 | 8/15-10/1 | 8/15-10/15 | 1.0 |
| 4. ANNUAL RYEGRASS | 100 | 1.0 | 8/15-6/1 8/1-9/15 | 3/1-5/15 8/15-10/1 | 2/15-5/1 8/15-10/15 | 0.5 |
| 5. WINTER CEREAL RYE | 112 | 2.8 | 8/1-11/1 | 8/1-11/15 | 8/1-12/15 | 1.0 |
| WARM SEASON GRASS | | | | | | |
| 6. PEARL MILLET | 20 | 0.5 | 6/1-8/1 | 5/15-8/15 | 5/1-9/1 | 1.0 |
| 7. MILLET (GERMAN OR HUNGARIAN) | 30 | 0.7 | 6/1-8/1 | 5/15-8/15 | 5/1-9/1 | 1.0 |

PERMANENT VEGETATIVE MIXTURES, PLANTING RATES, AND PLANTING DATES.
(REFERENCE STANDARDS FOR SOIL EROSION & SEDIMENT CONTROL IN NEW JERSEY, SECTION 4)

| SEED SELECTIONS | SEEDING RATE (POUNDS) | | OPTIMUM SEEDING DATE BASED ON PLANT HARDINESS ZONE | | |
|---|-----------------------|------------------|--|------------|------------|
| | PER ACRE | PER 1000 SQ. FT. | ZONE 5b, 6a | ZONE 6b | ZONE 7a, b |
| COOL SEASON GRASS (MIXTURE #7) | | | | | |
| 1. STRONG CREEPING RED FESCUE | 130 | 3 | 3/15-5/31 | 3/1-4/30 | 2/1-4/30 |
| KENTUCKY BLUEGRASS | 50 | 1 | 8/1-10/31 | 8/15-10/15 | 8/15-11/30 |
| PERENNIAL RYEGRASS OR REDTOP | 20 | 0.5 | | | |
| PLUS WHITE CLOVER | 10 | 0.25 | | | |
| PLUS WHITE CLOVER | 5 | 0.10 | | | |
| WARM SEASON GRASS (MIXTURE #1) | | | | | |
| 1. SWITCHGRASS AND/OR COASTAL PANIC GRASS PLUS OR FLATPEA | 15 | 0.35 | 3/15-5/31 | 3/1-4/30 | 2/1-4/30 |
| | 15 | 0.35 | | | |
| | 20 | 0.45 | | | |

MULCH
(REQUIRED ON ALL SEEDING)

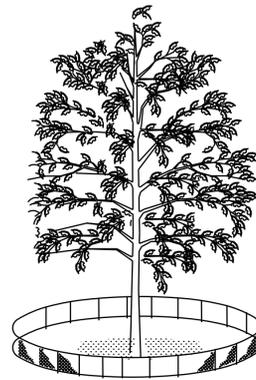
MULCH MATERIALS: UNROTTED SMALL - GRAIN STRAW, HAY OR SALT HAY.
ANCHORING METHODS: CRIMPER, LIQUID MULCH BINDERS, NETTING TIEDOWN.

APPLICATION RATES: POST - SEEDING: 70 - 90 LB'S./ 1000 SQ.FT.
*MULCH ONLY: 90 - 115 LB'S./ 1000 SQ.FT.

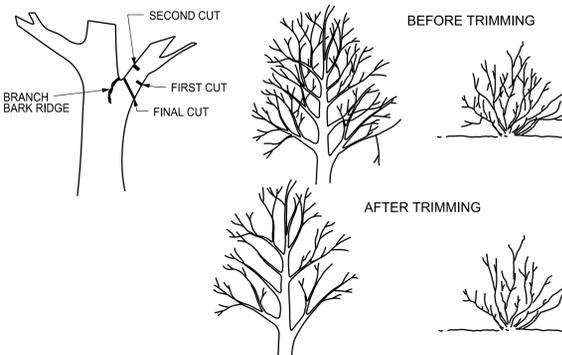
*WHERE SEASON / OTHER CONDITIONS UNSUITABLE FOR PLANTING OR WHERE STABILIZATION IS NEEDED FOR SHORT PERIOD UNTIL MORE SUITABLE PROTECTION CAN BE APPLIED.

NOTES:

- FENCING SHALL BE INSTALLED AT THE DRIP LINE (MIN. 5 FEET FROM TREE TRUNK).
- BOARDS SHALL NOT BE NAILED TO TREES DURING CONSTRUCTION.
- FEEDER ROOTS SHALL NOT BE CUT IN AREA INSIDE THE DRIP LINE.
- THE CONTRACTOR SHALL NOT ENCRUACH ONTO AREAS DELINEATED AS TREE PROTECTION. THE STORAGE OF EQUIPMENT OR MATERIALS IN THIS AREA IS STRICTLY FORBIDDEN. THE SILT FENCE ERECTED AT THE TREE PROTECTION LINE SHALL BE MAINTAINED AT ALL TIMES DURING THE CONSTRUCTION PROCESS AND SHALL NOT BE CROSSED.
- REFER TO PLAN SHEETS FOR LOCATION OF WOODS AND TREES TO REMAIN. ALL TREES TO REMAIN WHICH ARE ADJACENT TO CONSTRUCTION AREAS SHALL BE PROTECTED IN ACCORDANCE WITH THIS DETAIL.
- NO CLEARING, EARTHMOVING, OR CONSTRUCTION SHALL COMMENCE PRIOR TO INSTALLATION OF TREE PROTECTION FENCES AROUND ALL EXISTING TREES.
- A 48 INCH HIGH WOODEN OR PLASTIC FENCE WITH POSTS ON 8 FOOT CENTER TO CENTER (MAXIMUM) SHALL BE INSTALLED AROUND EACH EXISTING TREE PRIOR TO START OF CONSTRUCTION (TYPICAL FOR ALL TREES).



TREE PROTECTION DETAIL
N.T.S.

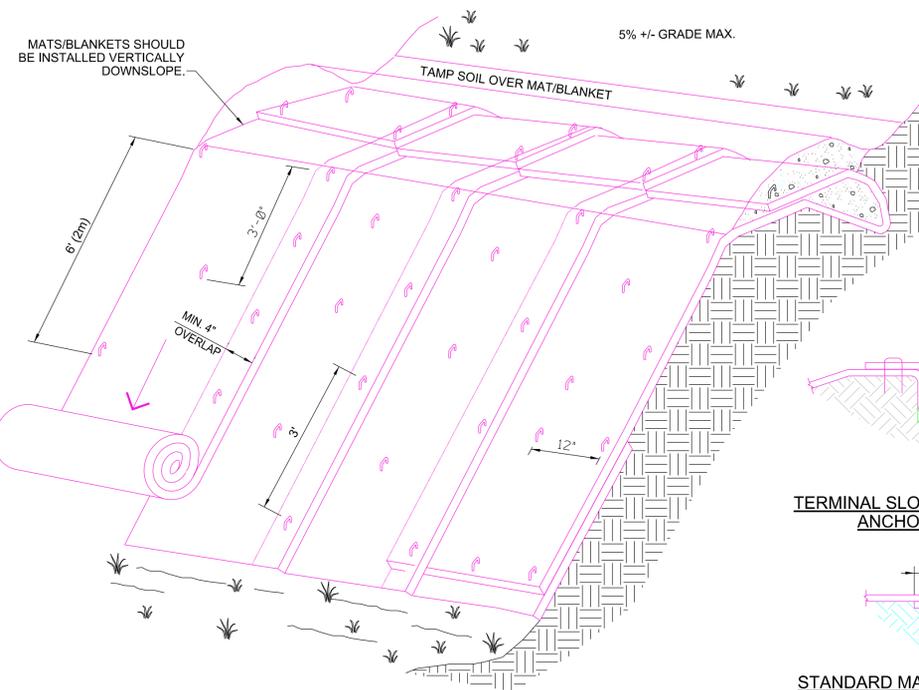


PRUNING DETAILS

DAMAGED BRANCHES SHALL BE TRIMMED OFF BELOW THE POINT OF INJURY. THE CENTRAL TRUNK OR "LEADER" SHOULD BE LEFT INTACT. BROKEN ROOTS SHALL BE CUT OFF ABOVE THE BREAK AND BRUISED ENDS CUT OFF CLEANLY.

MANAGEMENT OF HIGH ACID PRODUCING SOILS:

- HIGH ACID PRODUCING SOILS (HAPS) ARE SOILS WITH A PH OF 4 OR LESS, UPON EXCAVATION, OR CONTAINING IRON SULFIDE.
- TOPSOIL STRIPPED FROM THE SITE SHALL BE STORED SEPARATELY FROM TEMPORARILY STOCKPILED HIGH ACID-PRODUCING SOILS.
- STOCKPILES OF HIGH ACID PRODUCING SOIL SHOULD BE LOCATED ON LEVEL LAND TO MINIMIZE ITS MOVEMENT, ESPECIALLY WHEN THIS MATERIAL HAS A HIGH CLAY CONTENT.
- CONTRACTOR SHALL CONTACT THE APPROPRIATE SOIL CONSERVATION DISTRICT (SCD) TO DETERMINE THE HISTORICAL PRESENCE OF HAPS IN THE VICINITY OF THE SITE PRIOR TO COMMENCEMENT OF WORK.
- HAPS ARE COMMONLY BLACK, DARK BROWN, GRAY OR GREENISH WITH SILVERY PYRITE OR MARCASITE NUGGETS OR FLAKED.
- CONTRACTOR SHALL LIMIT EXCAVATION AREA AND EXPOSURE TIME WHEN HAPS ARE ENCOUNTERED.
- TEMPORARILY STOCKPILED HIGH ACID-PRODUCING SOIL MATERIAL TO BE STORED MORE THAN 48 HOURS SHOULD BE COVERED WITH PROPERLY ANCHORED, HEAVY GRADE SHEETS OF POLYETHYLENE WHERE POSSIBLE. IF NOT POSSIBLE, STOCKPILES SHALL BE COVERED WITH A MINIMUM OF 3 TO 6 INCHES OF WOOD CHIPS TO MINIMIZE EROSION OF THE STOCKPILE. SILT FENCE SHALL BE INSTALLED AT THE TOE OF THE SLOPE TO CONTAIN MOVEMENT OF THE STOCKPILED MATERIAL. TOPSOIL SHALL NOT BE APPLIED TO THE STOCKPILES TO PREVENT TOPSOIL CONTAMINATION WITH HIGH ACID-PRODUCING SOIL.
- HIGH ACID-PRODUCING SOILS WITH A PH OF 4.0 OR LESS OR CONTAINING IRON SULFIDE (INCLUDING BORROW FROM CUTS OR DREDGED SEDIMENT) SHALL BE ULTIMATELY PLACED OR BURIED WITH LIMESTONE APPLIED AT THE RATE OF 10 TONS PER ACRE (OR 450 POUNDS PER 1,000 SQUARE FEET OF SURFACE AREA) AND COVERED WITH A MINIMUM OF 12 INCHES OF SETTLED SOIL WITH A PH OF 5.0 OR MORE EXCEPT AS FOLLOWS:
 - AREAS WHERE TREES OR SHRUBS ARE TO BE PLANTED SHALL BE COVERED WITH A MINIMUM OF 24 INCHES OF SOIL WITH A PH OF 5 OR MORE.
 - DISPOSAL AREAS SHALL NOT BE LOCATED WITHIN 24 INCHES OF ANY SURFACE OF A SLOPE OR BANK, SUCH AS BERMS, STREAM BANKS, DITCHES AND OTHERS TO PREVENT POTENTIAL LATERAL LEACHING DAMAGES.
- EQUIPMENT USED FOR MOVEMENT OF HAPS SHOULD BE CLEANED AT THE END OF EACH DAY TO PREVENT SPREADING OF HAPS TO OTHER PARTS OF THE SITE, INTO STREAMS OR STORMWATER CONVEYANCES AND TO PROTECT MACHINERY FROM ACCELERATING RUSTING.
- NON VEGETATIVE EROSION CONTROL PRACTICES (STONE TRACKING PADS, STRATEGICALLY PLACED LIMESTONE CHECK DAM, SILT FENCE, WOOD CHIPS) SHALL BE INSTALLED TO LIMIT THE MOVEMENT OF HAPS FROM, AROUND, OR OFF THE SITE.



EROSION CONTROL FOR STEEP SLOPES
N.T.S.

* EROSION CONTROL BLANKETS SHALL BE PLACED ON ALL FILL AND CUT SLOPES STEEPER THAN 3:1. FOR ALL SLOPES EROSION CONTROL BLANKETS SHALL BE CURLEX NETFREE BLANKETS OR APPROVED EQUAL. FOLLOW MANUFACTURES NOTES FOR INSTALLATION

TERMINAL SLOPE AND CHANNEL ANCHOR TRENCH

STANDARD MATTING OVERLAP

DUST CONTROL METHODS

DUST CONTROL MATERIALS

| MATERIAL | WATER DILUTION | TYPE OF NOZZLE | APPLY GALLONS/ACRE |
|--|----------------|----------------|--|
| LATEX EMULSION | 12.5:1 | FINE SPRAY | 235 |
| RESIN IN WATER | 4:1 | FINE SPRAY | 300 |
| POLYACRYLAMIDE (PAM) - SPRAY ON POLYACRYLAMIDE (PAM) - DRY SPREAD | | | APPLY ACCORDING TO MANUFACTURER'S INSTRUCTIONS. MAY ALSO BE USED AS AN ADDITIVE TO SEDIMENT BASINS TO FLOCCULATE AND PRECIPITATE SUSPENDED COLLOIDS. |
| ACIDULATED SOY BEAN SOAP STICK | NONE | COARSE SPRAY | 1200 |

MULCHES - SEE MULCH THIS SHEET.

VEGETATIVE COVER - SEE VEGETATIVE STANDARDS THIS SHEET.

SPRAY-ON ADHESIVES - ON MINERAL SOILS (NOT EFFECTIVE ON MUCK SOILS). KEEP TRAFFIC OFF THESE AREAS.

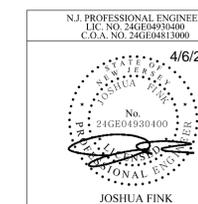
TILLAGE - TO ROUGHEN SURFACE AND BRING CLODS TO THE SURFACE. THIS IS A TEMPORARY EMERGENCY MEASURE WHICH SHOULD BE USE BEFORE SOIL BLOWING STARTS. BEGIN PLOWING ON WINDWARD SIDE OF SITE. CHISEL-TYPE PLOW SPACED ABOUT 12 INCHES APART, AND SPRING-TOOTHED HARROWS ARE EXAMPLES OF EQUIPMENT WHICH MAY PRODUCE THE DESIRED EFFECT.

SPRINKLING - SITE IS SPRINKLED UNTIL THE SURFACE IS WET.

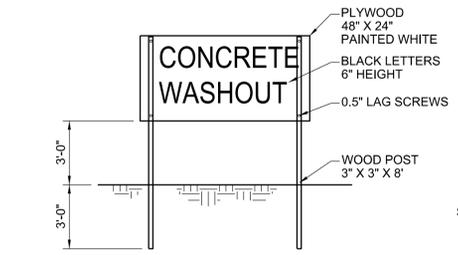
BARRIERS - SOLID BOARD FENCES, SNOW FENCES, BURLAP FENCES, CRATE WALLS, BALES OF HAY, AND SIMILAR MATERIAL CAN BE USED TO CONTROL AIR CURRENTS AND SOIL BLOWING.

CALCIUM CHLORIDE - SHALL BE IN THE FORM OF LOOSE, DRY GRANULES OR FLAKES FINE ENOUGH TO FEED THROUGH COMMONLY USED SPREADERS AT THE RATE THAT WILL KEEP SURFACE MOIST BUT NOT CAUSE POLLUTION OR PLANT DAMAGE. IF USED ON STEEPER SLOPES, THEN USE OTHER PRACTICES TO PREVENT WASHING INTO STREAMS, OR ACCUMULATION AROUND PLANTS.

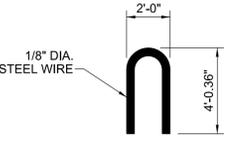
STONE - COVER SURFACE WITH CRUSHED STONE OR GRAVEL.



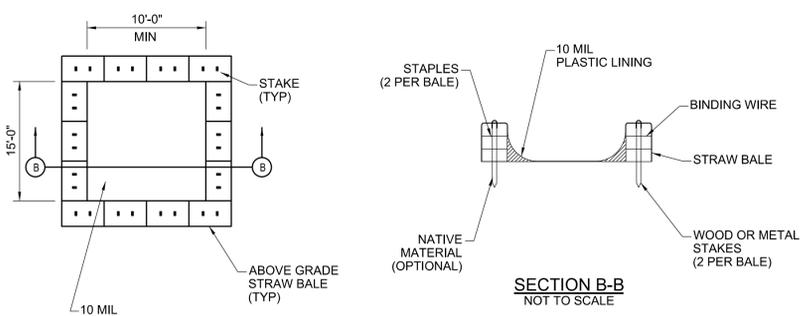
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG | |
|----------------|--|--|---------|------------------|
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG | |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG | |
| ZONE | REV | DESCRIPTION | DATE | APPROVED |
| REVISIONS | | | | |
| PSEG | | SUBJECT: RPV GENERAL NOTES AND DETAILS | | |
| SCALE: NTS | CHECKED: J. FINK | APPROVED/TITLE: J. WANG / MANAGER | | |
| DATE: 11/07/18 | DRAWN BY: J. GROSS | DISCIPLINE: CIVIL | | |
| LOCATION: RPV | SIZE ARCH D | DWG NO. TO-737087 | REV 1 | SHEET 121 OF 127 |



CONCRETE WASHOUT SIGN OR EQUIVALENT
NTS

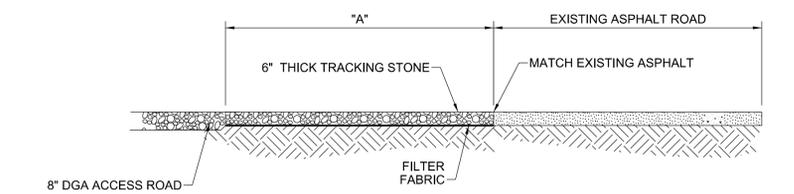
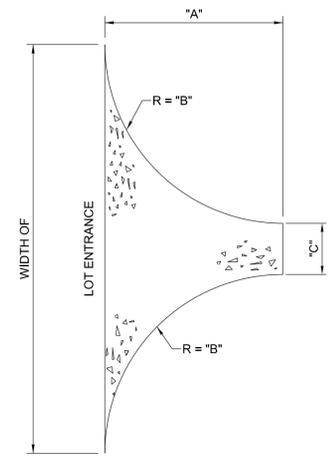


STAPLE DETAIL
NTS



CONCRETE WASHOUT DETAIL
NTS

- NOTES:
 1. ACTUAL LAYOUT DETERMINED IN FIELD.
 2. PREFABRICATED MOBILE WASHOUT STATIONS ARE PREFERRED AND MAY BE USED AT CONTRACTOR'S DISCRETION.
 3. THE CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 30 FT. OF THE TEMPORARY CONCRETE WASHOUT FACILITY.

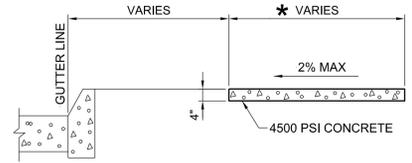
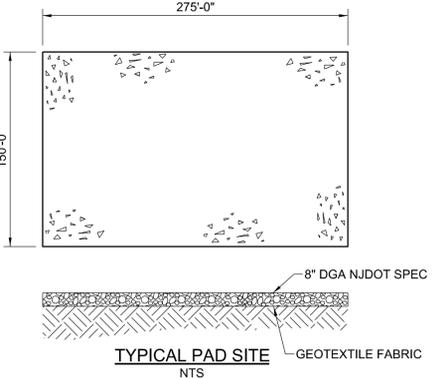
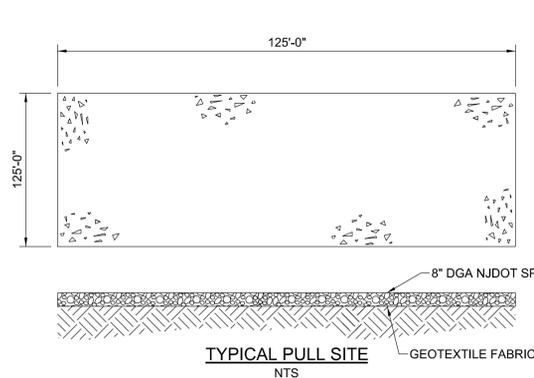


CONSTRUCTION ENTRANCE DETAIL

| CONSTRUCTION PAD DIMENSIONS | | | | CLEAN CRUSHED STONE CHART |
|-------------------------------------|-----------------|------------|------------|---------------------------|
| COUNTY | "A" DIMENSION | "B" DIMEN. | "C" DIMEN. | SIZE |
| HUDSON - ESSEX - PASSAIC COUNTY SCD | MINIMUM 50 FEET | 50' | 30' | 1" TO 2 1/2" |
| MORRIS COUNTY SCD | MINIMUM 50 FEET | 50' | 30' | 1" TO 2" |
| SOMERSET - UNION SCD | MINIMUM 50 FEET | 50' | 30' | 1 1/2" TO 2" |

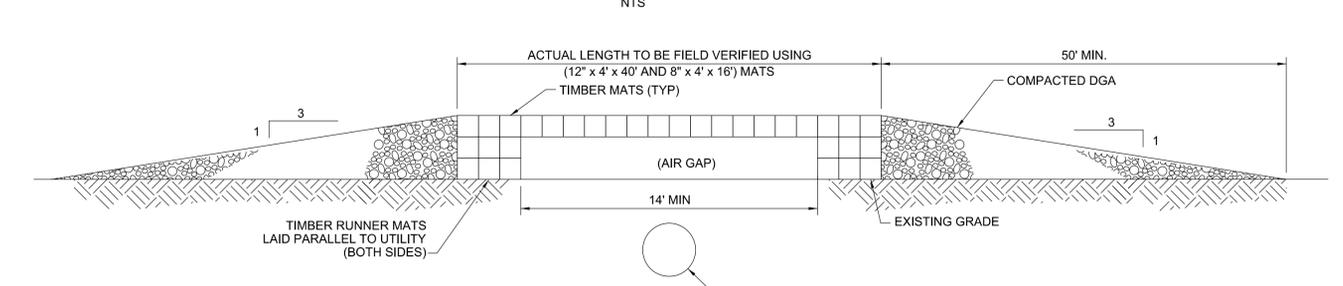
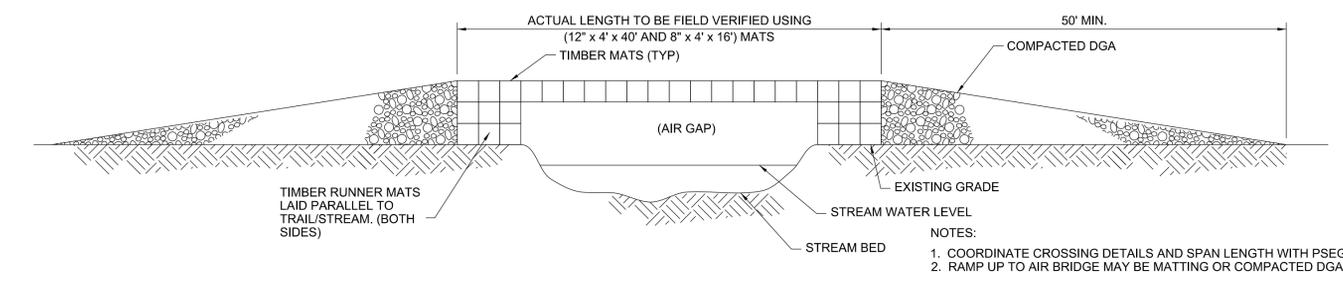
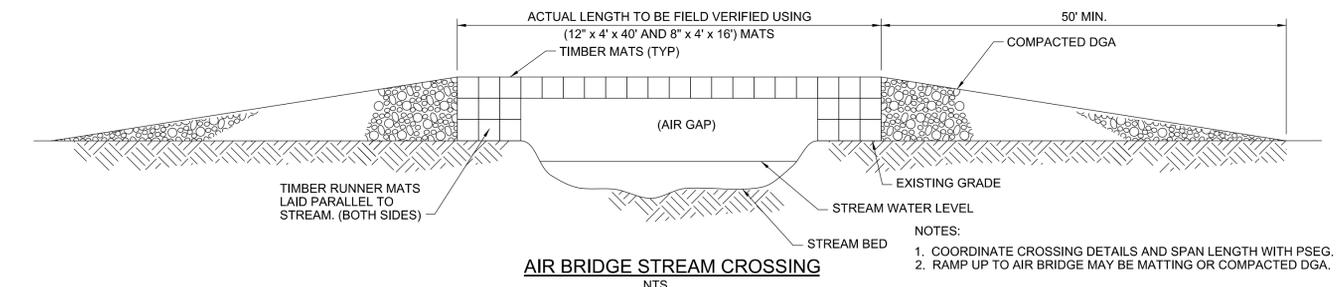
STABILIZED CONSTRUCTION ENTRANCE

CONSTRUCTION ENTRANCE WILL BE CLEAN CRUSHED ANGULAR STONE, 6" THICK, WILL BE PER MINIMUM SIZES SPECIFIED IN CONSTRUCTION PAD DIMENSIONS TABLE ABOVE (UNLESS OTHERWISE DEPICTED ON PLANS) AND SHALL BE UNDERLAIN WITH A SUITABLE SYNTHETIC SEDIMENT FILTER FABRIC AND MAINTAINED. REFER TO CHART FOR STONE SIZES

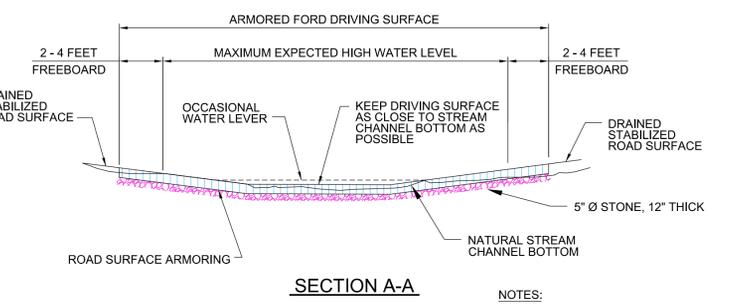
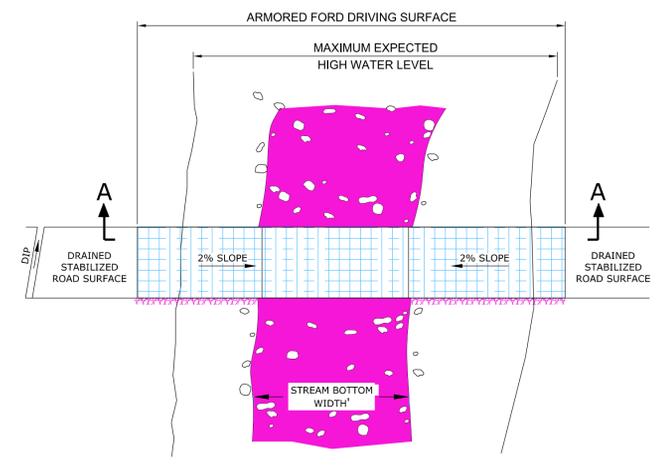


CONCRETE SIDEWALK DETAIL
NTS

- * MATCH EXISTING SIDEWALK WIDTH IF APPLICABLE. SIDEWALK REPLACEMENT TO COMPLY WITH MUNICIPAL COUNTY OR STATE REQUIREMENTS AS NEEDED
- PREFORMED BITUMINOUS EXPANSION JOINTS, 1/2" THICK SHALL BE INSTALLED EVERY 20 FEET.
- CONTRACTION JOINTS SHALL BE INSTALLED EVERY 4 FEET.



- NOTES:
 1. NOTIFY UTILITY PRIOR TO CONSTRUCTION.
 2. COORDINATE CROSSING DETAILS AND SPAN LENGTH WITH UTILITY.
 3. RAMP UP TO AIR BRIDGE MAY BE MATTING OR COMPACTED DGA.



TYPICAL FORD DETAIL
SCALE: N.T.S.

- NOTES:
 1. STONE BASE FOR ROAD SURFACE ARMOR SHALL BE VIBRATED IN PLACE TO A MINIMUM OF 12" PROVIDING A DRIVABLE SURFACE FOR MAINTENANCE VEHICLES.

| | | | |
|---|--|------------|---------|
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG |
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG |

| ZONE | REV | DESCRIPTION | DATE | APPROVED |
|------|-----|-------------|------|----------|
| | | | | |

4/6/2020

N.J. PROFESSIONAL ENGINEER
 L.I.C. NO. 24GE04930400
 C.O.A. NO. 24GE04813000

JOSHUA FINK

PSEG
 ASSET MANAGEMENT
 ELECTRIC TRANSMISSION ENGINEERING

RPV
 GENERAL NOTES AND DETAILS

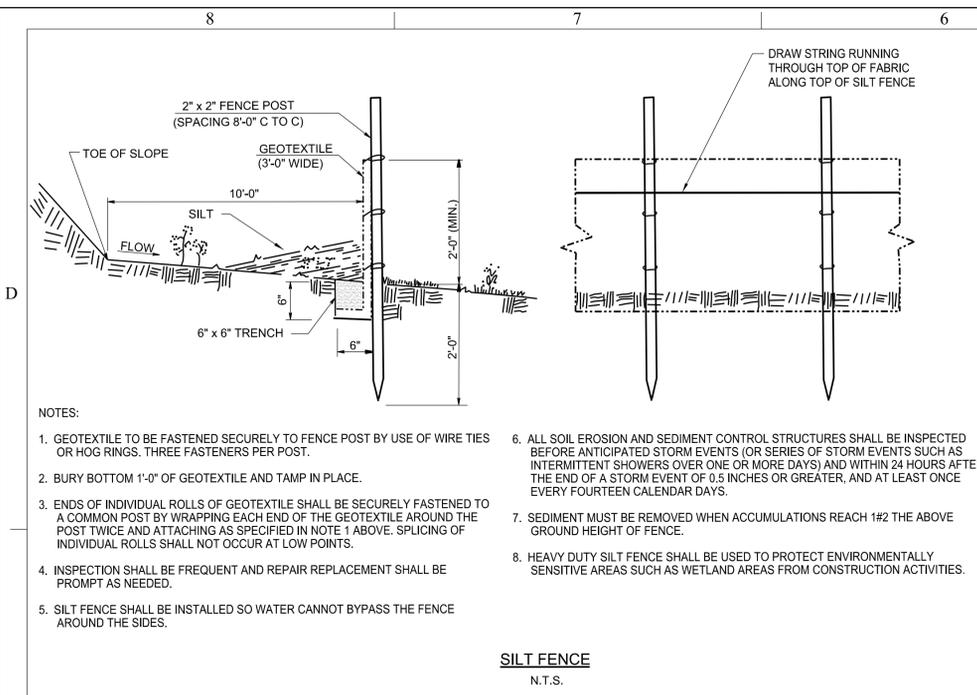
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|-------|----------|----------|----------|----------------|-------------------|
| SCALE | NTS | CHECKED | J. FINK | APPROVED/TITLE | J. WANG / MANAGER |
| DATE | 11/07/18 | DRAWN BY | J. GROSS | DISCIPLINE | CIVIL |

LOCATION: RPV

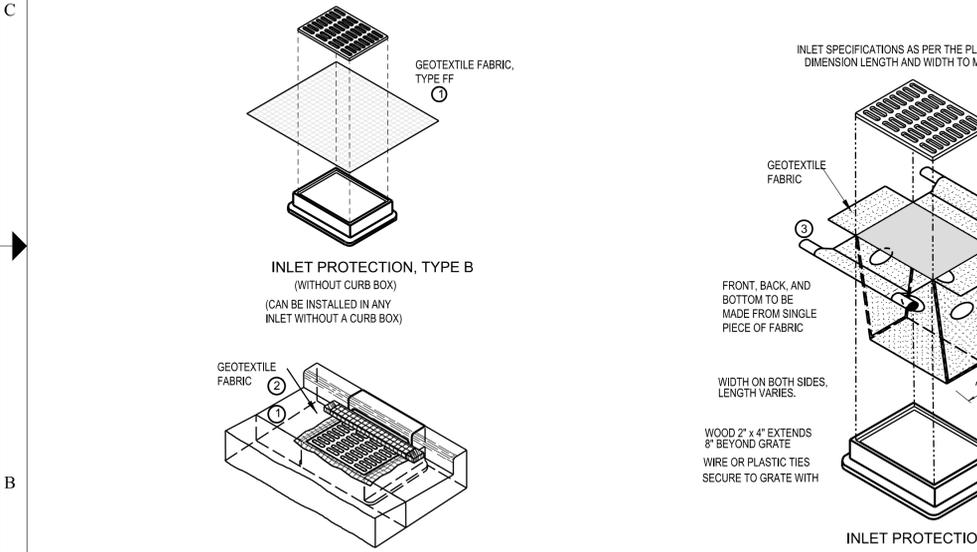
SIZE ARCH D

DWG. NO. TO-737087

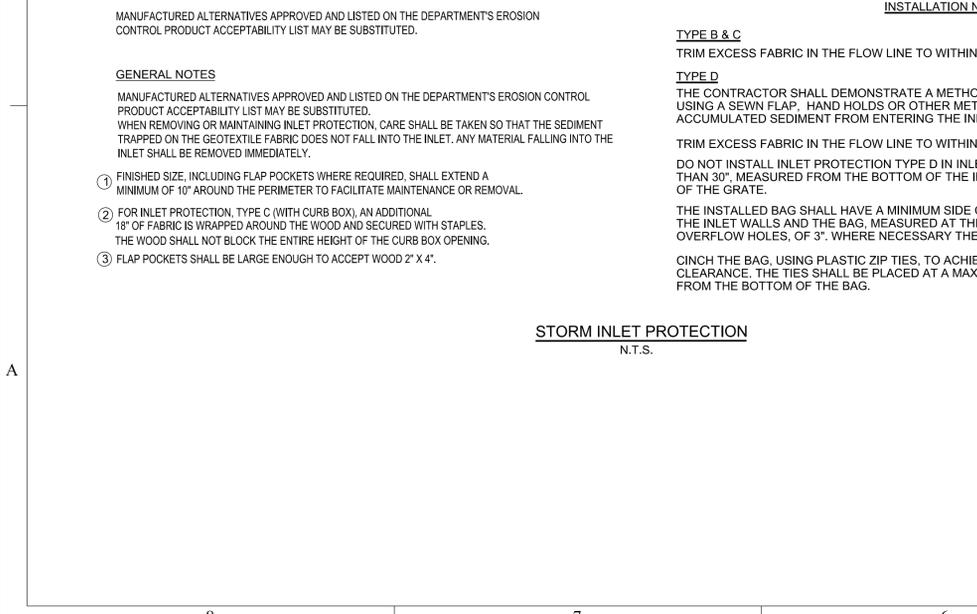
REV 1 SHEET 122 OF 127



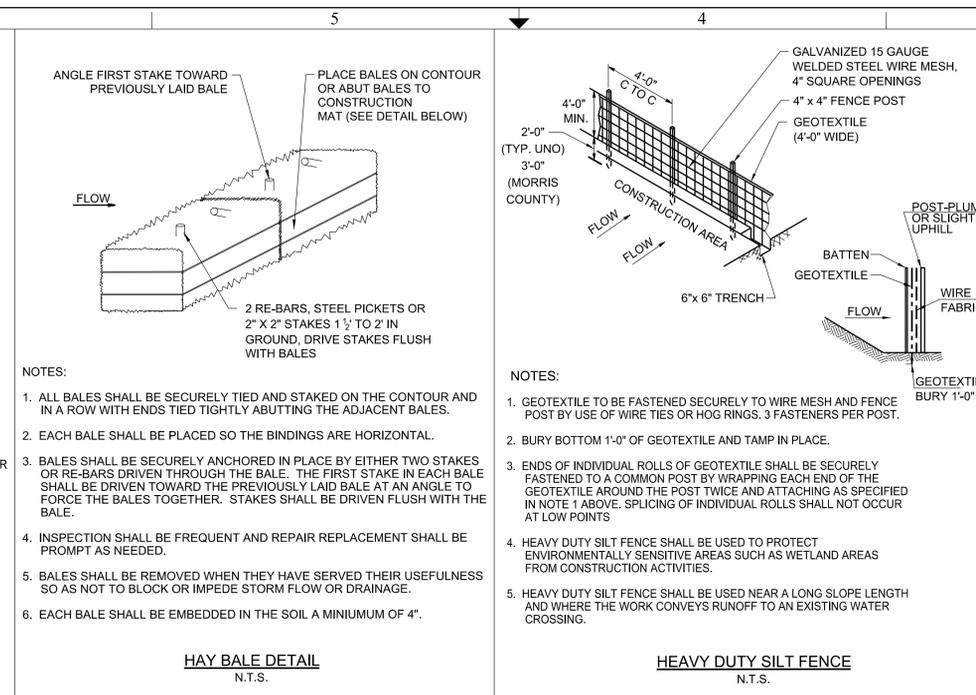
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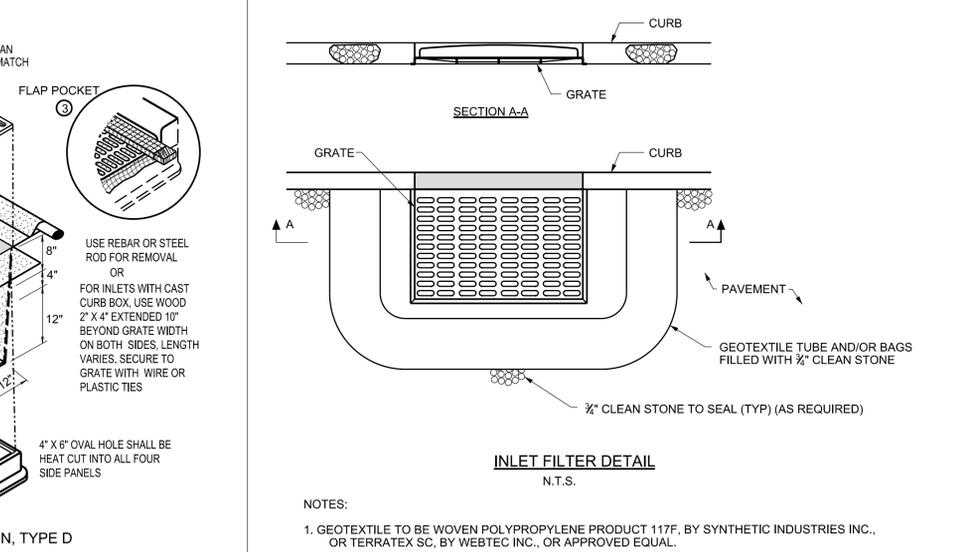
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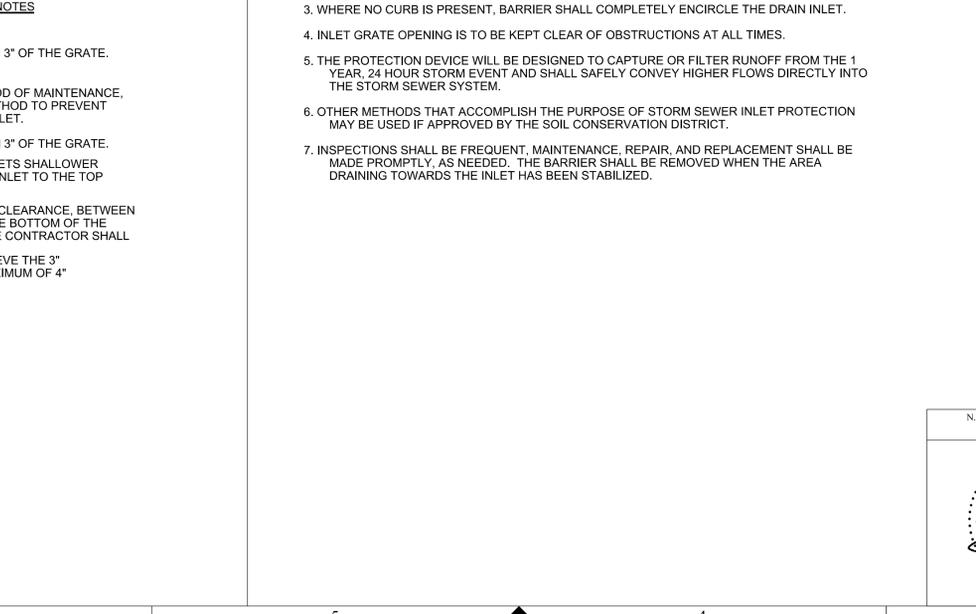
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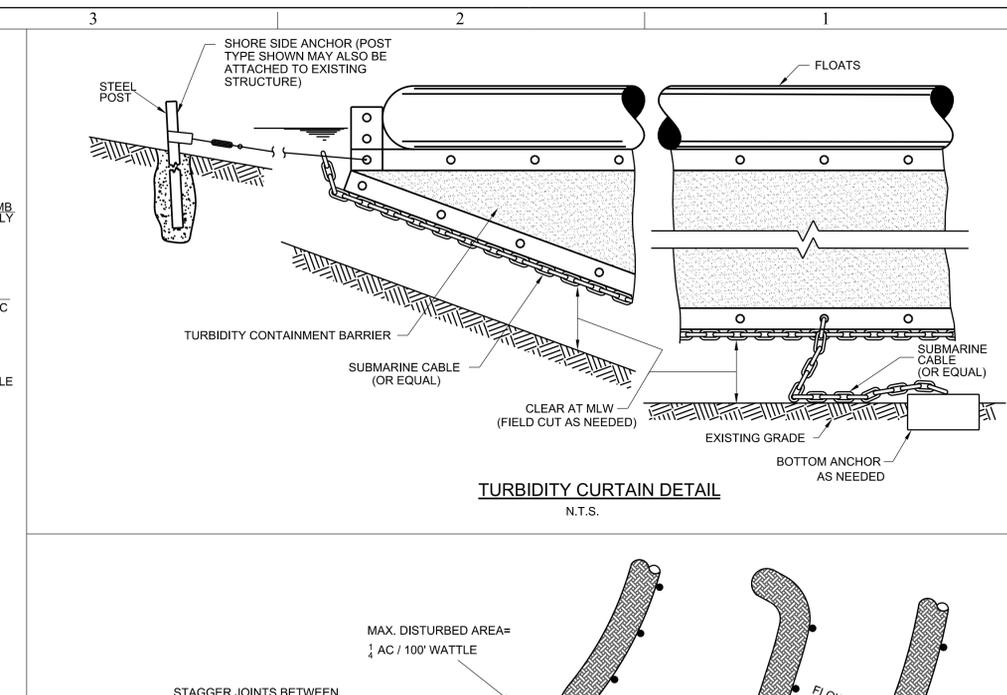
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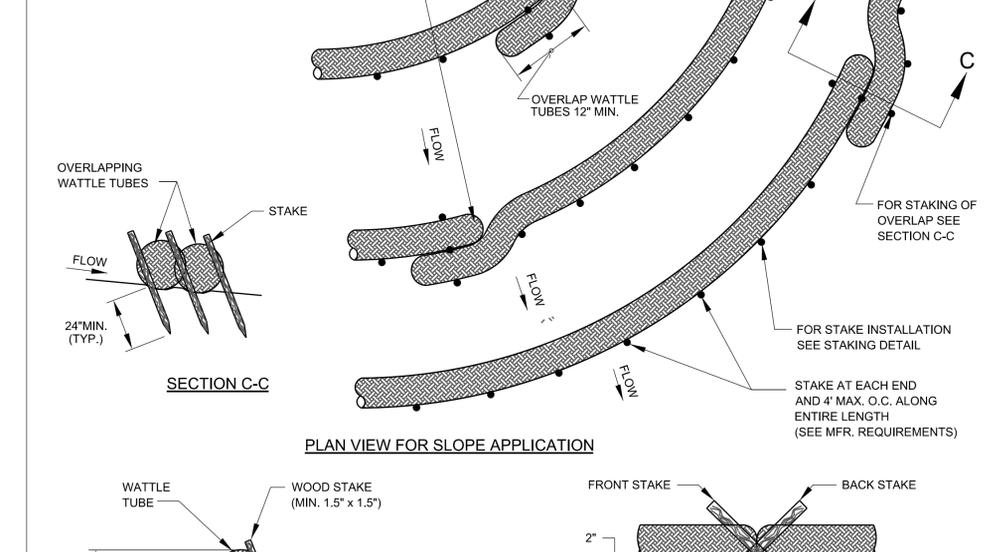
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STORM INLET PROTECTION
N.T.S.



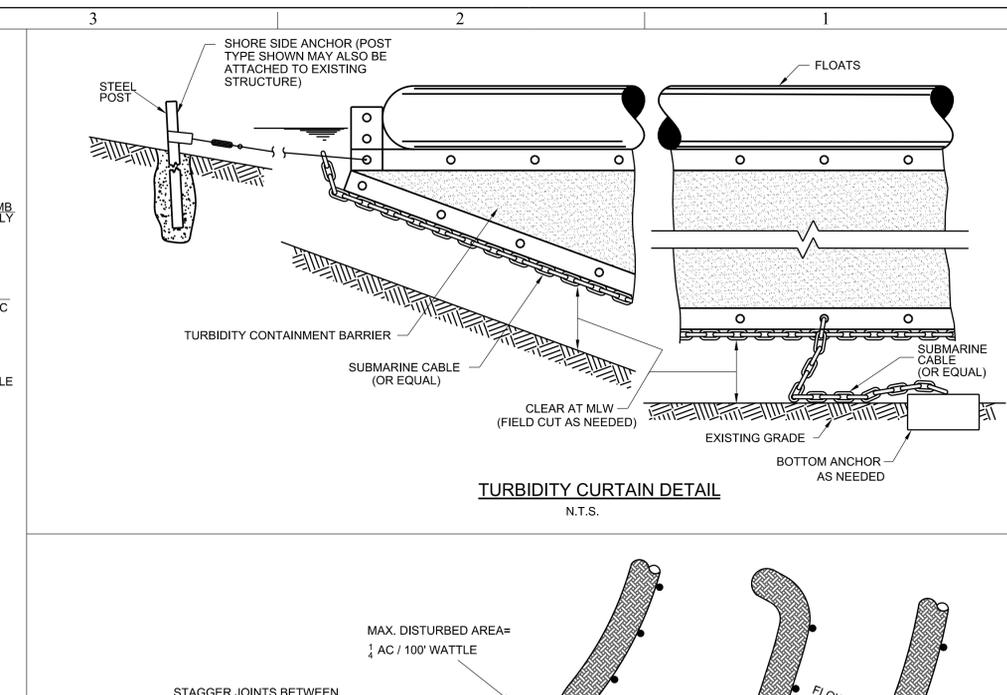
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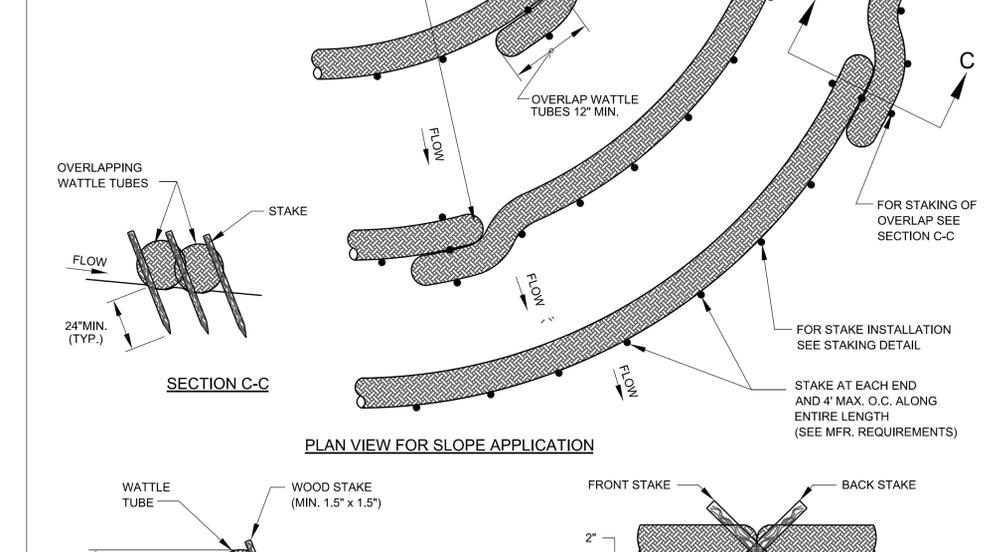
INSTALLATION NOTES



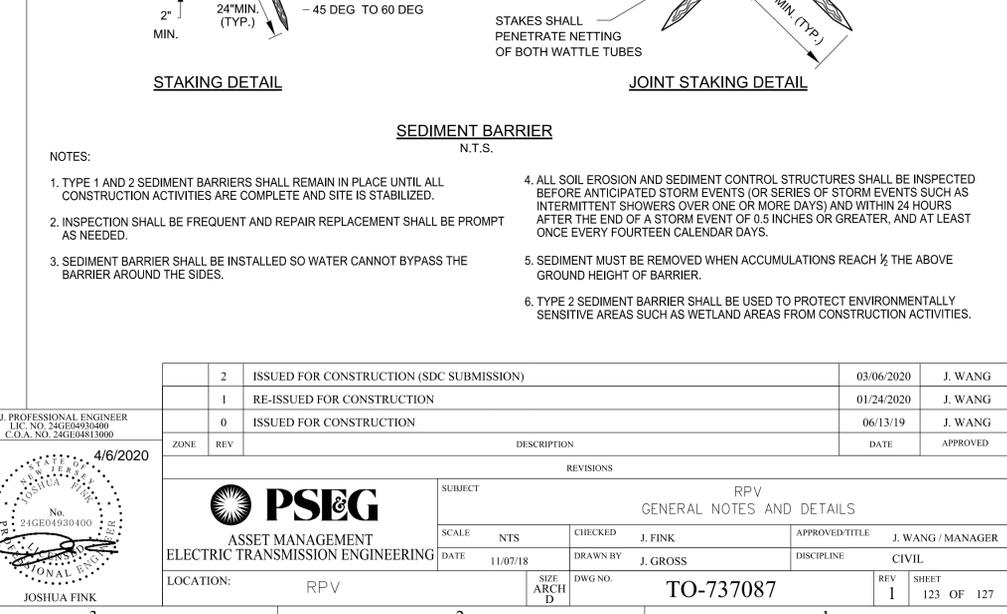
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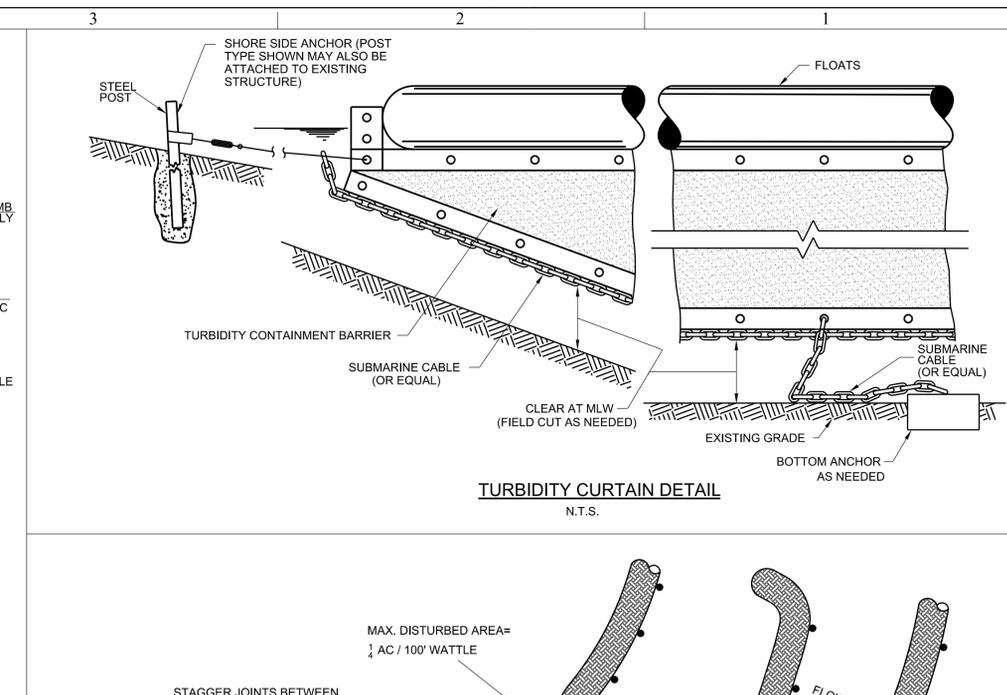
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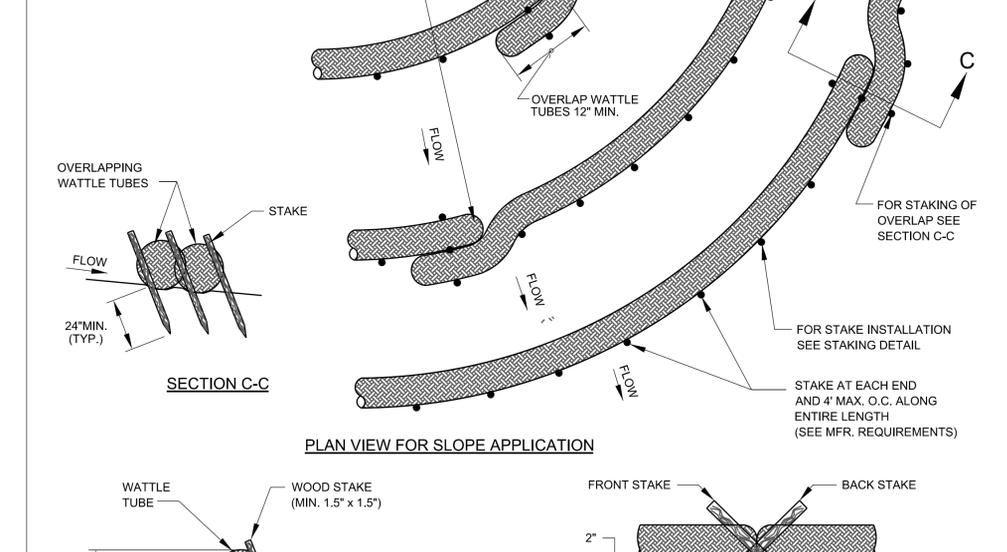
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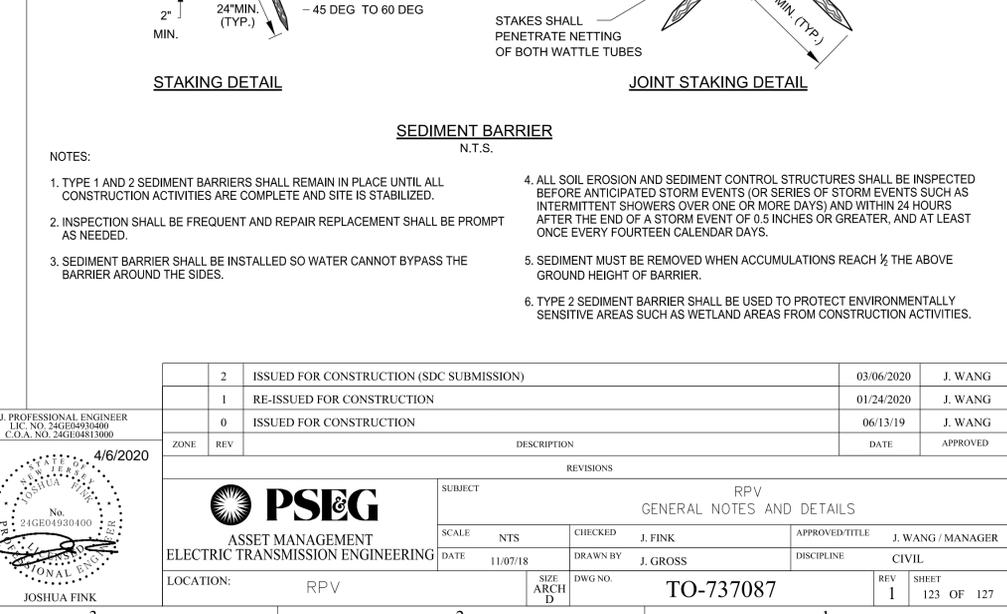
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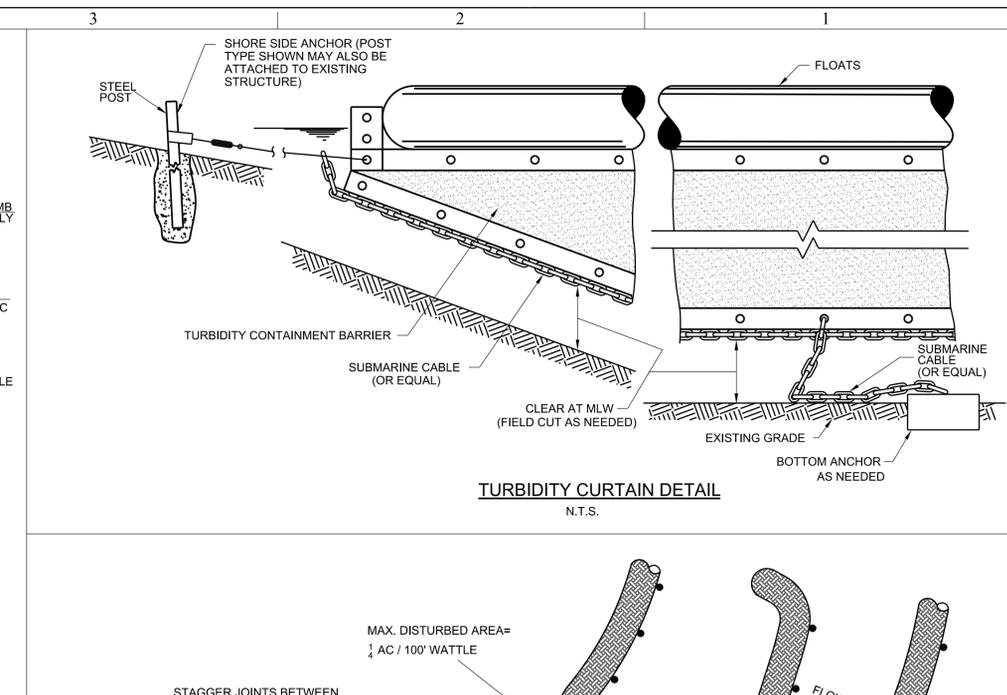
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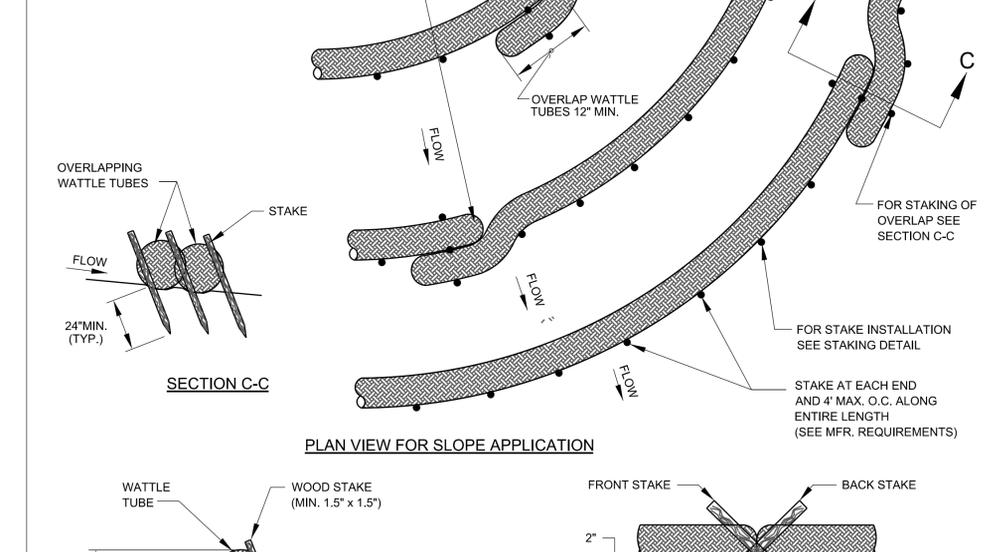
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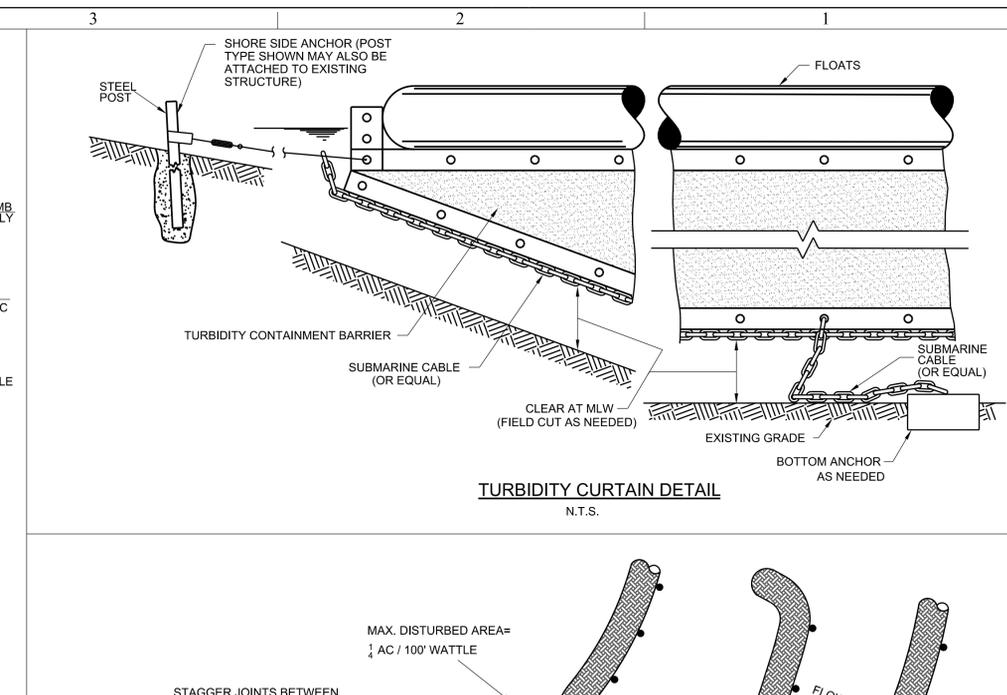
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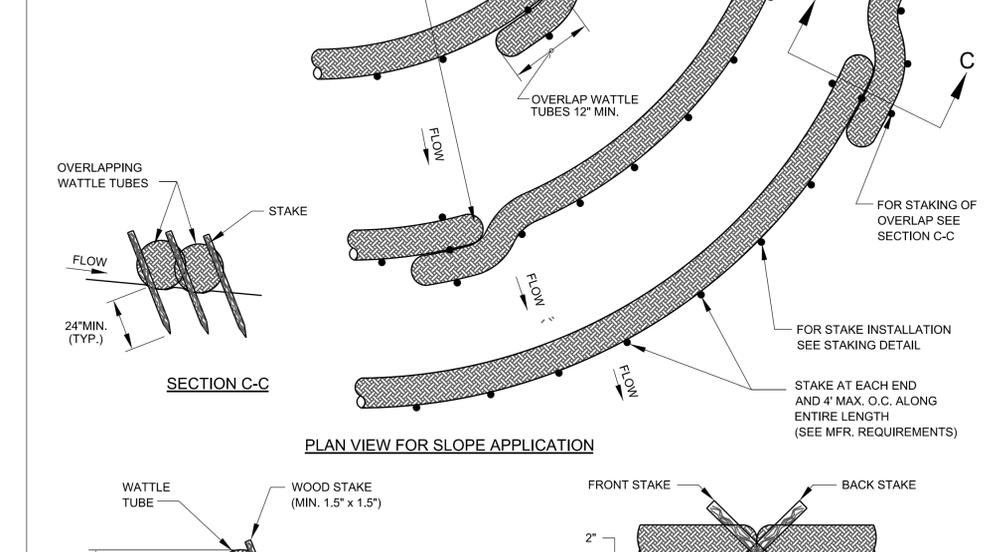
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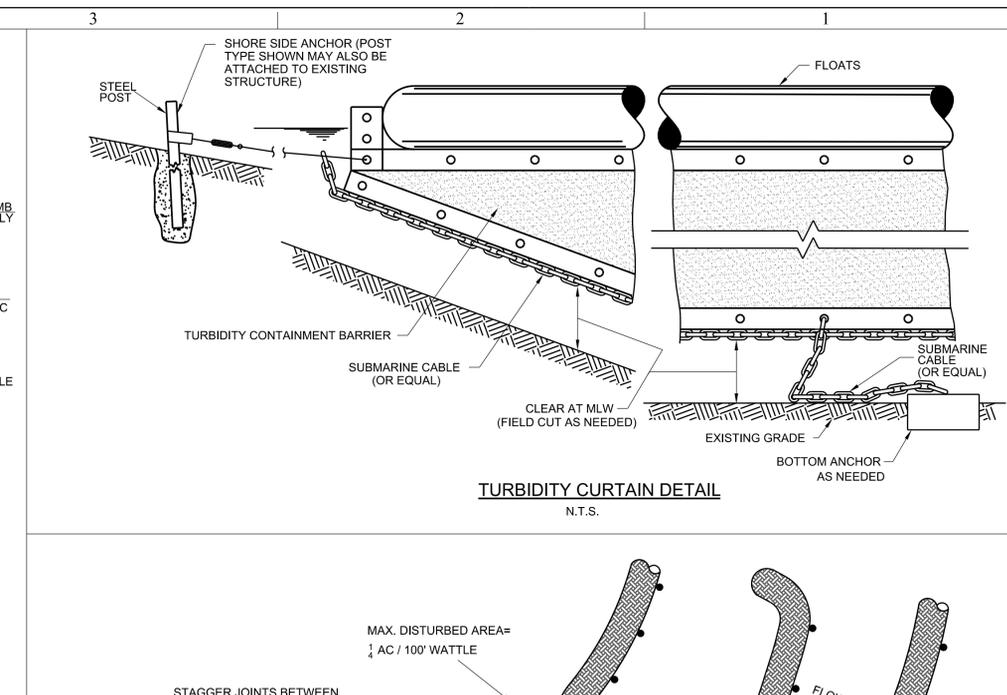
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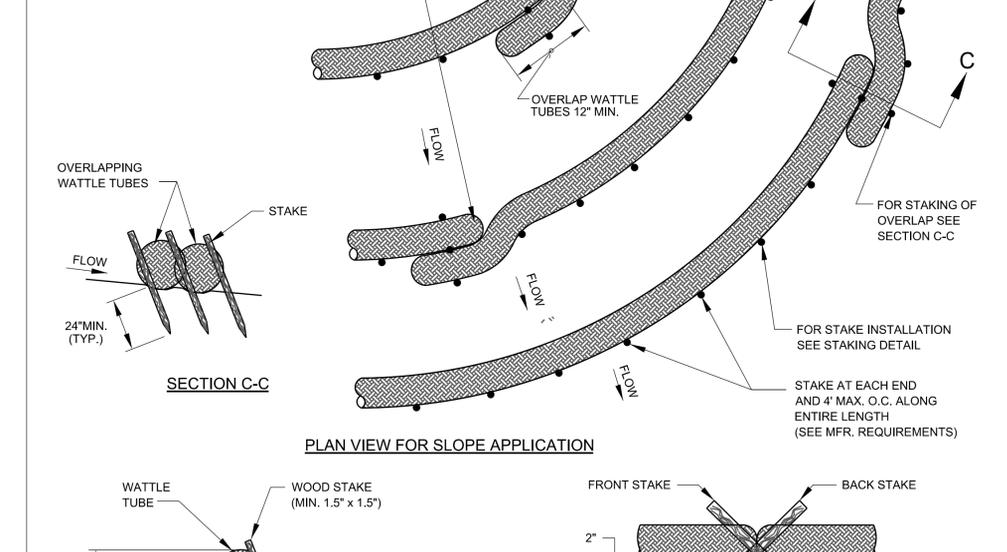
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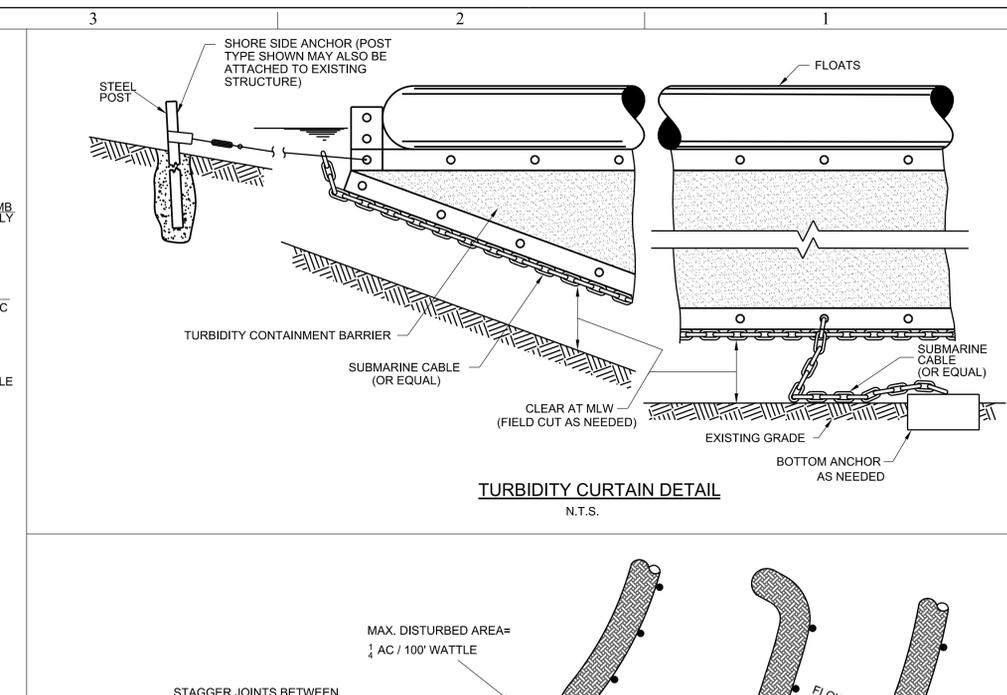
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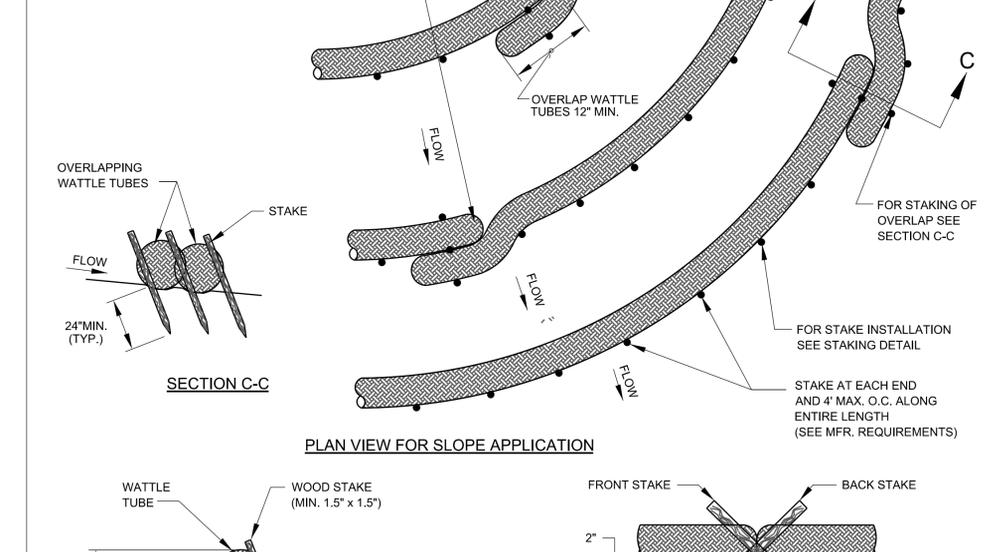
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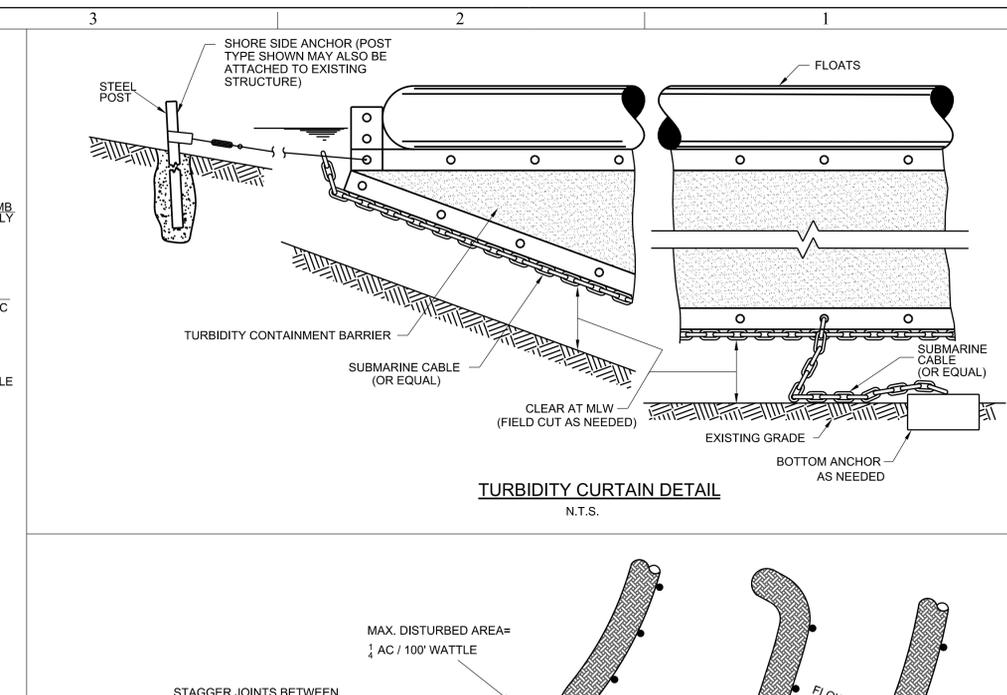
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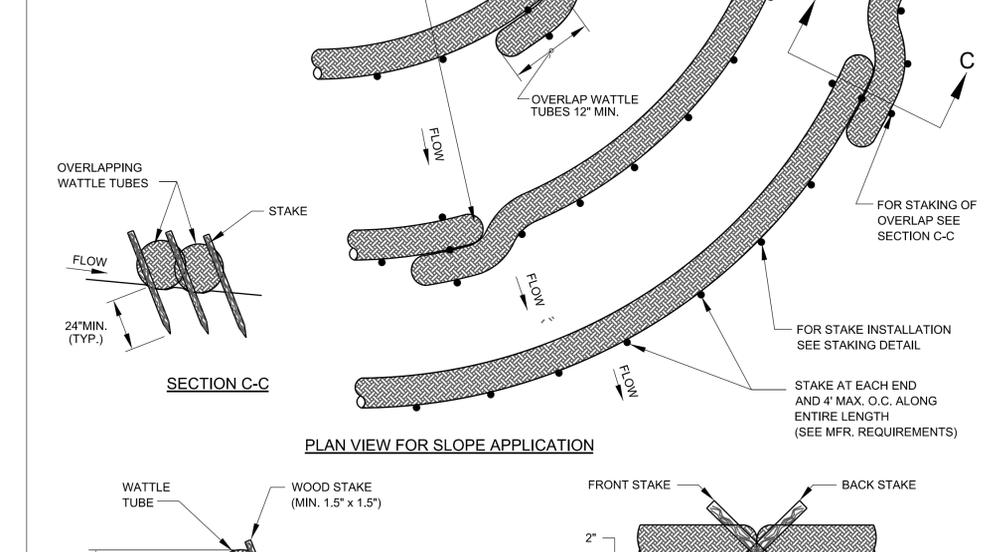
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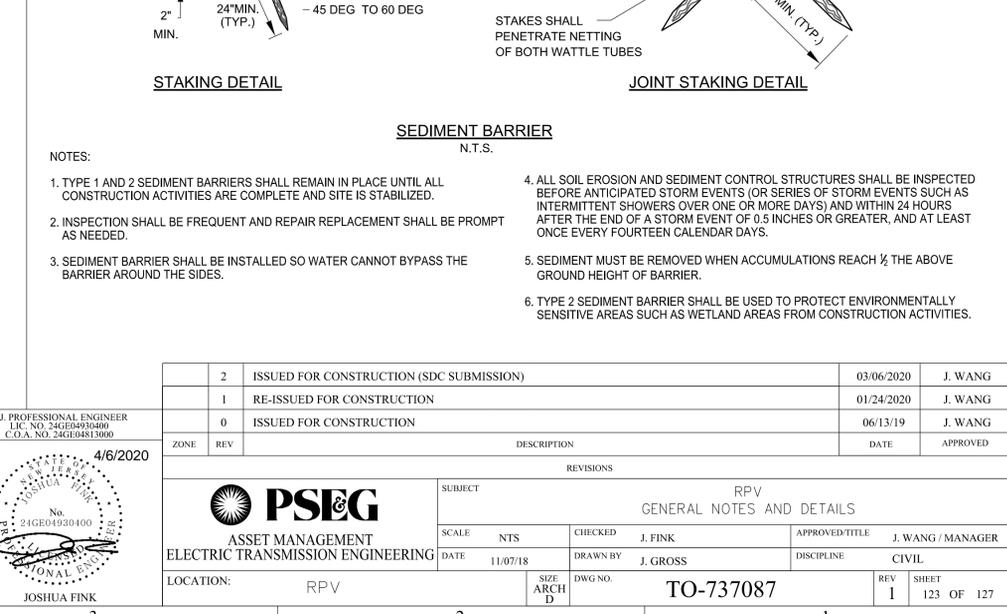
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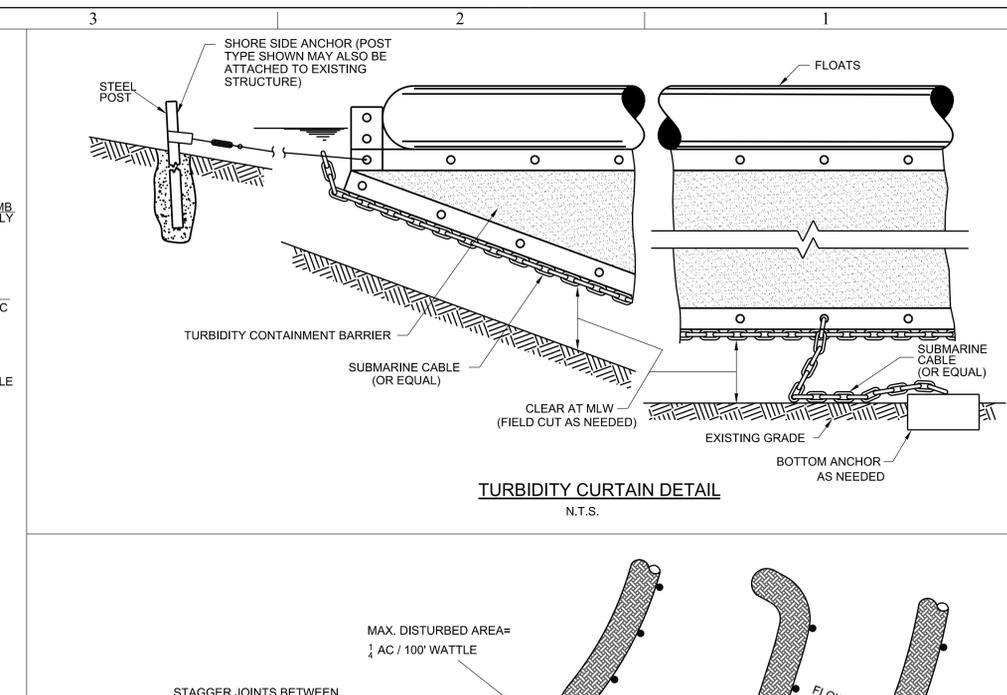
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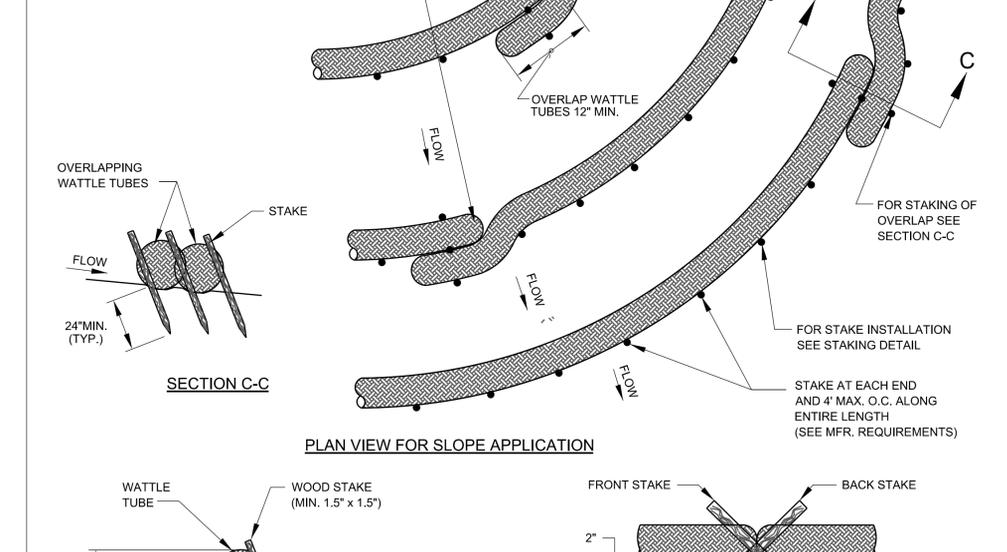
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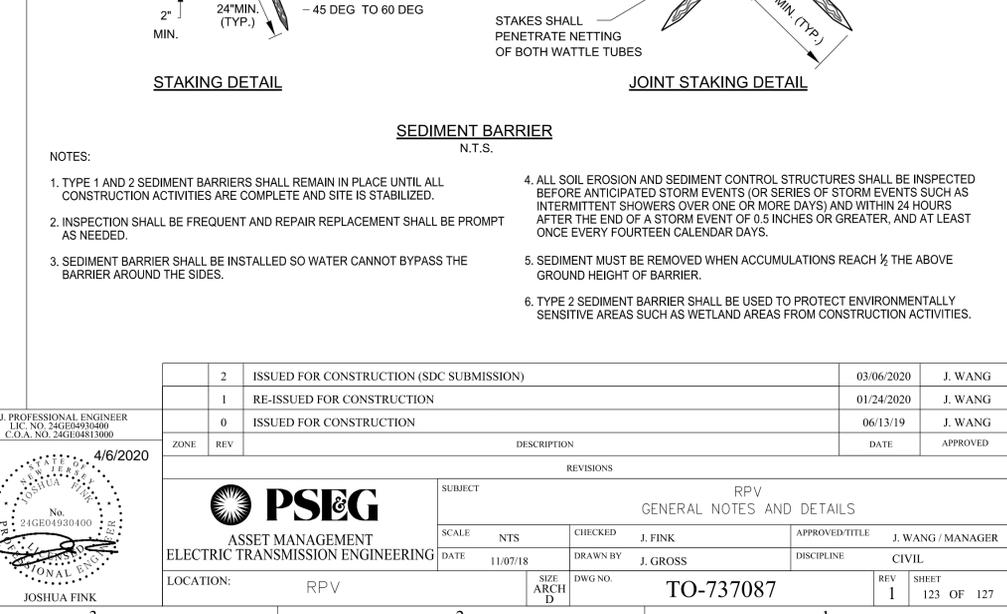
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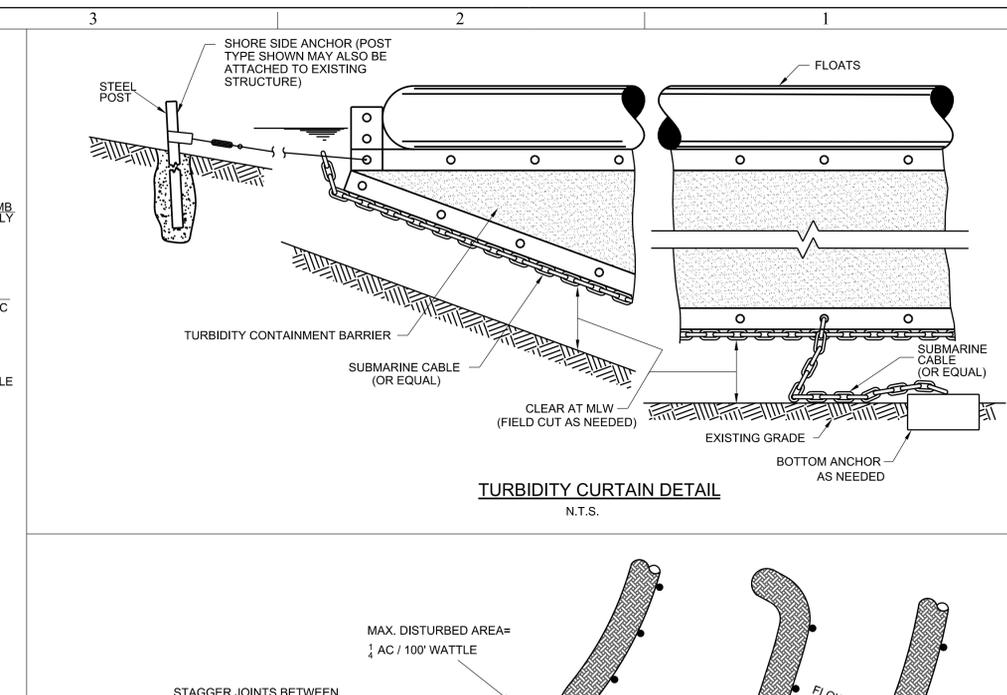
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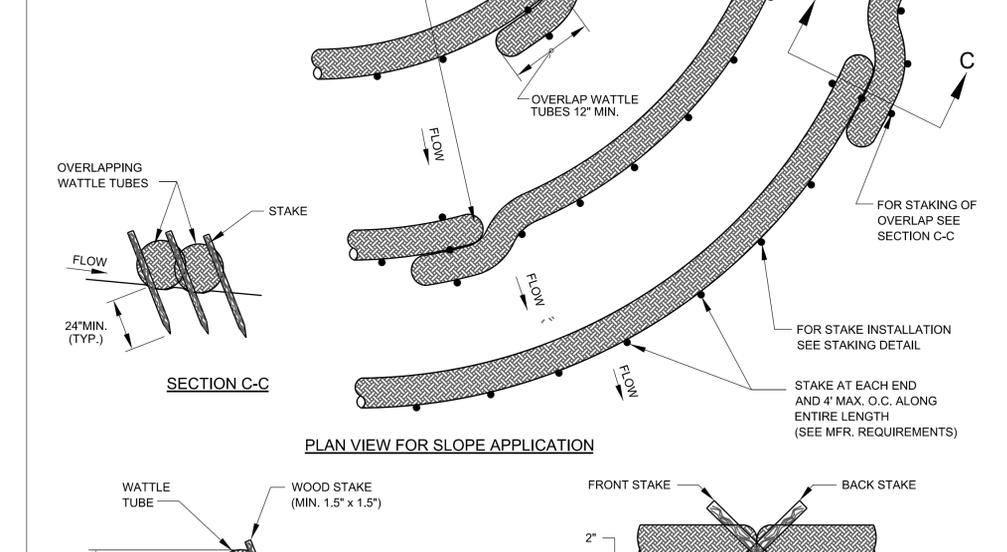
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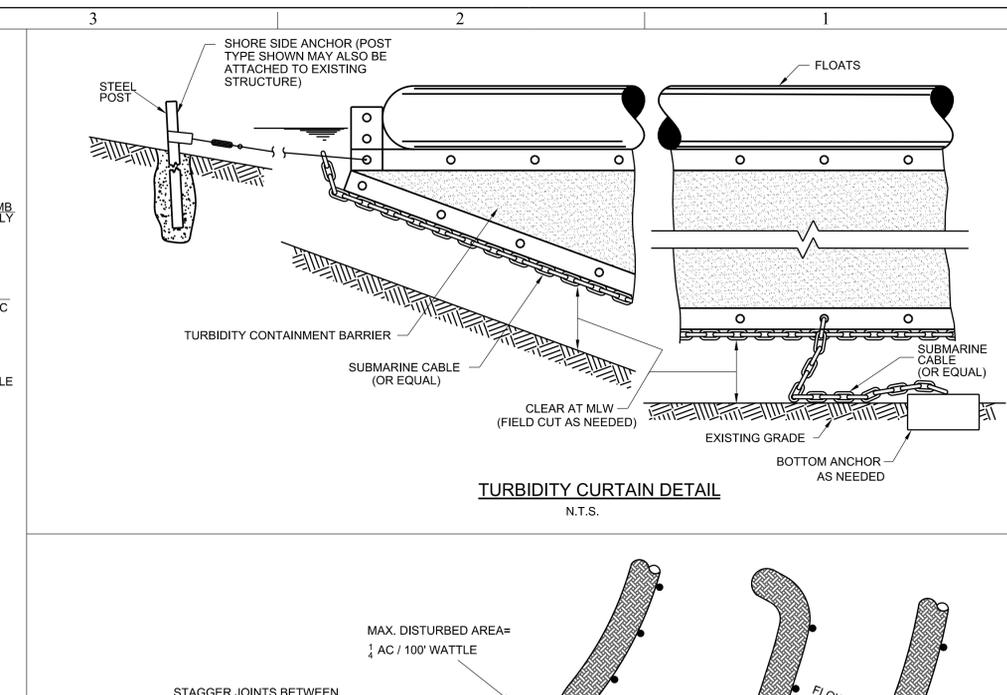
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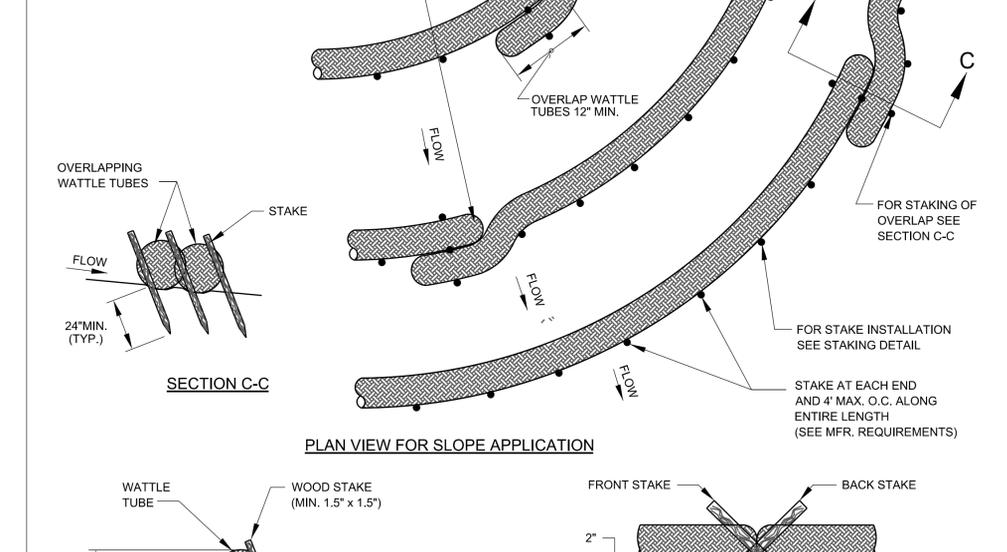
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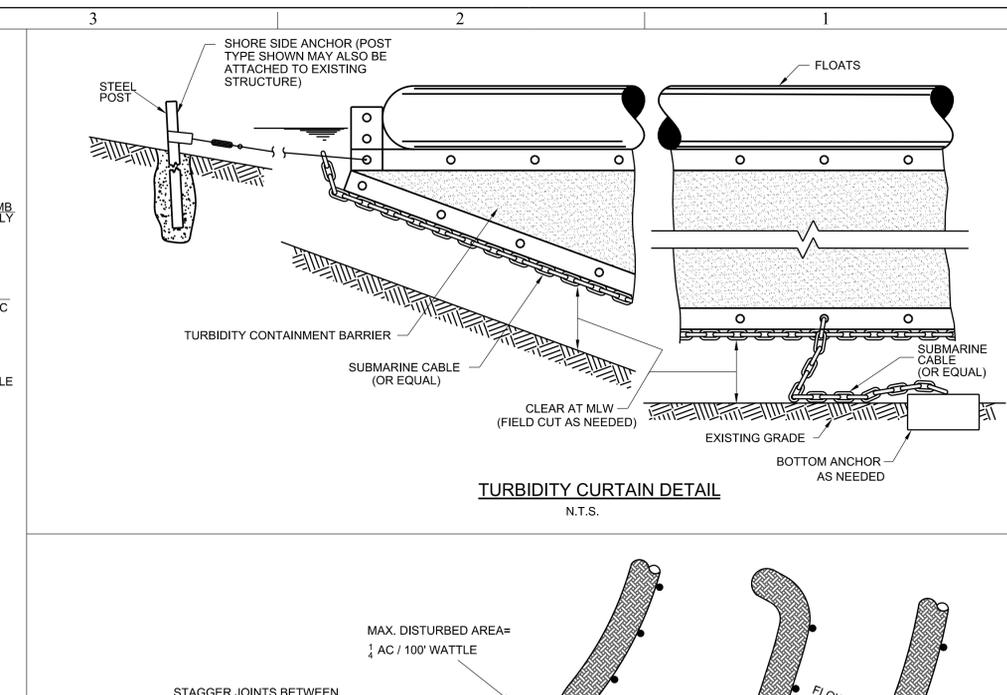
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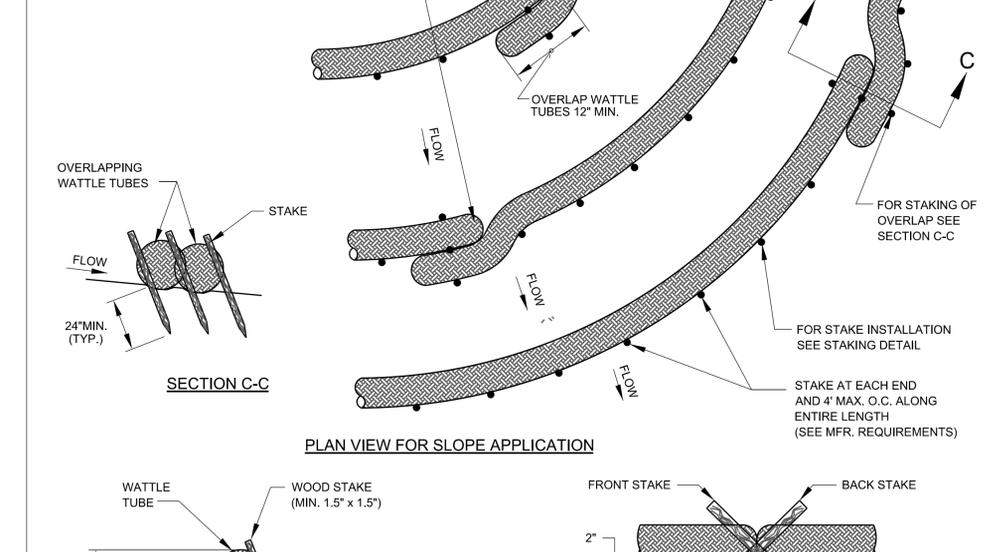
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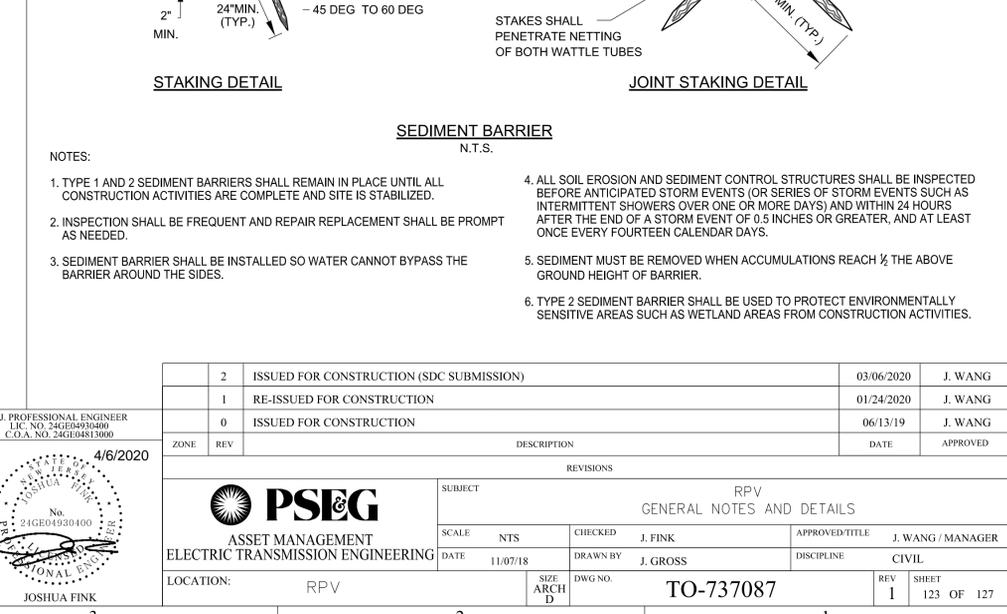
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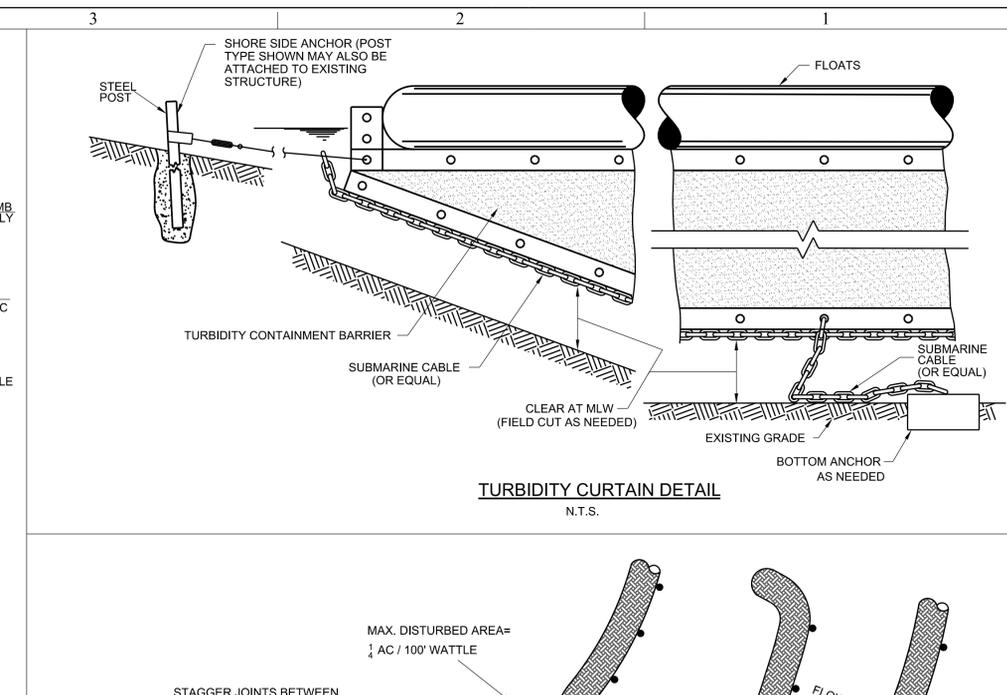
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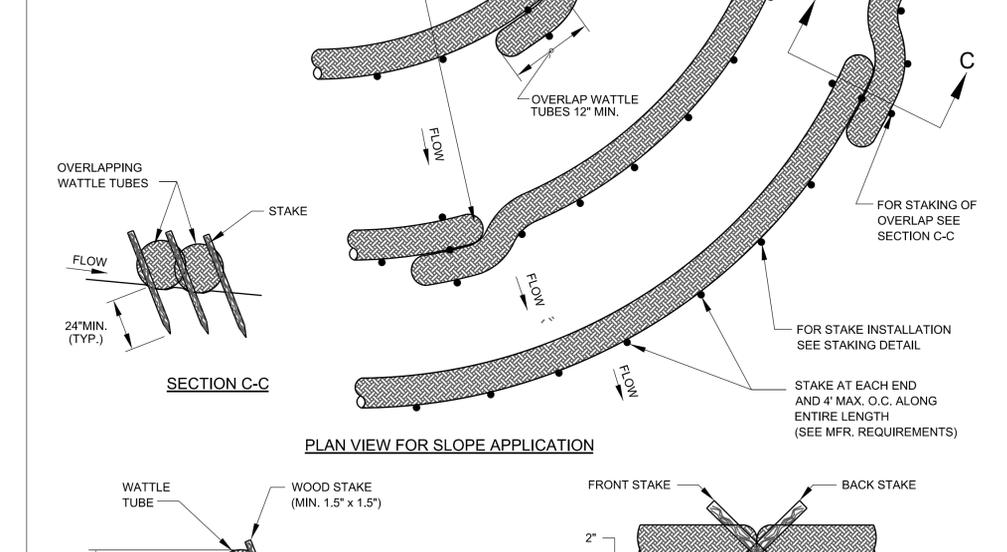
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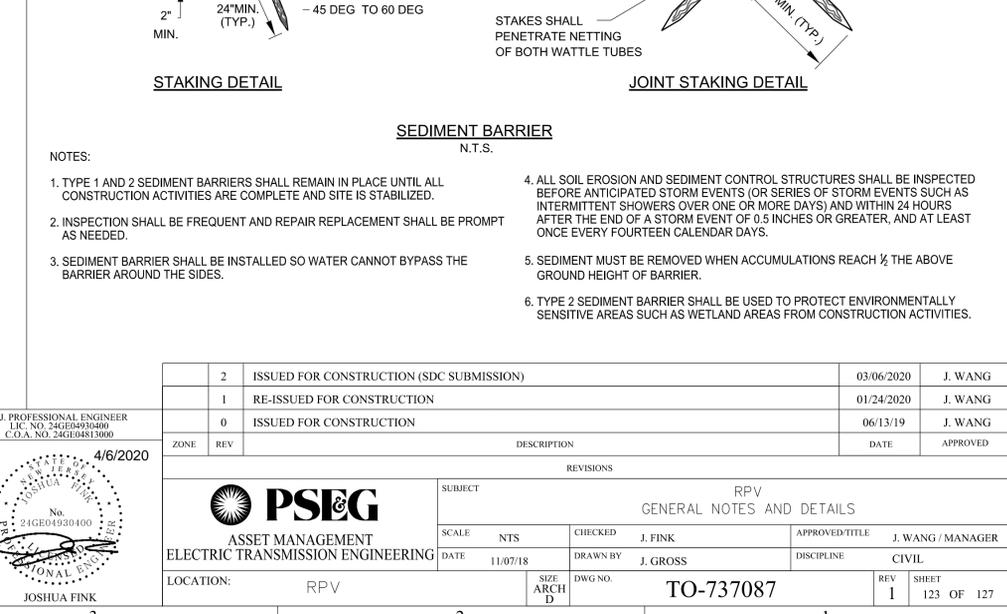
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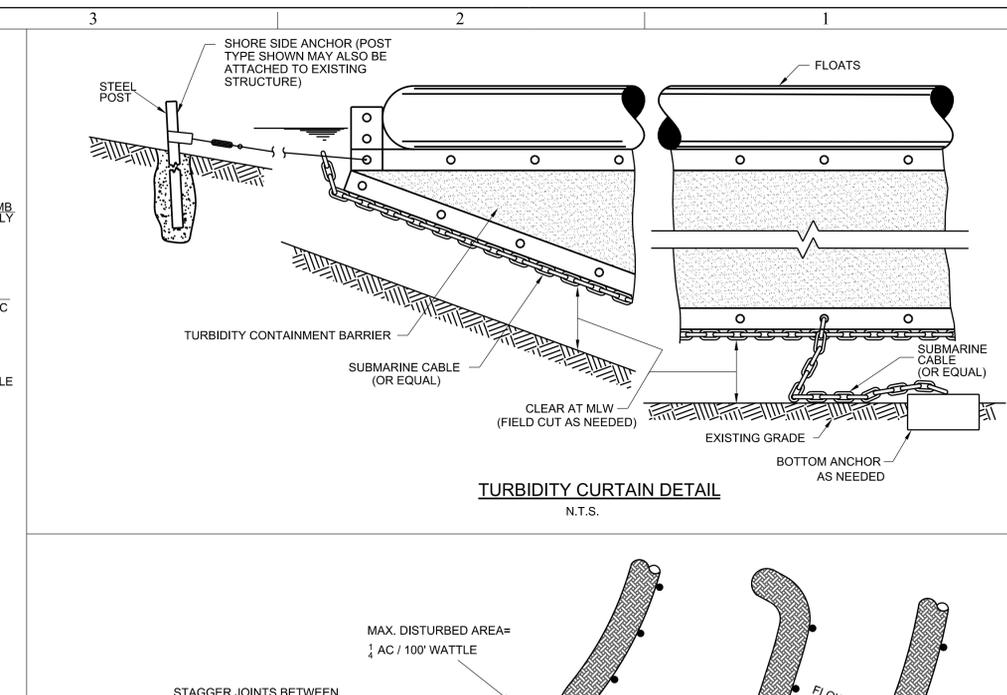
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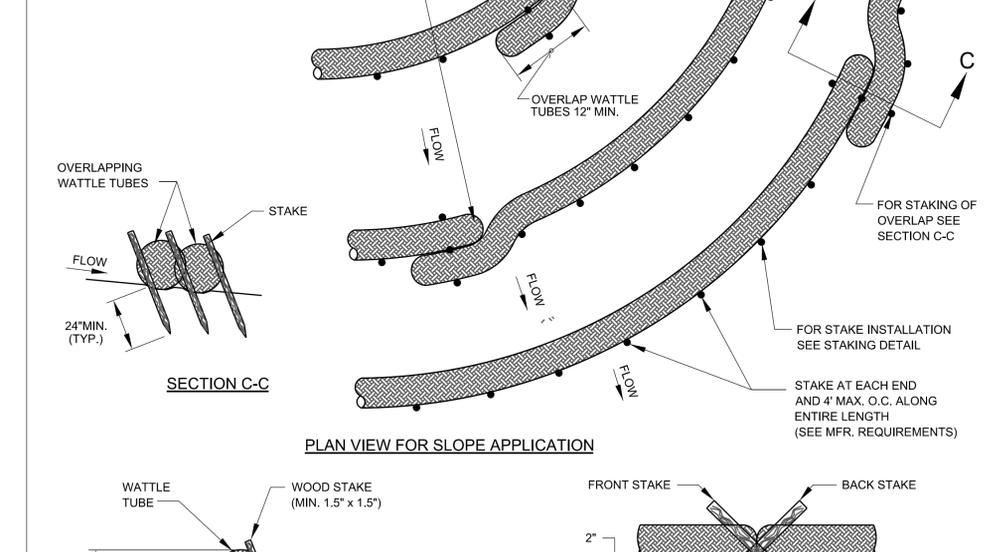
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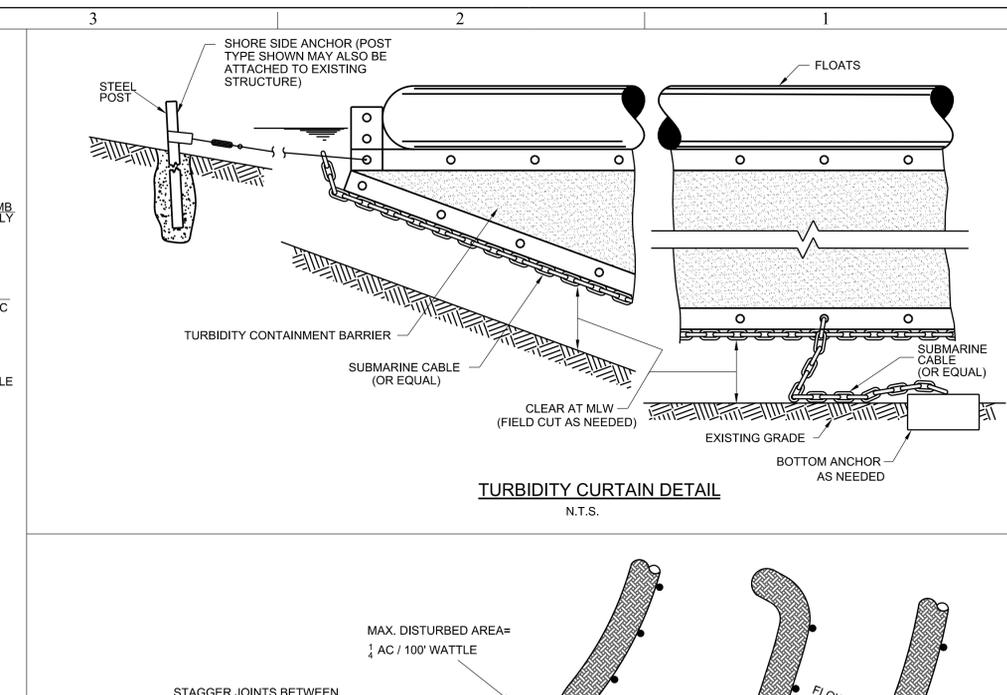
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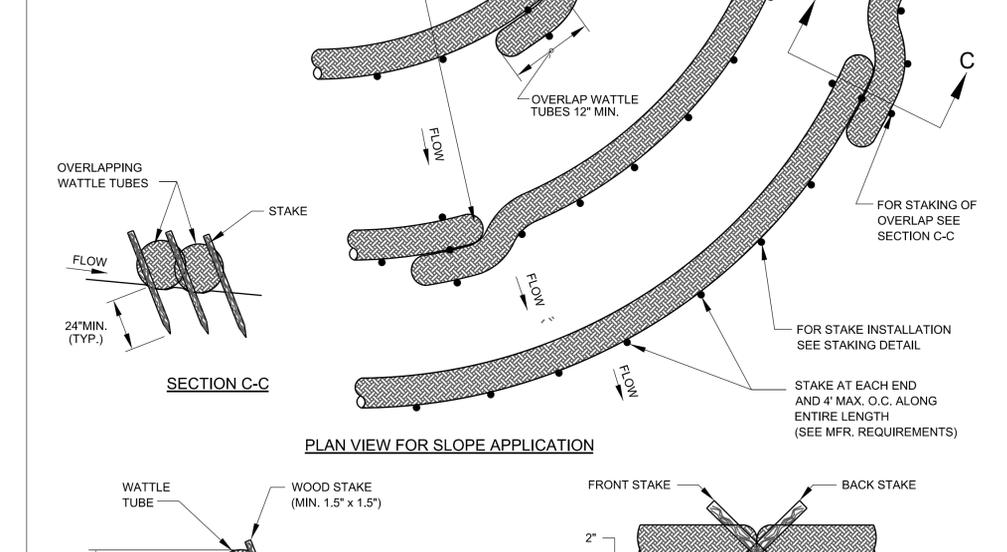
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HEAVY DUTY SILT FENCE
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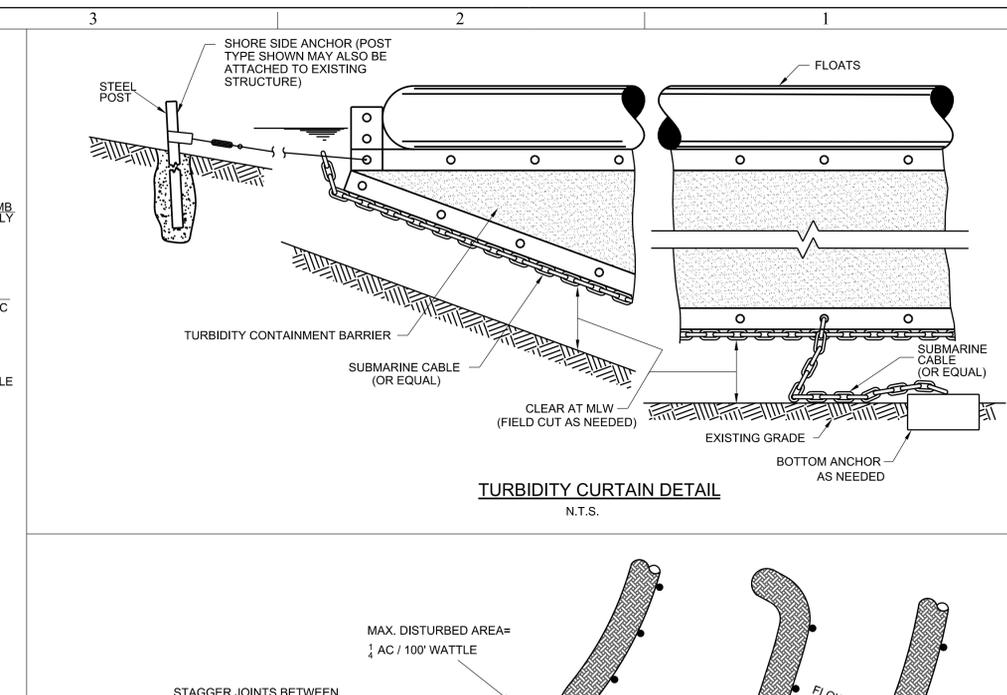
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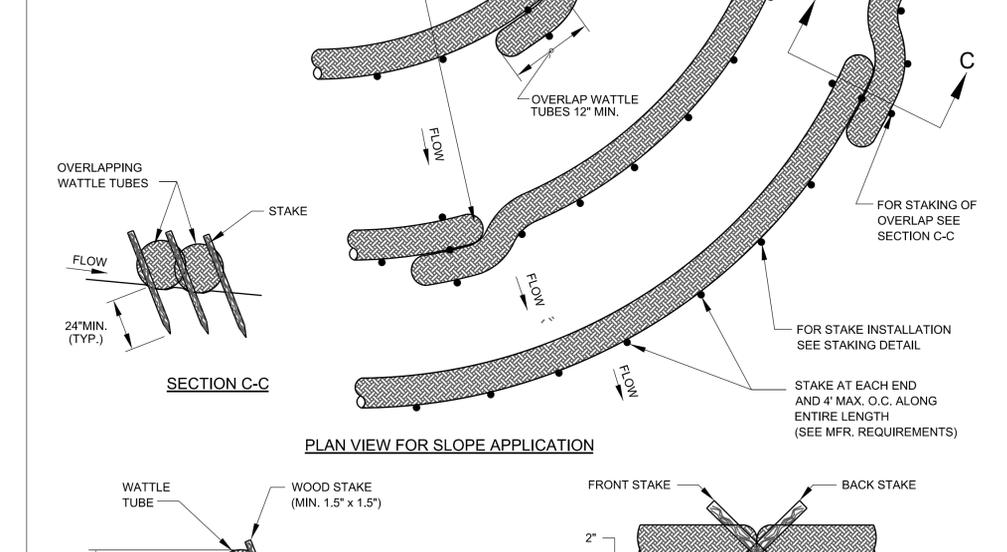
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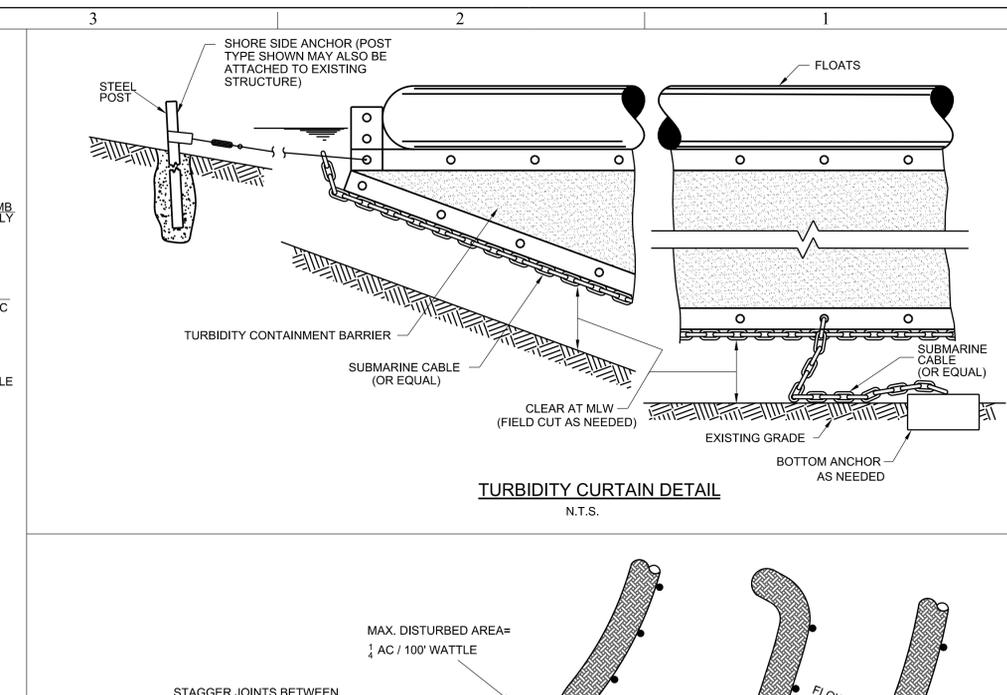
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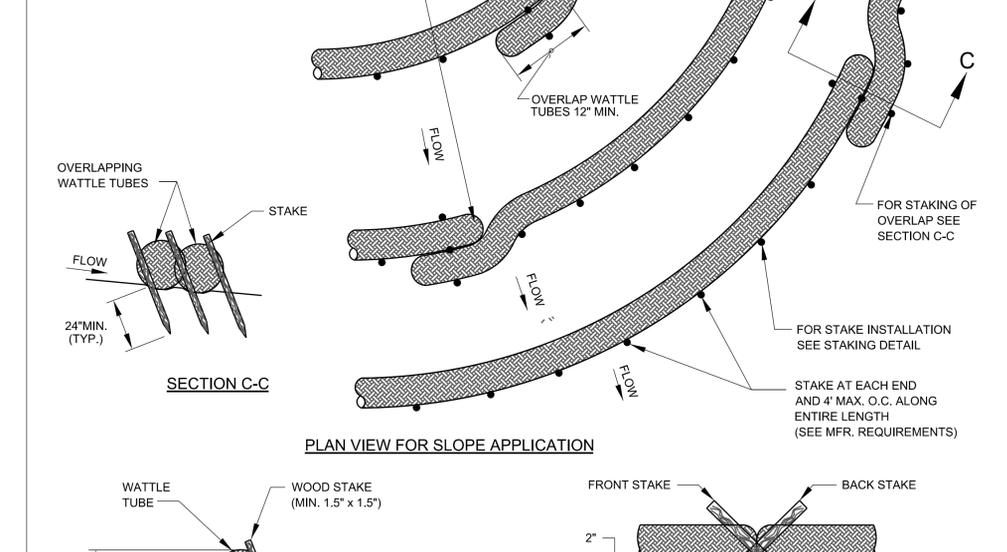
INSTALLATION NOTES



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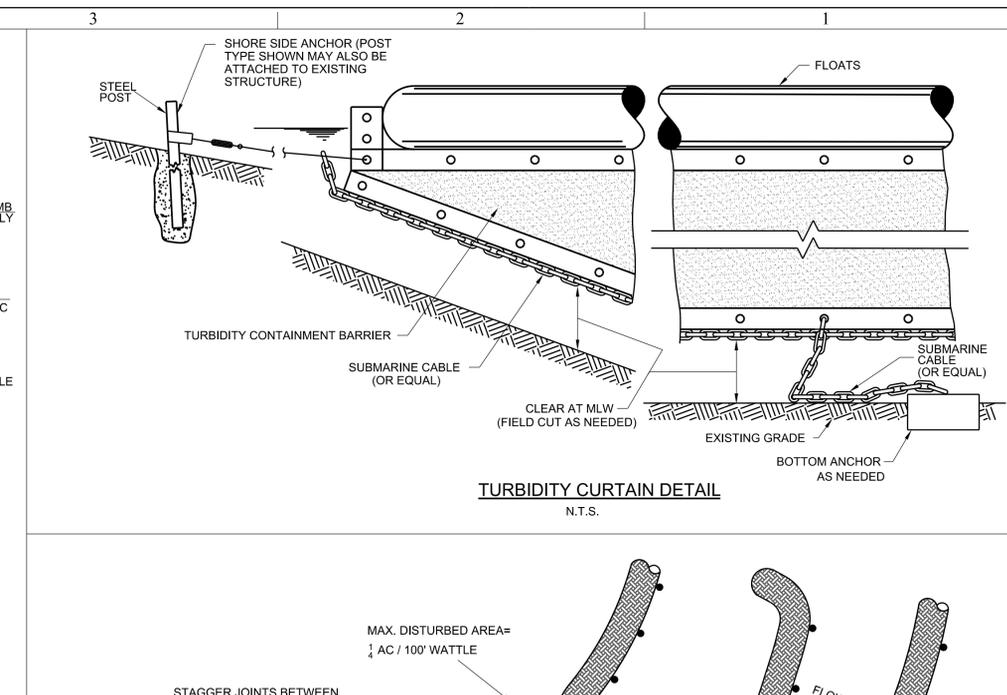
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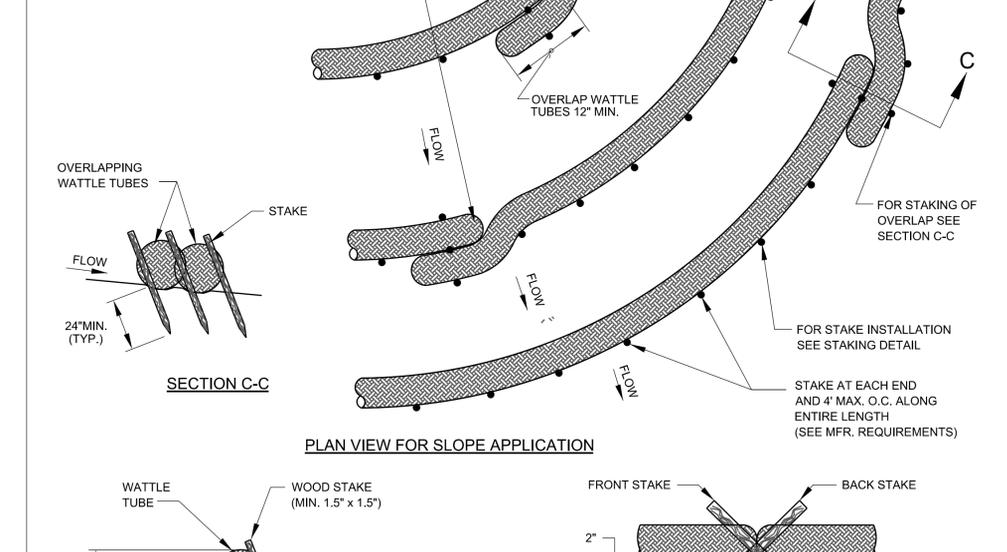
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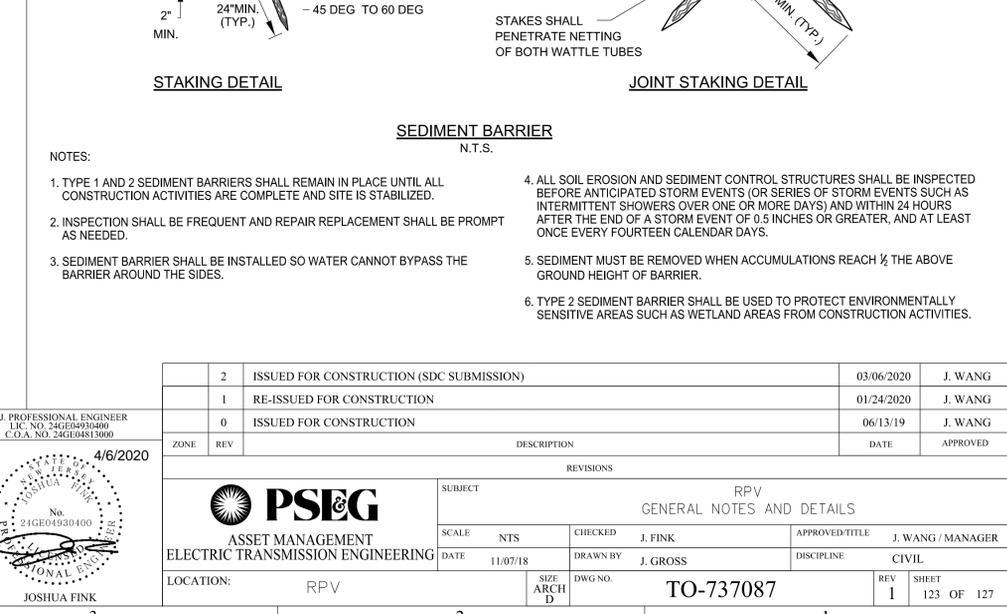
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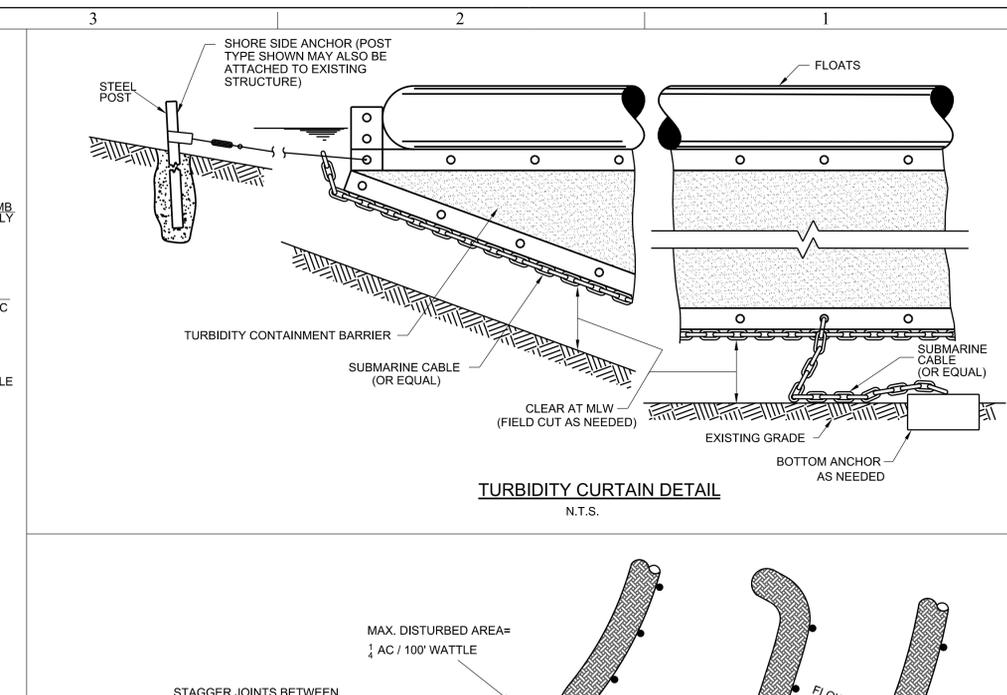
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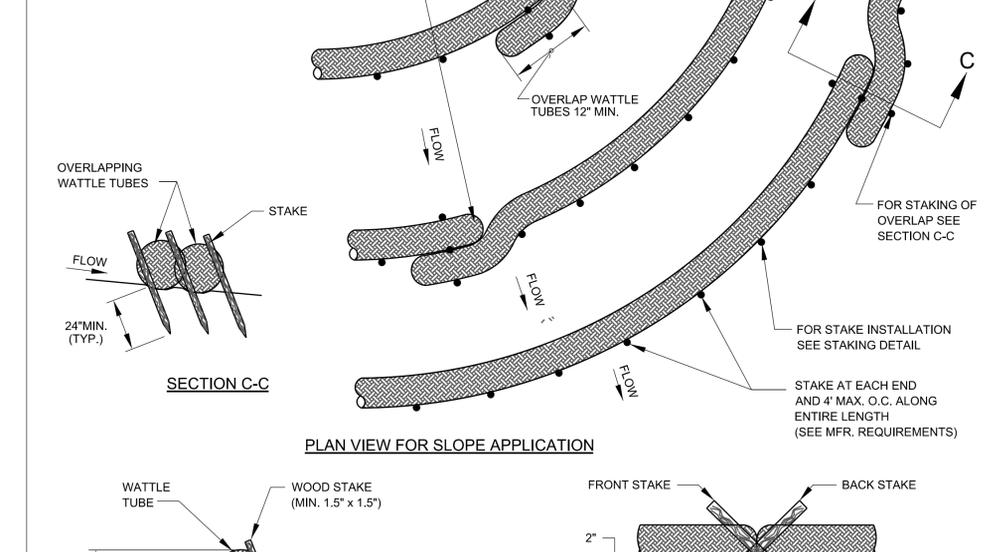
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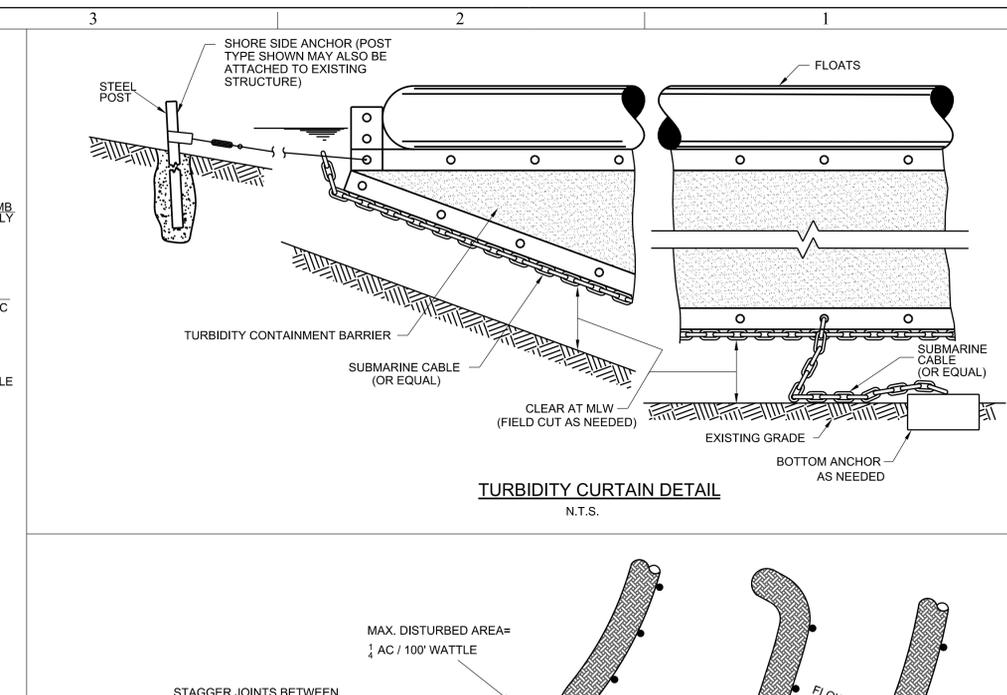
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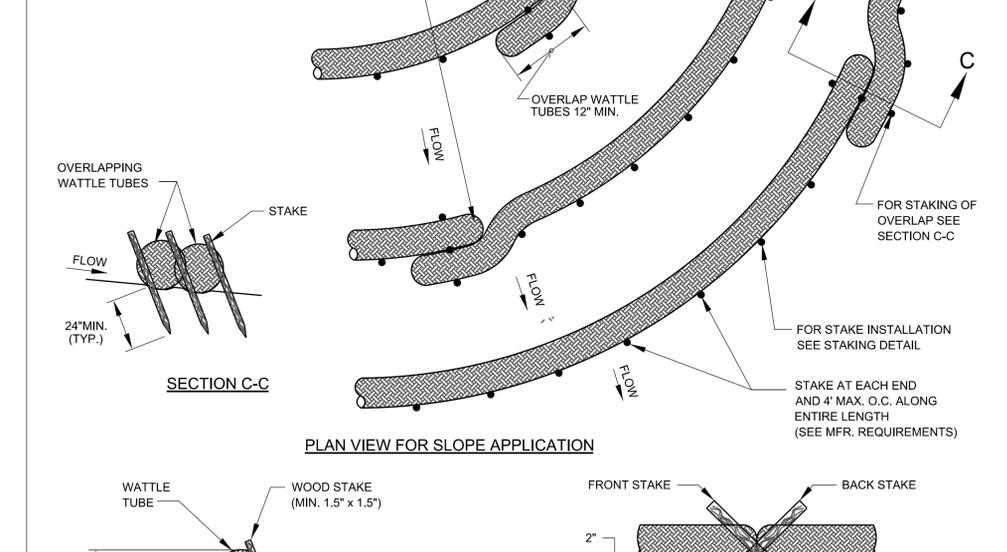
INSTALLATION NOTES



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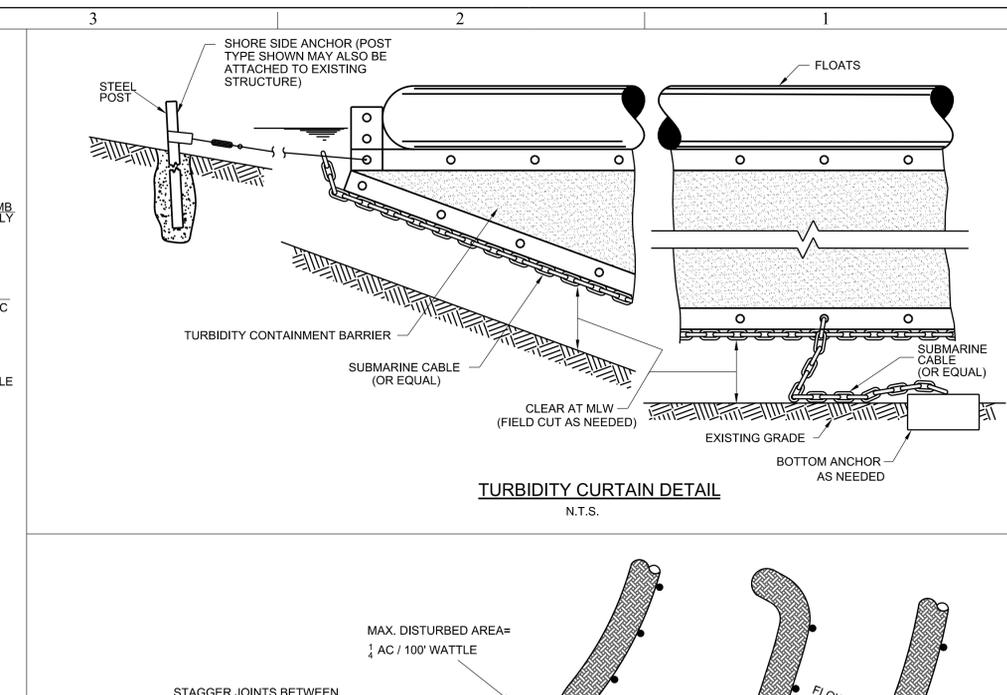
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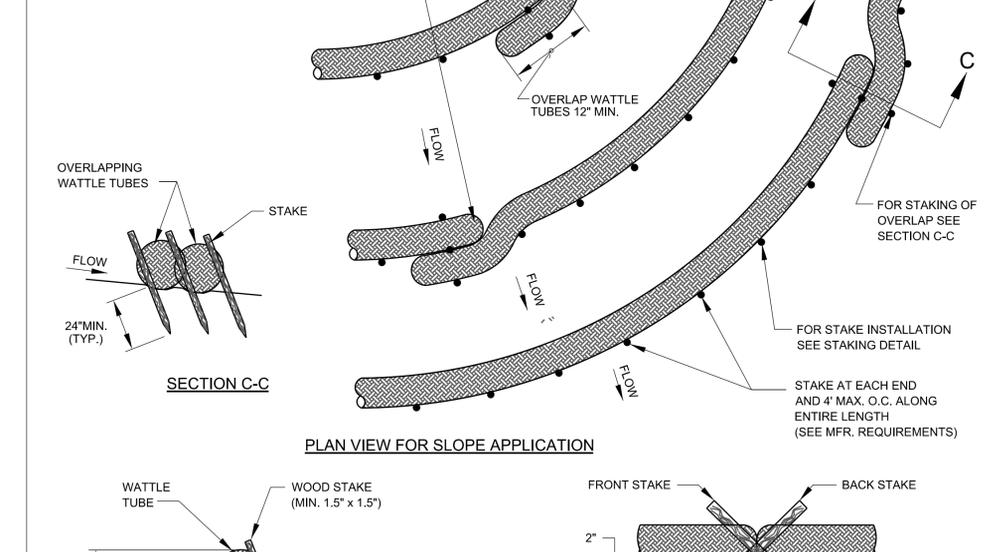
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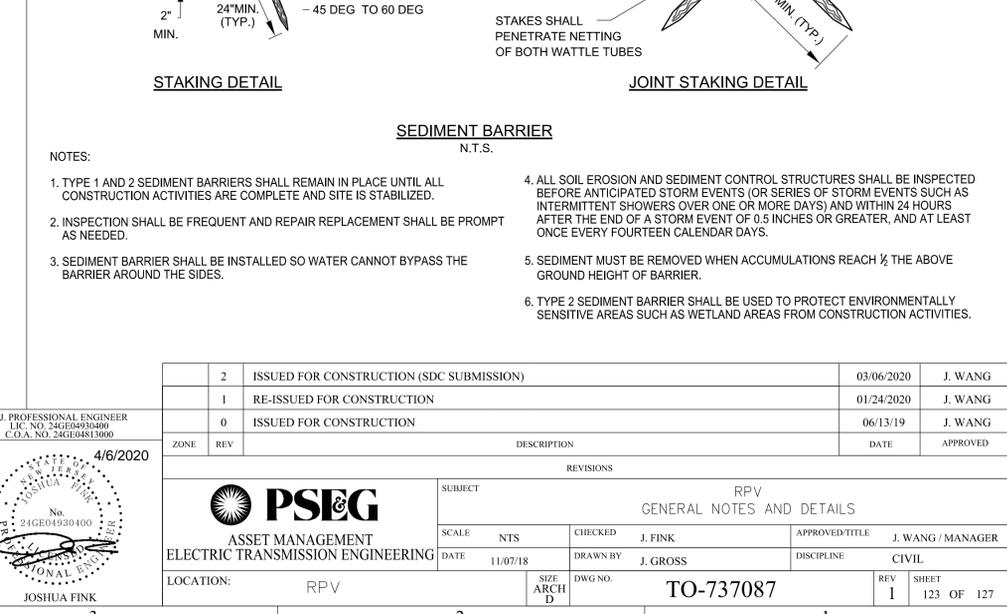
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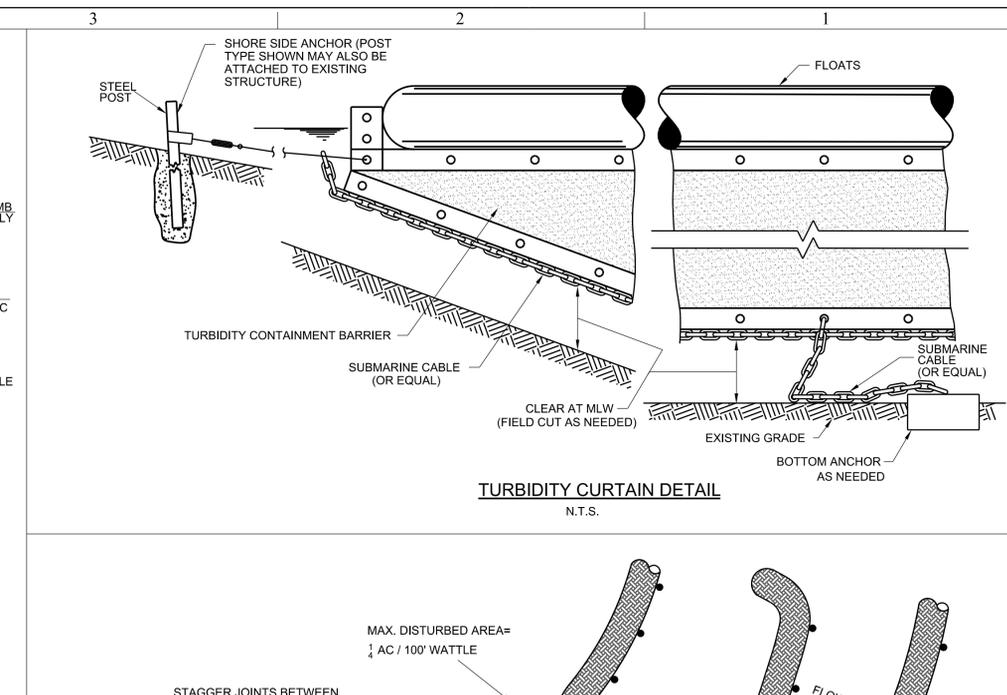
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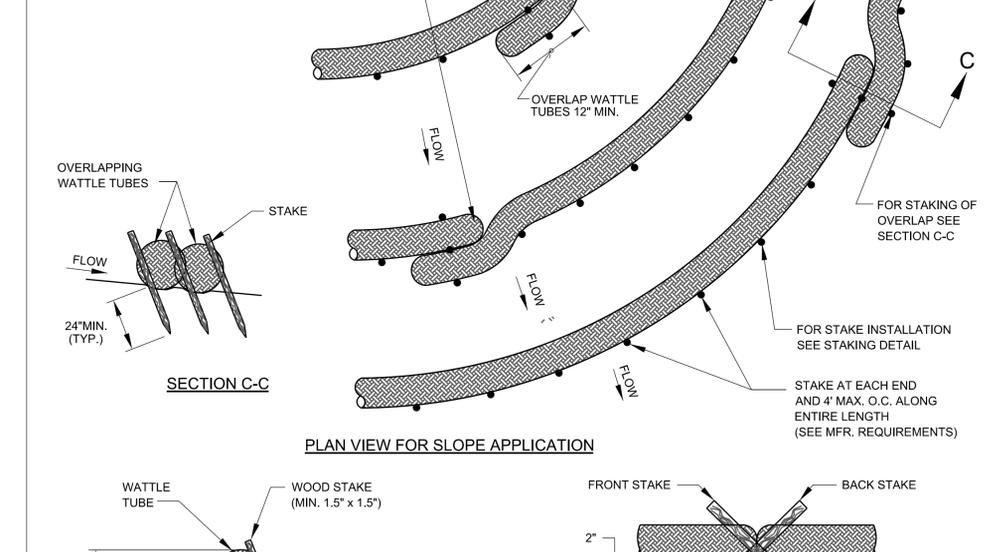
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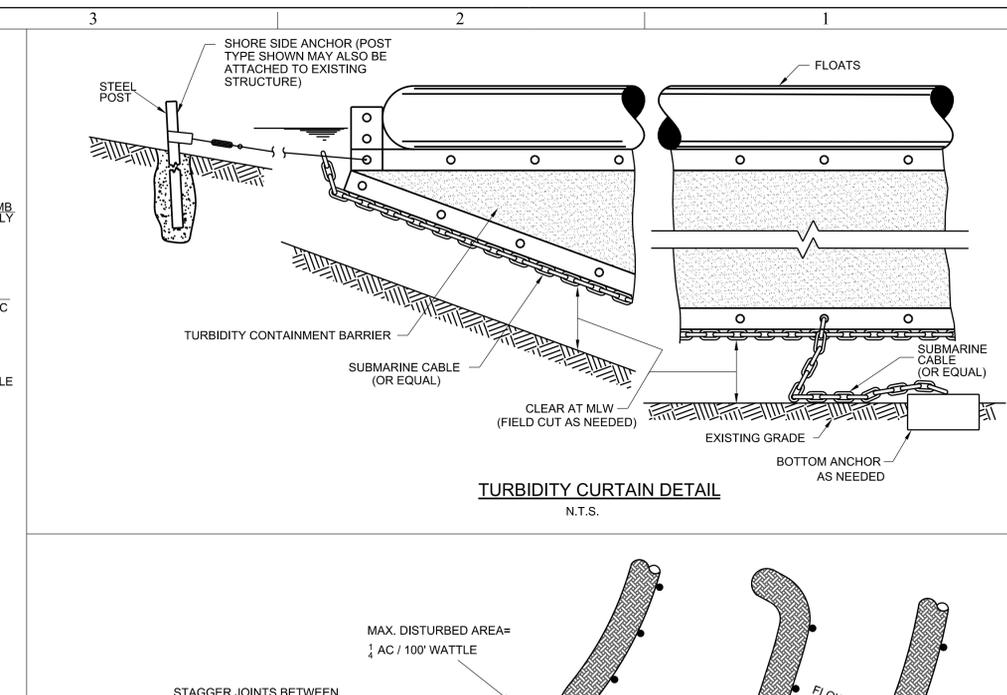
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N.T.S.



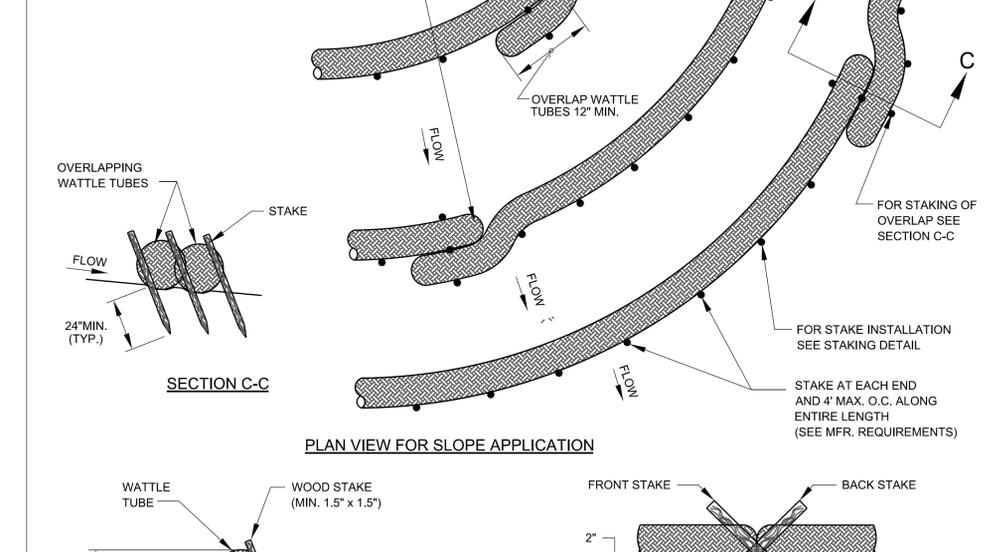
INSTALLATION NOTES



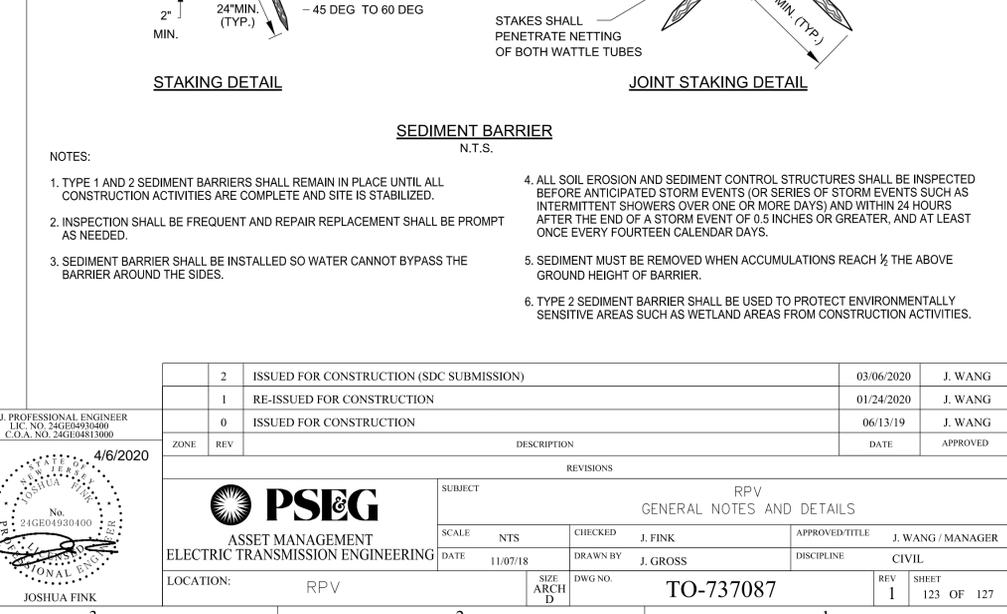
INLET FILTER DETAIL
N.T.S.



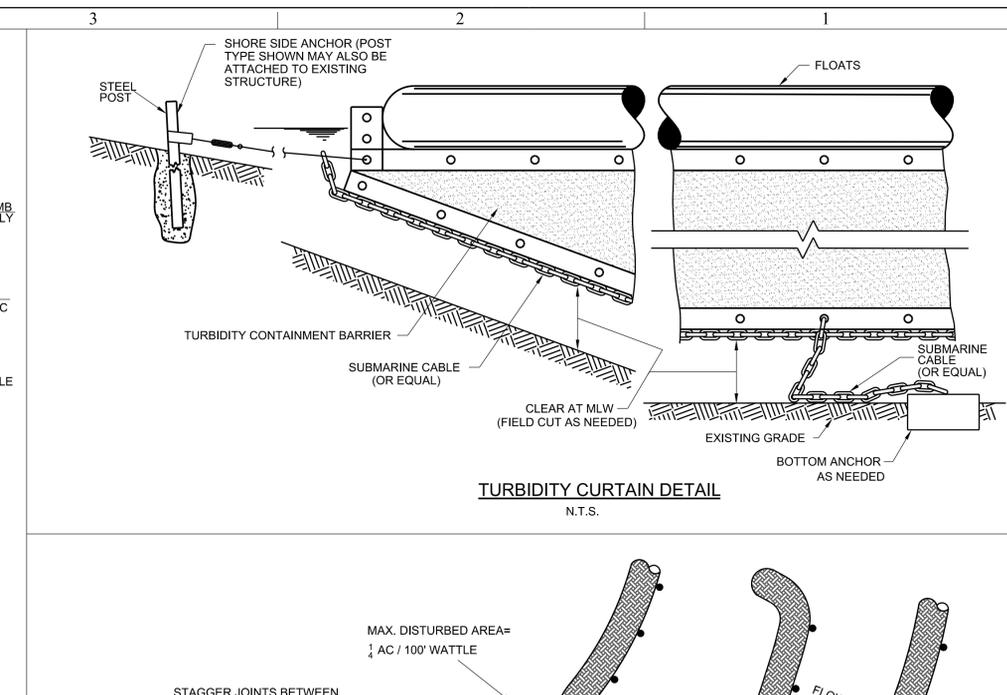
SILT FENCE
N.T.S.



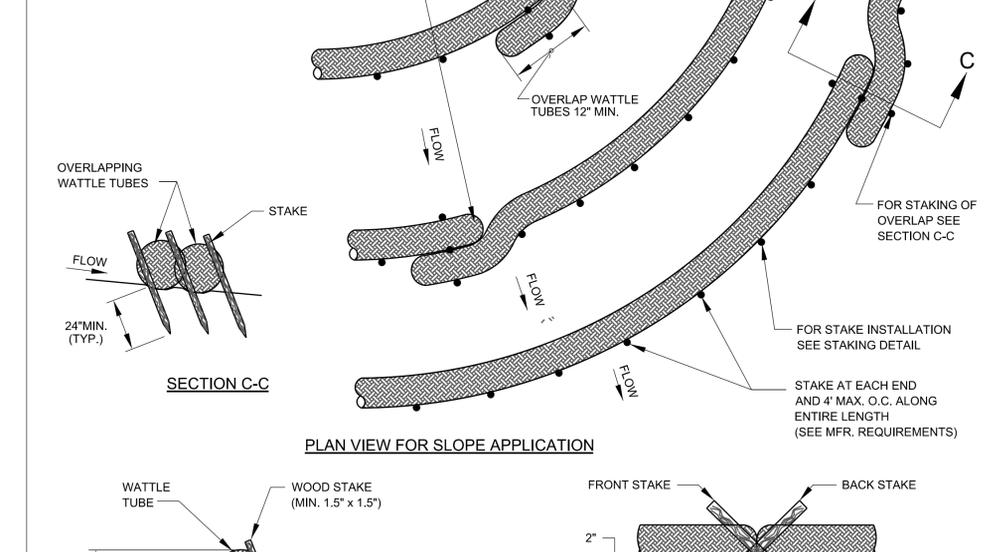
HAY BALE DETAIL
N.T.S.



HEAVY DUTY SILT FENCE
N.T.S.



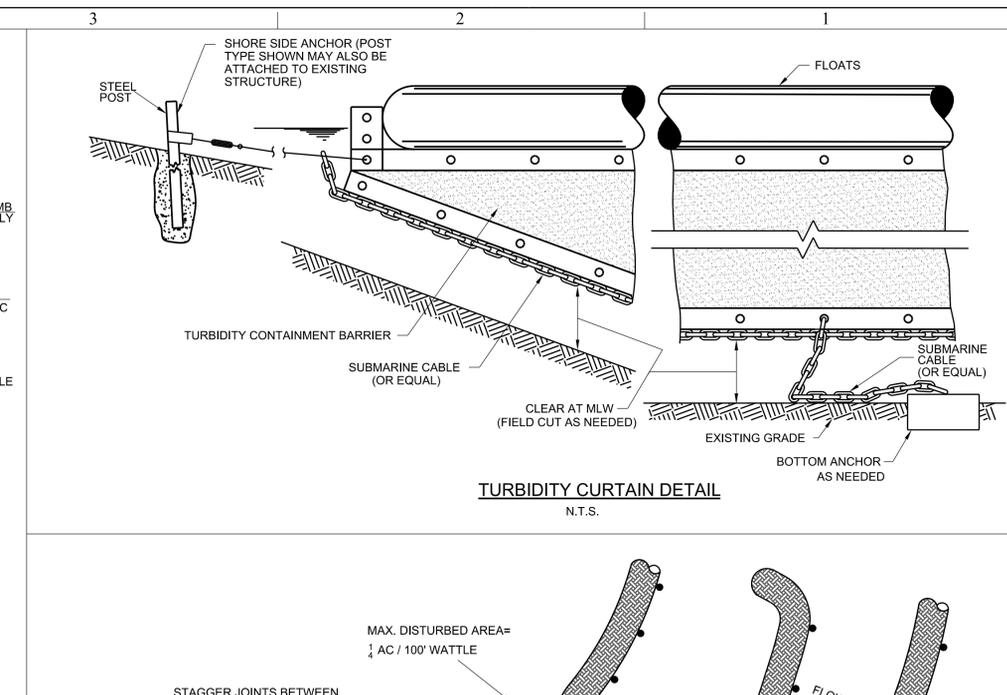
TURBIDITY CURTAIN DETAIL
N.T.S.



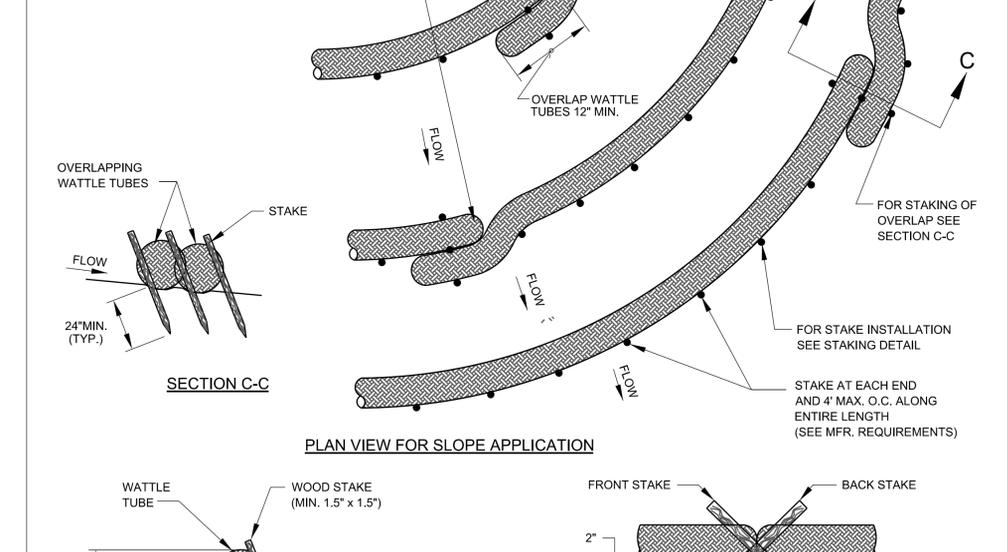
INSTALLATION NOTES



INLET FILTER DETAIL
N.T.S.



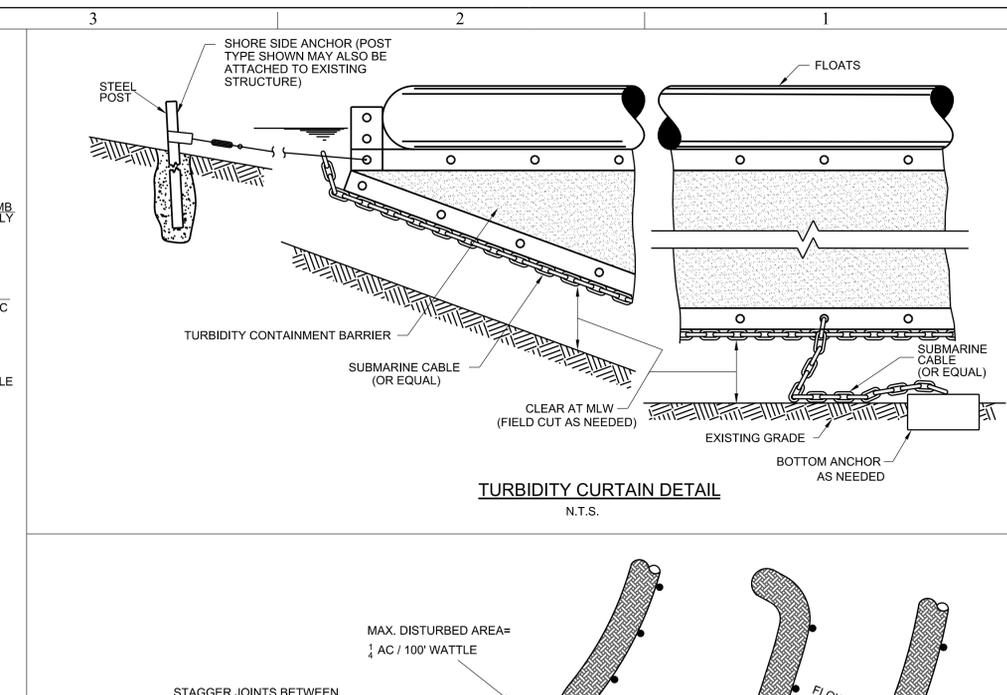
SEDIMENT BARRIER
N.T.S.



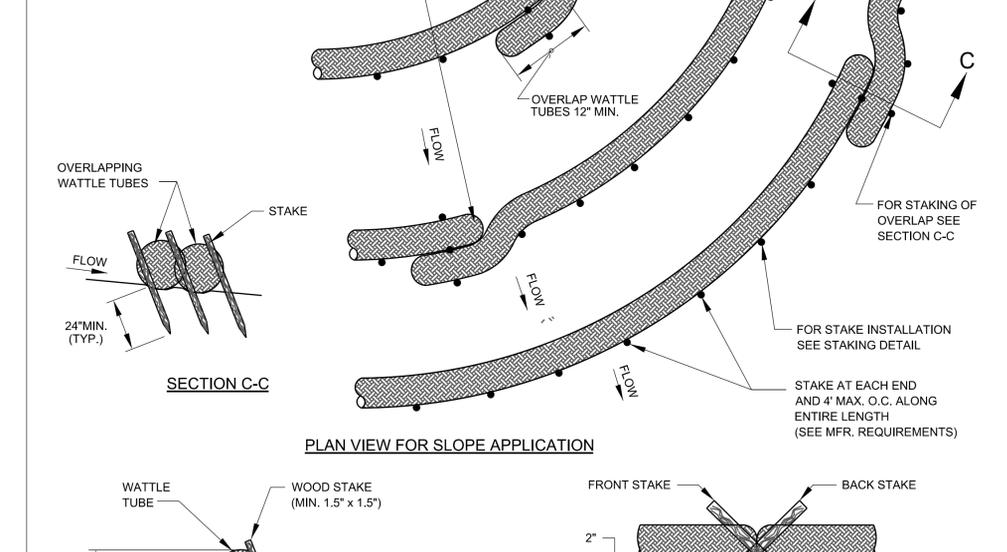
INSTALLATION NOTES



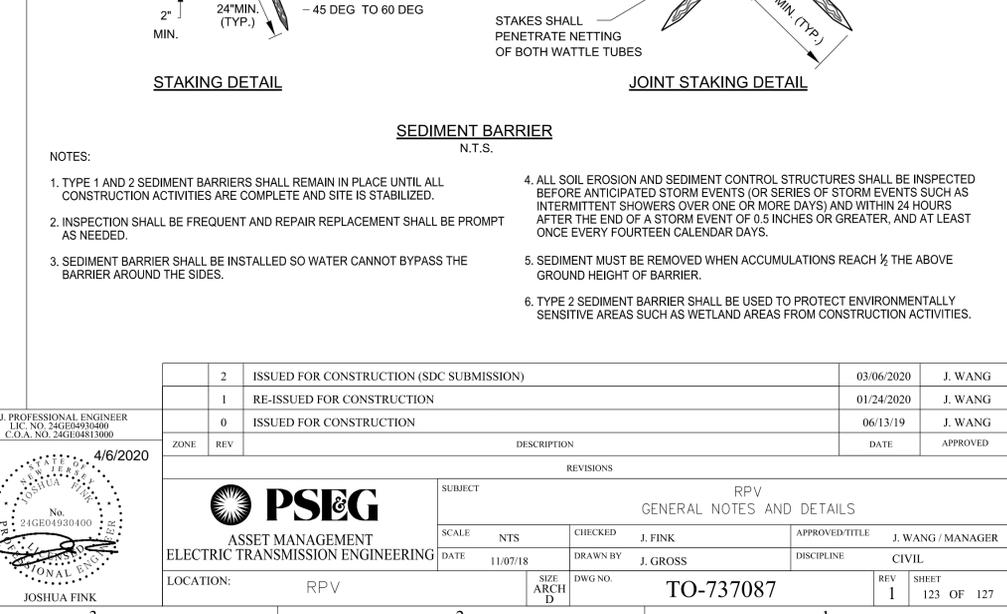
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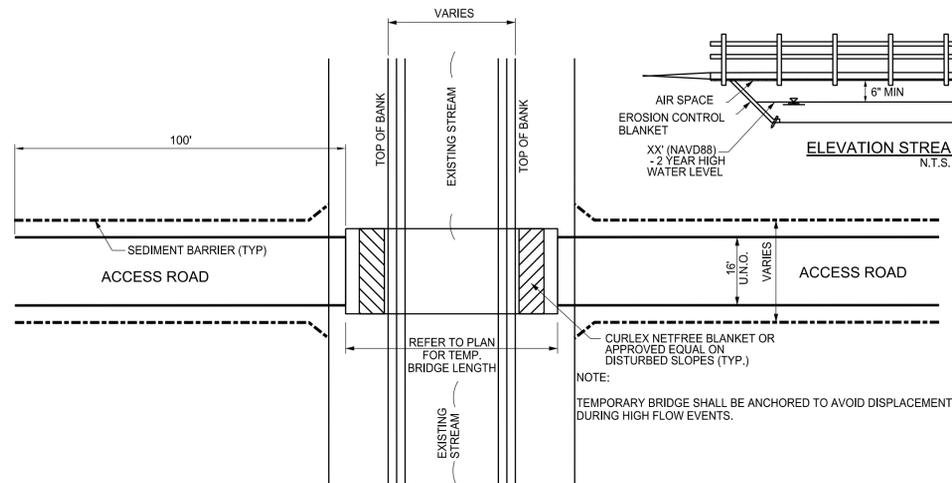
SILT FENCE
N.T.S.



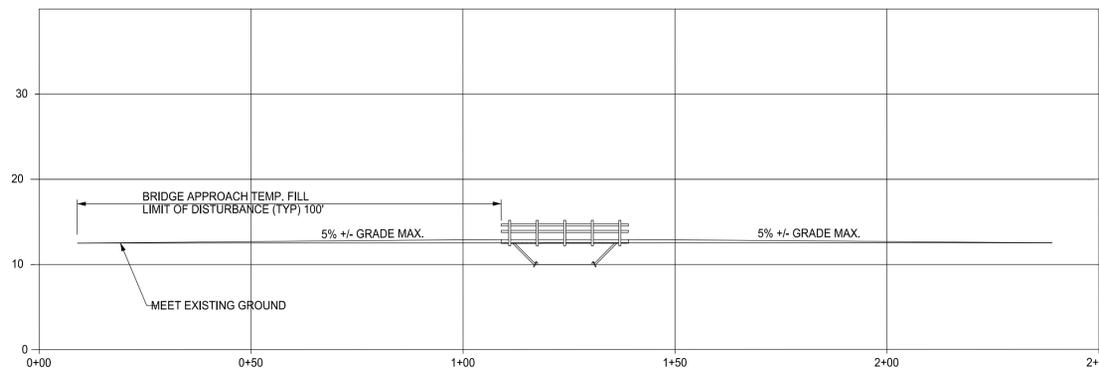
HAY BALE DETAIL
N.T.S.



<



PLAN AT STREAM CROSSING
SCALE: N.T.S.

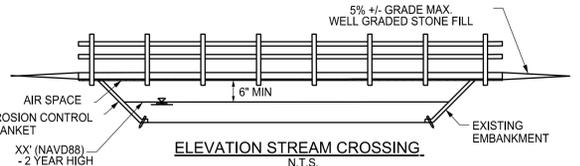


PROFILE AT STREAM CROSSING
SCALE: 1"=20' HORIZ.
1"=10' VERT.

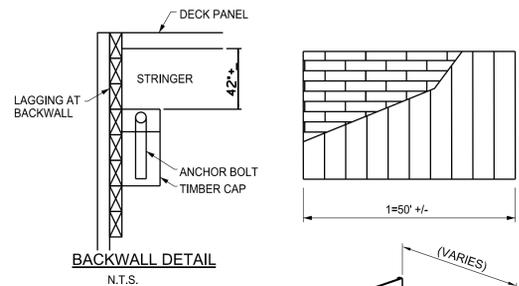
TEMPORARY BRIDGE DETAILS
(SEE SHEETS 28 AND 78 FOR LOCATION)

NOTES:

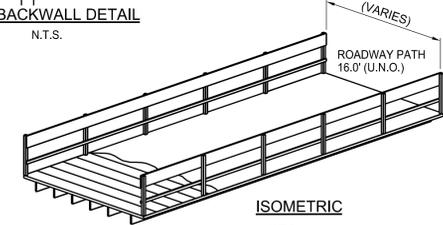
1. ALL CONSTRUCTION WILL COMPLY WITH DISTRICT SOIL EROSION AND SEDIMENT CONTROL STANDARDS TO INCLUDE TEMPORARY AND PERMANENT VEGETATION.
2. TEMPORARY BRIDGE AND CULVERT RELIEF ARE INTENDED TO MINIMIZE DISTURBANCE TO SENSITIVE AREAS.
3. WHEN CONDITIONS DICTATE, DECK MATS WILL BE UTILIZED AT UNDER STRENGTH CULVERT LOCATIONS.
4. BRIDGE SHALL BE DESIGNED BY THE CONTRACTOR FOR HS25 HIGHWAY LOADS (U.N.O.).



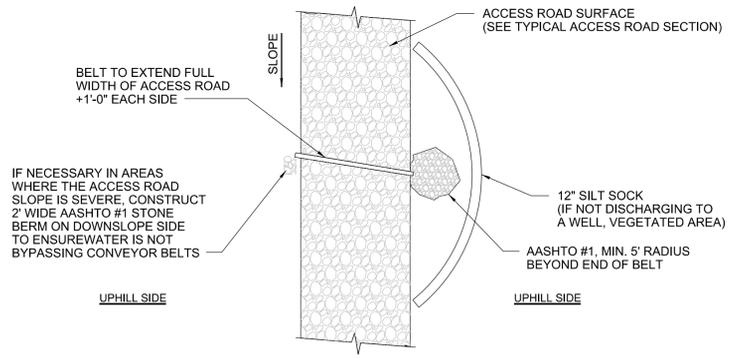
ELEVATION STREAM CROSSING
N.T.S.



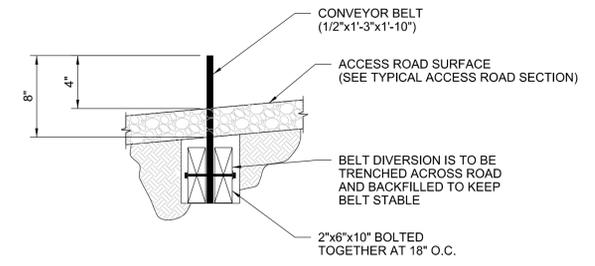
BACKWALL DETAIL
N.T.S.



ISOMETRIC
N.T.S.



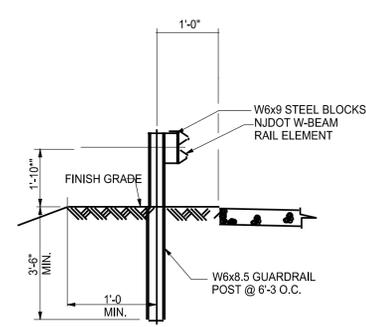
CONVEYOR BELT DIVERSION - PLAN VIEW
N.T.S.



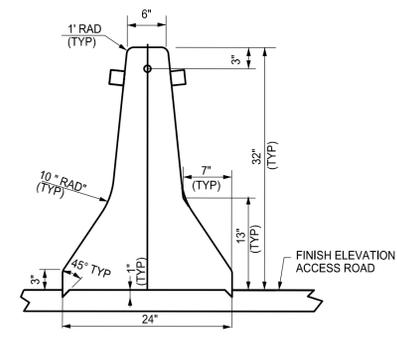
CONVEYOR BELT DIVERSION - SECTION VIEW
N.T.S.

NOTES:

1. CONVEYOR BELTS ARE TO BE INSTALLED ALONG ACCESS ROADS WHERE INDICATED ON THE E&S PLANS. IN GENERAL CONVEYOR BELT DIVERSIONS ARE TO BE PLACED AT 60' INTERVALS WHEN ACCESS ROADS EXCEED 15% AND 80' INTERVALS WHEN ACCESS ROADS EXCEED 10% OR DEEMED NECESSARY IN THE FIELD.
2. CONVEYOR BELTS SHOULD BE DIRECTED TO A WELL VEGETATED AREA OR PROVIDE AN 8" SOIL SOCK AS SHOWN ABOVE IN THE PLAN VIEW.
3. THE CONVEYOR BELTS SHOULD BE INSPECTED FOR WORN OR BROKEN PIECES AND REPLACED AS NECESSARY AFTER SEVERE STORMS AND IN THE SPRING AND FALL.
4. DUE TO THE PROPOSED DURATION OF CONSTRUCTION IN DISTURBED AREAS ALL SOXX (DIVERSION AND SEDIMENT CONTROL) SHALL BE HEAVY DUTY MULTI-FILAMENT POLY-PROPYLENE ONLY.
5. ALL SOCKS SHALL BE INSTALLED IN A CONTINUOUS FASHION WITH MECHANICALLY FUSED JOINTS AS NOTED ABOVE.



GUARDRAIL DETAIL
N.T.S.



CONTRACTOR TO PROVIDE PROJECT ENGINEER WITH SIGNED AND SEALED DETAILS AND CALCULATIONS FOR ANCHORING METHOD OF BARRIER CURB

TYPE I WHITE CONCRETE BARRIER CURB
N.T.S.

N.J. PROFESSIONAL ENGINEER
LIC. NO. 24GE04930400
C.O.A. NO. 24GE04813000

4/6/2020

JOSHUA FINK

| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG | |
|----------------|--|--|---------|-------------------|
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG | |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG | |
| ZONE | REV | DESCRIPTION | DATE | APPROVED |
| REVISIONS | | | | |
| PSEG | | SUBJECT: RPV GENERAL NOTES AND DETAILS | | |
| SCALE: NTS | CHECKED: J. FINK | APPROVED/TITLE: J. WANG / MANAGER | | |
| DATE: 11/07/18 | DRAWN BY: J. GROSS | DISCIPLINE: CIVIL | | |
| LOCATION: RPV | SIZE ARCH: D | DWG. NO.: TO-737087 | REV: 1 | SHEET: 124 OF 127 |

MORRIS COUNTY SOIL EROSION AND SEDIMENT CONTROL NOTES

- ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES WILL BE INSTALLED IN ACCORDANCE WITH THE STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL IN NEW JERSEY, AND WILL BE IN PLACE PRIOR TO ANY MAJOR SOIL DISTURBANCE OR IN THEIR PROPER SEQUENCE AND MAINTAINED UNTIL PERMANENT PROTECTION IS ESTABLISHED.
- ANY DISTURBED AREA THAT WILL BE LEFT EXPOSED FOR MORE THAN FOURTEEN (14) DAYS AND NOT SUBJECT TO CONSTRUCTION TRAFFIC SHALL IMMEDIATELY RECEIVE A TEMPORARY SEEDING. IF THE SEASON PROHIBITS TEMPORARY SEEDING, THE DISTURBED AREAS WILL BE MULCHED WITH STRAW OR HAY AND TACKED IN ACCORDANCE WITH THE NEW JERSEY STANDARDS. SEE NOTE 22 BELOW.
- PERMANENT VEGETATION IS TO BE ESTABLISHED ON EXPOSED AREAS WITHIN TEN (10) DAYS AFTER FINAL GRADING. MULCH IS TO BE USED FOR PROTECTION UNTIL VEGETATION IS ESTABLISHED. SEE NOTE 23 BELOW.
- IMMEDIATELY FOLLOWING INITIAL DISTURBANCE OR ROUGH GRADING, ALL CRITICAL AREAS (STEEP SLOPES, SANDY SOILS, WET CONDITIONS) SUBJECT TO EROSION WILL RECEIVE A TEMPORARY SEEDING IN ACCORDANCE WITH NOTE 22 BELOW.
- TEMPORARY DIVERSION BERMS ARE TO BE INSTALLED ON ALL CLEARED ROADWAYS AND EASEMENT AREAS. SEE THE DIVERSION DETAIL.
- PERMANENT SEEDING AND STABILIZATION TO BE IN ACCORDANCE WITH THE "STANDARD FOR PERMANENT VEGETATIVE COVER FOR SOIL STABILIZATION". SPECIFIED RATES AND LOCATIONS SHALL BE ON THE APPROVED SOIL EROSION AND SEDIMENT CONTROL PLAN.
- THE SITE SHALL AT ALL TIMES BE GRADED AND MAINTAINED SO THAT ALL STORMWATER RUNOFF IS DIVERTED TO SOIL EROSION AND SEDIMENT CONTROL FACILITIES.
- ALL SEDIMENTATION STRUCTURES (SILT FENCE, INLET FILTERS, AND SEDIMENT BASINS) WILL BE INSPECTED AND MAINTAINED DAILY.
- STOCKPILES SHALL NOT BE LOCATED WITHIN 50' OF A FLOODPLAIN, SLOPE, DRAINAGE FACILITY, OR ROADWAY. ALL STOCKPILES BASES SHALL HAVE A SILT FENCE PROPERLY ENTRENCHED AT THE TOE OF SLOPE.
- A STABILIZED CONSTRUCTION ACCESS WILL BE INSTALLED, WHENEVER AN EARTHEN ROAD INTERSECTS WITH A PAVED ROAD. SEE THE STABILIZED CONSTRUCTION ACCESS DETAIL AND CHART FOR DIMENSIONS.
- ALL NEW ROADWAYS WILL BE TREATED WITH SUITABLE SUB BASE UPON ESTABLISHMENT OF FINAL GRADE ELEVATIONS.
- PAVED ROADWAYS MUST BE KEPT CLEAN AT ALL TIMES.
- BEFORE DISCHARGE POINTS BECOME OPERATIONAL, ALL STORM DRAINAGE OUTLETS WILL BE STABILIZED AS REQUIRED.
- ALL DEWATERING OPERATIONS MUST BE DISCHARGED DIRECTLY INTO A SEDIMENT FILTER AREA. THE FILTER SHOULD BE COMPOSED OF A FABRIC OR APPROVED MATERIAL. SEE THE DEWATERING DETAIL.
- ALL SEDIMENT BASINS WILL BE CLEANED WHEN THE CAPACITY HAS BEEN REDUCED BY 50%. A CLEAN OUT ELEVATION WILL BE IDENTIFIED ON THE PLAN AND A MARKER INSTALLED ON THE SITE.
- DURING AND AFTER CONSTRUCTION, THE APPLICANT WILL BE RESPONSIBLE FOR THE MAINTENANCE AND UPKEEP OF THE DRAINAGE STRUCTURES, VEGETATION COVER, AND ANY OTHER MEASURES DEEMED APPROPRIATE BY THE DISTRICT. SAID RESPONSIBILITY WILL END WHEN COMPLETED WORK IS APPROVED BY THE MORRIS COUNTY SOIL CONSERVATION DISTRICT.
- ALL TREES OUTSIDE THE DISTURBANCE LIMIT INDICATED ON THE SUBJECT PLAN OR THOSE TREES WITHIN THE DISTURBANCE AREA WHICH ARE DESIGNATED TO REMAIN AFTER CONSTRUCTION ARE TO BE PROTECTED WITH TREE PROTECTION DEVICES. SEE THE TREE PROTECTION DETAIL.
- THE MORRIS COUNTY SOIL CONSERVATION DISTRICT MAY REQUEST ADDITIONAL MEASURES TO MINIMIZE ON SITE OR OFF SITE EROSION PROBLEMS DURING CONSTRUCTION.
- THE MORRIS COUNTY SOIL CONSERVATION DISTRICT MUST BE NOTIFIED, IN WRITING, AT LEAST 48 HOURS PRIOR TO ANY LAND DISTURBANCE, AND A PRE-CONSTRUCTION MEETING HELD.
- CONTRACTOR TO SET UP A MEETING WITH THE INSPECTOR FOR PERIODIC INSPECTIONS OF THE TEMPORARY SEDIMENT BASIN PRIOR TO AND DURING ITS CONSTRUCTION.
- TOPSOIL STOCKPILE PROTECTION
 - APPLY GROUND LIMESTONE AT A RATE OF 90 LBS PER 1000 SQ. FT.
 - APPLY FERTILIZER (10-20-10) AT A RATE OF 11 LBS. PER 1000 SQ. FT.
 - APPLY PERENNIAL RYEGRASS SEED AT 1 LB. PER 1000 SQ. FT.
 - MULCH STOCKPILE WITH STRAW OR HAY AT A RATE OF 90 LBS. PER 1000 SQ. FT.
 - APPLY A LIQUID MULCH BINDER OR TACK TO STRAW OR HAY MULCH.
 - PROPERTY ENTRENCH A SILT FENCE AT THE BOTTOM OF THE STOCKPILE.
- TEMPORARY STABILIZATION SPECIFICATIONS
 - APPLY GROUND LIMESTONE AT A RATE OF 90 LBS PER 1000 SQ. FT.
 - APPLY FERTILIZER (10-20-10) AT A RATE OF 11 LBS. PER 1000 SQ. FT.
 - APPLY PERENNIAL RYEGRASS SEED AT 1 LB. PER 1000 SQ. FT.
 - MULCH DISTURBED SOIL WITH STRAW OR HAY AT A RATE OF 90 LBS. PER 1000 SQ. FT.
 - APPLY A LIQUID MULCH BINDER OR TACK TO STRAW OR HAY MULCH.
- PERMANENT STABILIZATION SPECIFICATIONS
 - APPLY TOPSOIL TO A DEPTH OF 5 INCHES (UNSETTLED).
 - APPLY GROUND LIMESTONE AT A RATE OF 90 LBS PER 1000 SQ. FT. AND WORK FOUR INCHES INTO SOIL.
 - APPLY FERTILIZER (10-20-10) AT A RATE OF 11 LBS. PER 1000 SQ. FT.
 - APPLY HARD FESCUE SEED AT 2.7 LBS. PER 1000 SQ. FT. AND CREEPING RED FESCUE SEED AT 0.7 LBS PER 1000 SQ. FT. AND PERENNIAL RYEGRASS SEED AT 0.25 LBS PER 1000 SQ. FT.
 - MULCH STOCKPILE WITH STRAW OR HAY AT A RATE OF 90 LBS. PER 1000 SQ. FT.
 - APPLY A LIQUID MULCH BINDER OR TACK TO STRAW OR HAY MULCH.

* NOTE:
48 HOURS PRIOR TO ANY SOIL DISTURBANCE, NOTICE IN WRITING, SHALL BE GIVEN TO THE MORRIS COUNTY SOIL CONSERVATION DISTRICT AND A PRE-CONSTRUCTION MEETING HELD.

CHATHAM TOWNSHIP - SOIL EROSION AND SEDIMENT CONTROL NOTES

- ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ON THIS PLAN WILL BE CONSTRUCTED IN ACCORDANCE WITH THE "NEW JERSEY STANDARDS FOR SOIL EROSION AND SEDIMENT CONTROL," (REVISED 1999) AND WILL BE IN PLACE PRIOR TO ANY SOIL DISTURBANCE OR IN THEIR PROPER SEQUENCE AND MAINTAINED UNTIL PERMANENT PROTECTION IS ESTABLISHED.
- CHATHAM TOWNSHIP WILL BE NOTIFIED 72 HOURS PRIOR TO ANY LAND DISTURBANCE.
- DURING AND AFTER CONSTRUCTION, THE OWNER WILL BE RESPONSIBLE FOR THE MAINTENANCE AND UPKEEP OF THE DRAINAGE STRUCTURES, VEGETATIVE COVER AND ANY OTHER MEASURES DEEMED APPROPRIATE BY THE TOWNSHIP.
- A CRUSHED STONE VEHICLE WHEEL CLEANING BLANKET WILL BE INSTALLED WHEREVER A CONSTRUCTION ACCESS ROAD INTERSECTS ANY PAVED ROADWAY. SAID BLANKET WILL BE COMPOSED OF 2½" CRUSHED STONE, WILL BE AT LEAST 50 FEET LONG AND THE WIDTH OF THE EXIT ROADWAY OR DRIVEWAY, AND WILL BE PROPERLY MAINTAINED.
- ALL PAVED ROADWAYS MUST BE KEPT CLEAN AT ALL TIMES.
- ALL NEW ROADWAYS AND DRIVEWAYS WILL BE TREATED WITH A SUITABLE SUBBASE UPON ESTABLISHMENT OF FINAL GRADE ELEVATIONS.
- DISTURBED AREAS SHALL BE MAINTAINED IN A ROUGH GRADED CONDITION AND TEMPORARILY SEEDED AND MULCHED UNTIL PROPER WEATHER CONDITIONS EXIST FOR THE ESTABLISHMENT OF PERMANENT VEGETATIVE COVER.
- ALL SOIL STOCKPILED FOR A PERIOD OF GREATER THAN 30 DAYS WILL BE TEMPORARILY SEEDED AND MULCHED.
- STOCKPILES SHALL NOT BE LOCATED WITHIN 50 FEET OF A FLOODPLAIN, SLOPE, DRAINAGE FACILITY OR ROADWAY. ALL STOCKPILE BASES SHALL BE PROTECTED BY A HAY BALE BARRIER OR SEDIMENT FENCE.
- IMMEDIATELY FOLLOWING INITIAL DISTURBANCE OR ROUGH GRADING, ALL CRITICAL AREAS SUBJECT TO EROSION WILL RECEIVE A TEMPORARY SEEDING IN COMBINATION WITH STRAW MULCH OR SUITABLE EQUAL, AT A 2 TON/ACRE RATIO RATE, ACCORDING TO STATE STANDARDS.
- TEMPORARY STABILIZATION - ANY DISTURBED AREA THAT WILL BE LEFT EXPOSED FOR MORE THAN THIRTY (30) DAYS AND NOT SUBJECT TO CONSTRUCTION ACTIVITIES SHALL IMMEDIATELY BE STABILIZED UPON DISTURBANCE BY APPLYING THE FOLLOWING:
 - GROUND LIMESTONE AT A RATE OF 90 POUNDS PER 1,000 SQUARE FEET.
 - FERTILIZER AT A RATE OF 14 POUNDS PER 1,000 SQUARE FEET USING A 10-20-10 ANALYSIS OR AN EQUIVALENT WORKED INTO THE SOIL A MINIMUM OF 4".
 - SEED SHALL BE ANNUAL RYEGRASS APPLIED AT NOT LESS THAN 1 POUND PER 1,000 SQUARE FEET.
 - MULCH ALL NEWLY SEEDED AREA WITH UNROTTED SALT HAY OR SMALL GRAIN STRAW AT A RATE OF 90 POUNDS PER 1,000 SQUARE FEET ACCORDING TO THE NJ STANDARD. MULCH SHALL NOT BE GROUND INTO SHORT PIECES AND IN NO CASE SHALL MORE THAN 5 DAYS ELAPSE BETWEEN SEEDING AND MULCHING.
 - MULCH SHALL BE ANCHORED WITH A LIQUID MULCH BINDER APPLIED AT A RATE OF 1 GAL/1,000 SF. OR BY APPROVED METHODS (I.E. PEG AND TWINE, MULCH NETTING).
- BETWEEN OCTOBER 1 AND MARCH 1 AND WHEN THE SEASON PROHIBITS TEMPORARY SEEDING OR WHEN DISTURBED AREAS ARE SCHEDULED FOR IMMEDIATE LANDSCAPING, APPLYING THE AFORE MENTIONED ITEMS "D)" AND "E)" WILL BE ADEQUATE.
- SEEDING DATES: THE FOLLOWING ARE RECOMMENDED SEEDING DATES FOR THE ESTABLISHMENT OF TEMPORARY OR PERMANENT VEGETATION.
 - SPRING: (MARCH 1 - MAY 15)
 - FALL: (AUGUST 15 - OCTOBER 1)
- PERMANENT VEGETATIVE COVER IS TO BE ESTABLISHED ON EXPOSED AREAS WITHIN 10 DAYS AFTER FINAL GRADING. MULCH IS TO BE USED FOR PROTECTION UNTIL FINAL VEGETATION IS ESTABLISHED.
- PERMANENT SEEDING AND STABILIZATION TO BE IN ACCORDANCE WITH THE STANDARDS FOR PERMANENT VEGETATIVE COVER - ALL EXPOSED SURFACES WILL BE TREATED WITH 4" TOPSOIL PRIOR TO FINAL STABILIZATION AND THE FOLLOWING ITEMS APPLIED AT THE DESIGNATED RATES:
 - LIME SHALL BE APPLIED AT 90 POUNDS PER 1,000 SQUARE FEET CONSISTING OF GROUND LIMESTONE INCORPORATED INTO THE TOP 4" OF TOPSOIL.
 - FERTILIZER SHALL BE 14 POUNDS PER 1,000 SQUARE FEET 10-20-10 INCORPORATED INTO THE TOP 4" OF TOPSOIL.
 - SEED SHALL BE 25 POUNDS PER ACRE OF KENTUCKY BLUEGRASS, 15 POUNDS PER ACRE OF RED FESCUE, SPREADING FESCUE AT 15 POUNDS PER ACRE, AND 10 POUNDS PER ACRE OF PERENNIAL RYEGRASS.
 - IN SHADE AREAS INCREASE RED FESCUE 20 POUNDS PER ACRE AND DECREASE KENTUCKY BLUEGRASS 20 POUNDS PER ACRE.
 - MULCH ALL NEWLY SEEDED AREA WITH UNROTTED SALT HAY OR SMALL GRAIN STRAW AT A RATE OF 90 POUNDS PER 1,000 SQUARE FEET ACCORDING TO THE NJ STANDARD. MULCH SHALL NOT BE GROUND INTO SHORT PIECES AND IN NO CASE SHALL MORE THAN 5 DAYS ELAPSE BETWEEN SEEDING AND MULCHING.
 - MULCH SHALL BE ANCHORED WITH A LIQUID MULCH BINDER APPLIED AT A RATE OF 1 GAL./1,000 SQUARE FEET OR BY APPROVED METHODS (I.E. PEG AND TWINE, MULCH NETTING).

- MAXIMUM SIDE SLOPES OF ALL EXPOSED SURFACES SHALL NOT EXCEED 3:1 UNLESS OTHERWISE APPROVED BY THE TOWNSHIP.
- THE SITE SHALL, AT ALL TIMES, BE GRADED AND MAINTAINED SUCH THAT ALL STORM WATER RUN - OFF IS DIVERTED TO SOIL EROSION AND SEDIMENT CONTROL FACILITIES.
- ALL DEWATERING OPERATIONS MUST DISCHARGE DIRECTLY INTO A SEDIMENT FILTER AREA. THE SEDIMENT FILTER SHOULD BE COMPOSED OF SUITABLE FILTER FABRIC FILTER.
- ALL SEDIMENTATION STRUCTURES WILL BE INSPECTED AND MAINTAINED ON A REGULAR BASIS.
- ALL STORM DRAIN INLETS SHALL BE PROTECTED WITH GRAVEL FILTERS TO PREVENT ENTRY OF SEDIMENT CARRIED BY RUNOFF WATER UNTIL VEGETATION AND/OR PAVING IS ESTABLISHED.
- ALL STORM DRAINAGE OUTLETS WILL BE STABILIZED AS REQUIRED BEFORE THE DISCHARGE POINTS BECOME OPERATIONAL.
- ALL TREES TO REMAIN AFTER CONSTRUCTION ARE TO BE PROTECTED WITH TREE PROTECTION DEVICES OR SEDIMENT BARRIERS.
- THE TOWNSHIP MAY REQUEST ADDITIONAL MEASURE TO MINIMIZE ON OR OFF SITE EROSION PROBLEMS DURING CONSTRUCTION.
- SEQUENCE OF CONSTRUCTION
 - INSTALL VEHICLE WHEEL CLEANING BLANKET AND INLET PROTECTION.
 - INSTALL SILT FENCE
 - CLEAR SITE
 - INSTALL TIMBER MATTING
 - PROVIDE TEMPORARY STABILIZATION IF REQUIRED.
 - PROVIDE PERMANENT STABILIZATION
 - REMOVE TEMPORARY SILT FENCE, INLET PROTECTION AND OTHER SOIL EROSION CONTROLS
- A COPY OF THE SOIL EROSION AND SEDIMENT CONTROL PLAN MUST BE ON-SITE AT ALL TIMES AND MADE AVAILABLE TO A TOWNSHIP REPRESENTATIVE DURING INSPECTION.

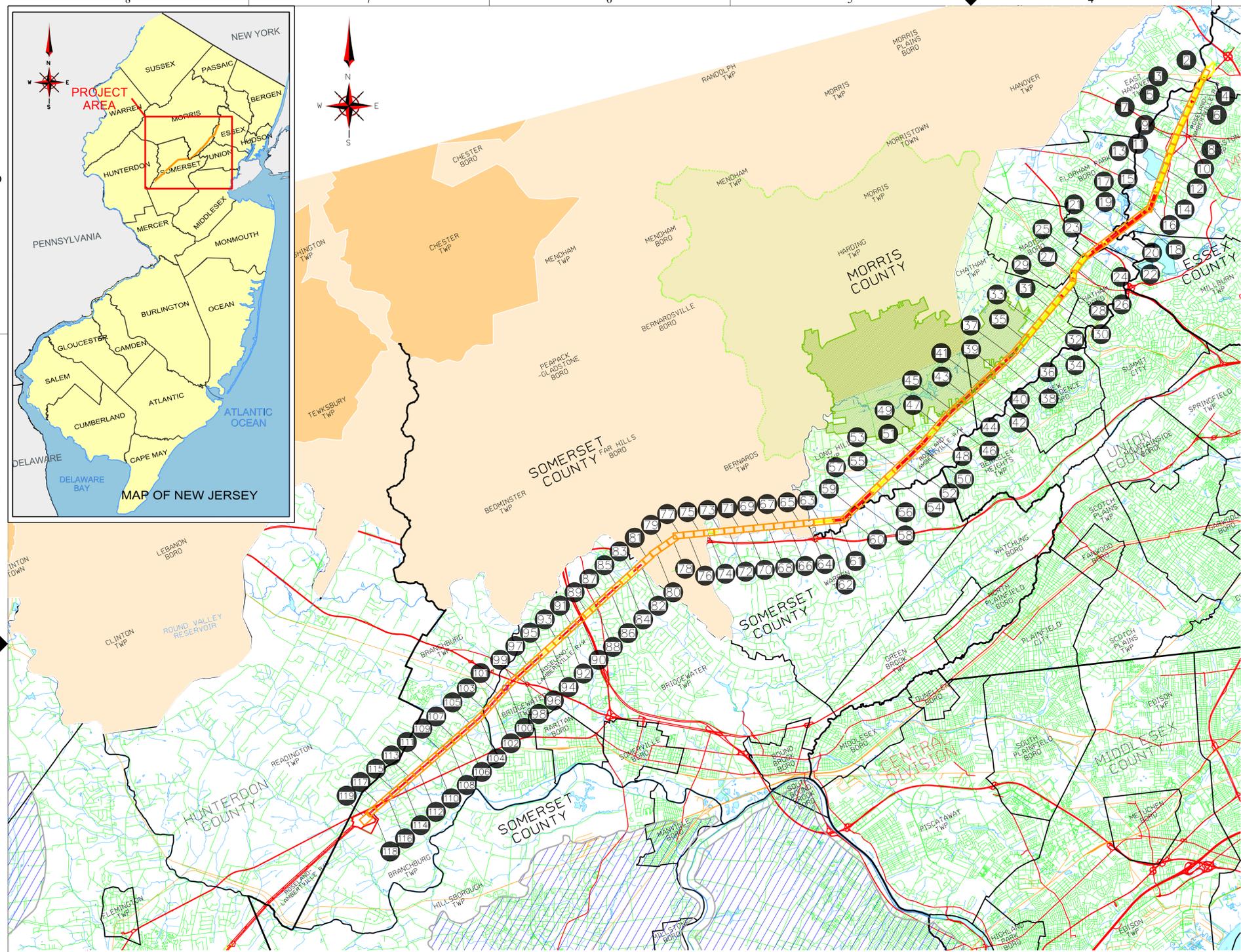
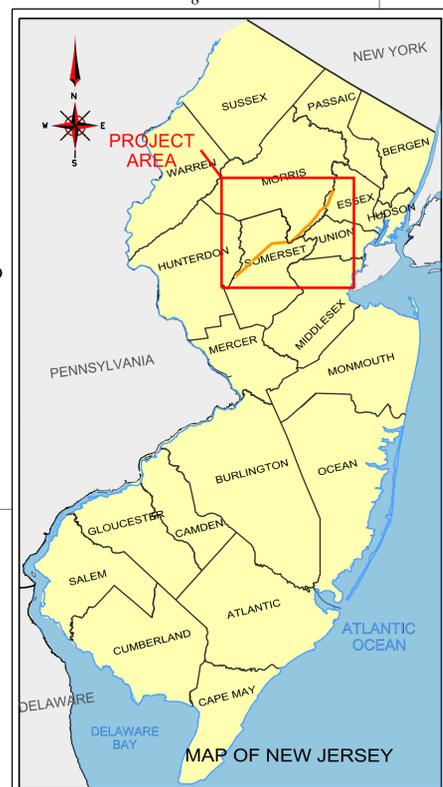
SEQUENCE OF CONSTRUCTION (PER ACCESS ROAD):

APPROXIMATE PROJECT DURATION: 09/01/2019 TO 04/01/2022

- THE LOCAL SOIL CONSERVATION DISTRICT IS TO BE NOTIFIED 48 HOURS PRIOR TO ACCESS ROAD SURVEY STAKE OUT TO INVESTIGATE LOCAL DRAINAGE CONDITIONS THAT MAY NEED TO BE ADDRESSED WITH ADDITIONAL EROSION CONTROL MEASURES.
- CONTRACTOR SHALL EXAMINE THE EXISTING ROADWAY AT THE ACCESS ROAD ENTRANCE. INSTALL INLET PROTECTION AT EACH INLET WITHIN 100 FEET OF THE ENTRANCE.
- INSTALL STABILIZED CONSTRUCTION DRIVEWAYS ALONG LOCAL ROADS AND STABILIZED CONSTRUCTION ENTRANCES ON COUNTY ROADS, STATE AND INTERSTATE HIGHWAYS. ENTRANCES ON INTERSTATE HIGHWAYS SHALL ONLY BE CONSTRUCTED/USED AS NEEDED FOR STRINGING OPERATIONS.
- INSTALL SEDIMENT BARRIER ALONG THE ACCESS ROAD ROUTE. CONSTRUCT ACCESS ROAD USING MATTING OR STONE, AS INDICATED ON THE PLAN SHEETS, INCLUDING WATERBREAKS AND TEMPORARY BRIDGES.
- CONSTRUCT WORK, LAYDOWN AND STRINGING AREAS USING MATTING OR STONE, AS INDICATED ON THE PLAN SHEETS. EACH AREA INCLUDES SEDIMENT BARRIER AND TEMPORARY CONCRETE WASHOUT FACILITY. STOCKPILES TO BE CONSTRUCTED AS NEEDED.
- CONSTRUCT TOWER FOUNDATION AND TOWER AND STRING CABLES. DEMOLISH EXISTING TOWERS AND CUT THE EXISTING FOUNDATIONS.
- REMOVE ACCESS ROAD AND WORK AREA MATERIALS.
- STABILIZE DISTURBED AREAS FOLLOWING PERMANENT VEGETATIVE STANDARDS.
- REMOVE REMAINING SEDIMENTATION CONTROLS ONCE PERMANENT VEGETATION IS ESTABLISHED.



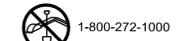
| | | | | | |
|--|--|-------------|----------|----------------|-------------------|
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG | | |
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG | | |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG | | |
| ZONE | REV | DESCRIPTION | DATE | APPROVED | |
| REVISIONS | | | | | |
| SUBJECT: RPV GENERAL NOTES MORRIS COUNTY SOIL EROSION AND SEDIMENT CONTROL | | | | | |
| SCALE | NTS | CHECKED | J. FINK | APPROVED/TITLE | J. WANG / MANAGER |
| DATE | 11/07/18 | DRAWN BY | J. GROSS | DISCIPLINE | CIVIL |
| LOCATION: | RPV | SIZE ARCH | D | DWG NO. | TO-737087 |
| | | | | REV | 1 |
| | | | | SHEET | 126 OF 127 |



| INDEX TO DRAWINGS | |
|-------------------|----------------------------------|
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| 1 | COVER SHEET / GENERAL NOTES |
| 2 | PLAN |
| 3 | PLAN |
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| 59 | PLAN - SOMERSET-UNION SCD |
| 60 | PLAN - MORRIS COUNTY SCD |
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| 124 | NOTES & DETAILS |
| 125 | NOTES - HUDSON-ESSEX-PASSAIC SCD |
| 126 | NOTES - MORRIS COUNTY SCD |
| 127 | NOTES - SOMERSET-UNION SCD |

- GENERAL NOTES:**
- EXISTING BACKGROUND INFORMATION AND SURVEY PROVIDED BY P&E&G.
 - ELEVATIONS, CONTOURS AND BENCHMARKS ARE BASED ON NAVD 1988 VERTICAL DATUM.
 - HORIZONTAL LOCATIONS ARE BASED ON NEW JERSEY STATE PLANE COORDINATE SYSTEM NAD 83 (2011).
 - CONTRACTOR(S) TO TAKE AND VERIFY ALL DIMENSIONS AND CONDITIONS OF THE WORK AND BE RESPONSIBLE FOR COORDINATION OF SAME. FIELD VERIFY ALL EXISTING CONDITIONS PRIOR TO START OF WORK.
 - EXISTING CONDITIONS SURVEY INFORMATION ON EXISTING UTILITIES AND STORM DRAINAGE SYSTEMS HAS BEEN COMPILED FROM AVAILABLE INFORMATION INCLUDING UTILITY COMPANY AND MUNICIPAL RECORD MAPS AND/OR FIELD SURVEY AND IS NOT GUARANTEED CORRECT OR COMPLETE. UTILITIES AND STORM DRAINAGE SYSTEMS ARE SHOWN TO ALERT THE CONTRACTOR TO THEIR PRESENCE AND THE CONTRACTOR IS SOLELY RESPONSIBLE FOR DETERMINING ACTUAL LOCATIONS AND ELEVATIONS OF ALL UTILITIES AND STORM DRAINAGE SYSTEMS INCLUDING SERVICES. PRIOR TO DEMOLITION OR CONSTRUCTION, THE CONTRACTOR SHALL CONTACT "DIGSAFE" PRIOR TO COMMENCEMENT OF WORK AT "811" OR 800-272-1000 AND VERIFY ALL UTILITY AND STORM DRAINAGE SYSTEM LOCATIONS.
 - CONTRACTOR SHALL TAKE CARE TO AVOID ANY DAMAGE TO UNDERGROUND GAS LINES DURING THE DURATION OF THE PROJECT. CONTRACTOR SHALL WORK WITH THE GAS COMPANIES TO CROSS OR WORK ON TOP OF GAS LINES THAT EXIST IN THE RIGHT-OF-WAY.
 - THE CONTRACTOR SHALL VERIFY ALL EXISTING SITE AND BUILDING CONDITIONS IN THE FIELD AND CONTACT THE OWNER AND ENGINEER IF THERE ARE ANY QUESTIONS AND/OR CONFLICTS REGARDING THE POST CONSTRUCTION STORMWATER MANAGEMENT PLANS (PCSM) AND/OR EXISTING FIELD CONDITIONS PRIOR TO CONSTRUCTION. REFER TO THE PROJECT SPECIFICATIONS MANUAL FOR ADDITIONAL INFORMATION. SHOULD ANY UNCHARTED OR INCORRECTLY CHARTED, EXISTING PIPING OR OTHER UTILITY BE UNCOVERED DURING EXCAVATION, INFORM THE OWNER AND CONSULT THE CIVIL ENGINEER IMMEDIATELY FOR DIRECTIONS BEFORE PROCEEDING FURTHER WITH WORK IN THIS AREA.
 - DO NOT INTERRUPT EXISTING SERVICING UTILITIES AND FACILITIES OCCUPIED AND USED BY THE OWNER OR OTHERS DURING OCCUPIED HOURS EXCEPT WHEN SUCH INTERRUPTIONS HAVE BEEN AUTHORIZED IN WRITING BY THE OWNER, THE LOCAL MUNICIPALITIES, THE UTILITY PROVIDER, AND ANY APPLICABLE REGULATORY AGENCY. INTERRUPTIONS SHALL ONLY OCCUR AFTER ACCEPTABLE TEMPORARY SERVICE HAS BEEN PROVIDED.
 - THE CONTRACTOR SHALL PROVIDE RECORD AS-BUILT DRAWINGS OF ALL CONSTRUCTION IN ACCORDANCE WITH OWNER AND REGULATORY AGENCY REQUIREMENTS (INCLUDING UNDERGROUND UTILITIES) TO THE OWNER AT THE END OF CONSTRUCTION.
 - WRITTEN DIMENSIONS HAVE PRECEDENCE OVER SCALED DIMENSIONS. THE CONTRACTOR SHALL USE CAUTION WHEN SCALING PLANS. IN CASE OF CONFLICT BETWEEN PLAN SET AND ANY OTHER DRAWING AND/OR SPECIFICATION, THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY FOR CLARIFICATION.
 - IF A CONFLICT ARISES BETWEEN PLANS, SPECIFICATIONS, AND/OR DETAILS, THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY FOR CLARIFICATION.
 - THE CONTRACTOR SHALL ABIDE BY ALL OSHA, FEDERAL, STATE, AND LOCAL REGULATIONS IN ALL INSTANCES AND WHEN OPERATING CRANES, BOOMS, HOISTS, ETC. IN CLOSE PROXIMITY TO OVERHEAD ELECTRIC LINES, IF CONTRACTOR MUST OPERATE EQUIPMENT CLOSE TO ELECTRIC LINES, CONTACT POWER COMPANY TO MAKE ARRANGEMENT FOR PROPER SAFEGUARDS. ANY UTILITY COMPANY FEES SHALL BE PAID FOR BY THE CONTRACTOR.
 - THE ENGINEER IS NOT RESPONSIBLE FOR SITE SAFETY MEASURES TO BE EMPLOYED DURING CONSTRUCTION. THE ENGINEER HAS NO CONTRACTUAL DUTY TO CONTROL THE SAFEST METHODS OR MEANS OF THE WORK, JOB SITE RESPONSIBILITIES, SUPERVISION OR TO SUPERVISE SAFETY AND DOES NOT VOLUNTARILY ASSUME ANY SUCH DUTY OR RESPONSIBILITY.
 - ALL NOTES AND DIMENSIONS DESIGNATED "TYPICAL" OR "TYP.:" APPLY TO ALL LIKE OR SIMILAR CONDITIONS THROUGHOUT THE PROJECT.
 - ALTERNATIVE METHODS AND PRODUCTS OTHER THAN THOSE SPECIFIED MAY BE USED IF SUBMITTED, REVIEWED, AND APPROVED BY THE OWNER, ENGINEER, AND APPROPRIATE REGULATORY AGENCY PRIOR TO CONSTRUCTION.
 - THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OF ALL PRODUCTS AND MATERIALS PER PLANS AND SPECIFICATIONS TO THE OWNER AND ENGINEER FOR REVIEW AND APPROVAL PRIOR TO ORDERING, FABRICATION, OR DELIVERY TO THE SITE. FOR EACH SUBMITTAL, ALLOW A MINIMUM OF 14 WORKING DAYS FOR REVIEW.
 - THE CONTRACTOR SHALL RESTORE ANY DRAINAGE STRUCTURE, PIPE, UTILITY, PAVEMENT, CURBS, SIDEWALKS, LANDSCAPED AREAS OR SIGNAGE AND OTHER INCIDENTAL DISTURBANCES AND DAMAGES DURING CONSTRUCTION TO THEIR ORIGINAL CONDITION OR BETTER, AS APPROVED BY THE OWNER, ENGINEER AND REGULATORY AGENCY.
 - NO CONSTRUCTION OR DEMOLITION SHALL BEGIN UNTIL APPROVAL OF THE FINAL PLANS IS GRANTED BY ALL GOVERNING AND REGULATORY AGENCIES.
 - DEMOLITION OF EXISTING CONDITIONS INCLUDING BUT NOT LIMITED TO STRUCTURES, PAVEMENT, FENCES, TREES, ETC. SHALL BE PER THE DIRECTION OF P&E&G AND SHALL BE IN ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL REGULATIONS.
 - ELECTRICAL SUBSTATION COMPONENTS, UNDERGROUND TRANSMISSION LINES, OVER HEAD TRANSMISSION LINES AND THEIR FOUNDATIONS DEPICTED HEREIN ARE FOR REFERENCE ONLY.
 - ANY CLEARED AND EXCAVATED MATERIALS WHICH ARE SUSPECTED OF BEING ENVIRONMENTALLY POLLUTED, CONTAMINATED, OR IMPACTED SHALL BE STOCKPILED ON-SITE ON TOP OF POLYETHYLENE SHEETING AND COVERED WITH POLYETHYLENE SHEETING. THE OWNER AND ENGINEER SHALL BE IMMEDIATELY INFORMED UPON ENCOUNTERING THIS MATERIAL. STORAGE, TESTING, TREATMENT, REMOVAL, AND DISPOSAL OF ENVIRONMENTALLY POLLUTED, CONTAMINATED, OR IMPACTED MATERIAL SHALL BE IN ACCORDANCE WITH APPLICABLE LOCAL, STATE, AND FEDERAL REGULATIONS.
 - CONTRACTOR SHALL TAKE PRECAUTIONS TO ENSURE NO DISTURBANCE BEYOND THE DEPICTED LIMIT OF DISTURBANCE.
 - NO CHANGES SHALL BE MADE TO THE SOIL EROSION AND SEDIMENT CONTROL PLANS WITHOUT APPROVAL OF THE COMPANY CONSTRUCTION MANAGER AND THE NEW JERSEY LICENSED PROFESSIONAL ENGINEER CHARGED WITH SUPERVISION OF THE CONSTRUCTION OF THE SOIL EROSION AND SEDIMENT CONTROL MEASURES. THE CHANGES SHALL BE DOCUMENTED IN AN AS-BUILT PLAN, WHICH SHALL BE SIGNED AND SEALED BY THE SUPERVISING ENGINEER.

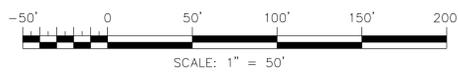
CALL BEFORE YOU DIG!
 NEW JERSEY LAW REQUIRES 3
 BUSINESS DAYS FOR A MARK OUT.
 BEGIN DIGGING WITHIN 10 BUSINESS DAYS.
 TICKET EXPIRES IN 45 BUSINESS DAYS.
 NEW JERSEY ONE CALL



CONTRACTORS ARE RESPONSIBLE FOR VERIFYING ALL EXISTING CONDITIONS PRIOR TO CONSTRUCTION. CONTRACTOR TO VERIFY LOCATION OF TIE-IN WITH EXISTING DRIVES, ROADS AND SITE GRADES, ETC. DISCREPANCIES TO BE REPORTED AND ANY ADJUSTMENTS COORDINATED AND APPROVED BY THE ENGINEER. ALL REQUIRED PERMITS SHALL BE OBTAINED PRIOR TO ANY CONSTRUCTION ACTIVITIES.

ABBREVIATIONS:-
 SCD - SOIL CONSERVATION DISTRICT
 DGA - DENSE GRADED AGGREGATE
 HAPS - HIGH ACID PRODUCING SOILS
 HMA - HOT MIX ASPHALT
 LOD - LIMIT OF DISTURBANCE
 PCSM - POST CONSTRUCTION STORMWATER MANAGEMENT
 NJDOT SPEC - NEW JERSEY DEPARTMENT OF TRANSPORTATION LATEST ISSUE

| CALCULATED DISTURBANCE | |
|---|-------|
| HUDSON-ESSEX-PASSAIC SOIL CONSERVATION DISTRICT | |
| SQ. FEET | ACRES |
| 2,059,903 | 47.3 |
| MORRIS COUNTY SOIL CONSERVATION DISTRICT | |
| SQ. FEET | ACRES |
| 3,785,948 | 86.9 |
| SOMERSET-UNION SOIL CONSERVATION DISTRICT | |
| SQ. FEET | ACRES |
| 4,169,967 | 95.7 |



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|---|--|------------|---------|
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG |
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG |

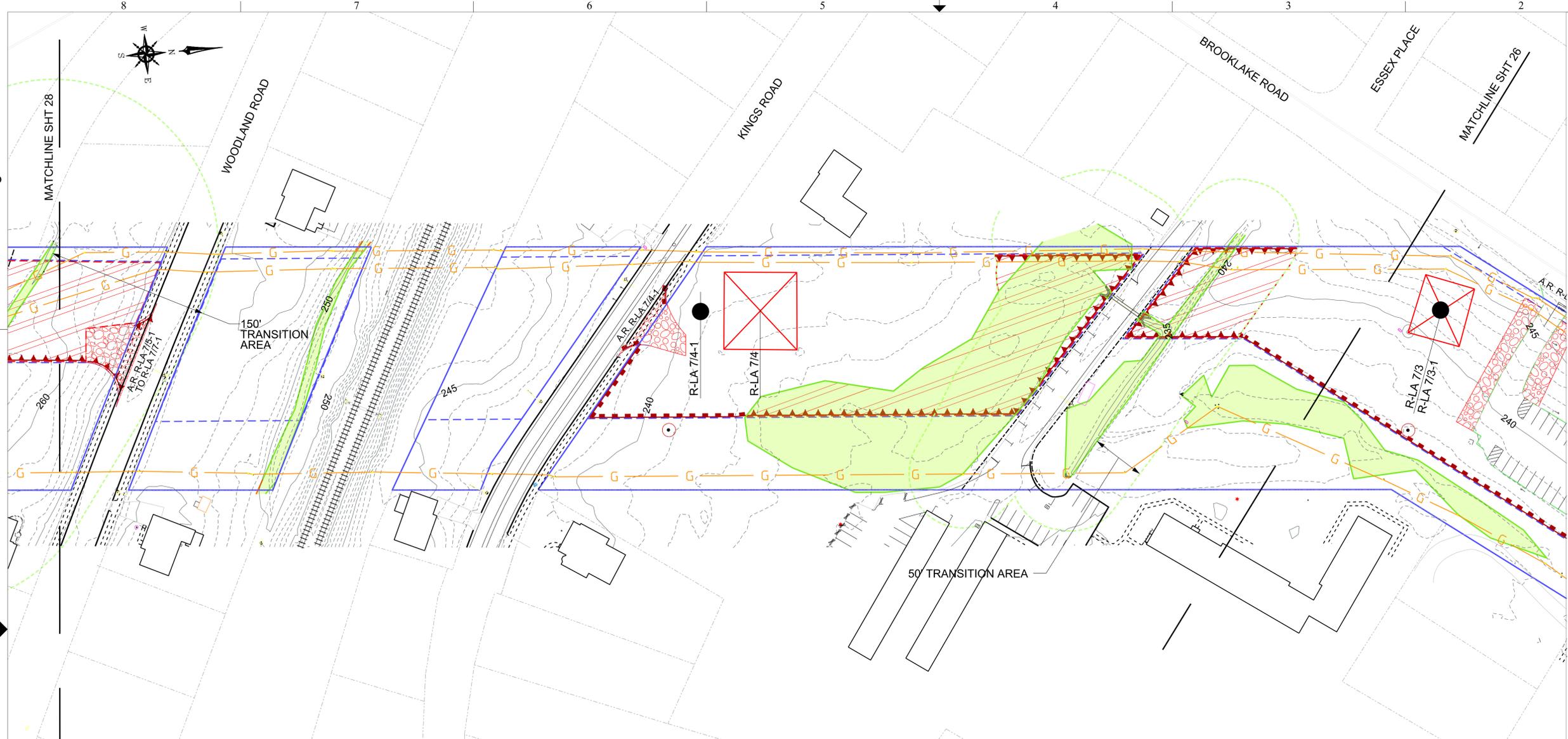
| ZONE | REV | DESCRIPTION | DATE | APPROVED |
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ASSET MANAGEMENT
ELECTRIC TRANSMISSION ENGINEERING

| | | | | | |
|---------|--|----------|----------|----------------|-------------------|
| SUBJECT | RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | | | |
| SCALE | 1" = 50' | CHECKED | J. FINK | APPROVED/TITLE | J. WANG / MANAGER |
| DATE | 11/07/18 | DRAWN BY | J. GROSS | DISCIPLINE | CIVIL |

LOCATION: RPV SIZE ARCH D DWG NO. TO-737087 REV 1 SHEET 1 OF 127

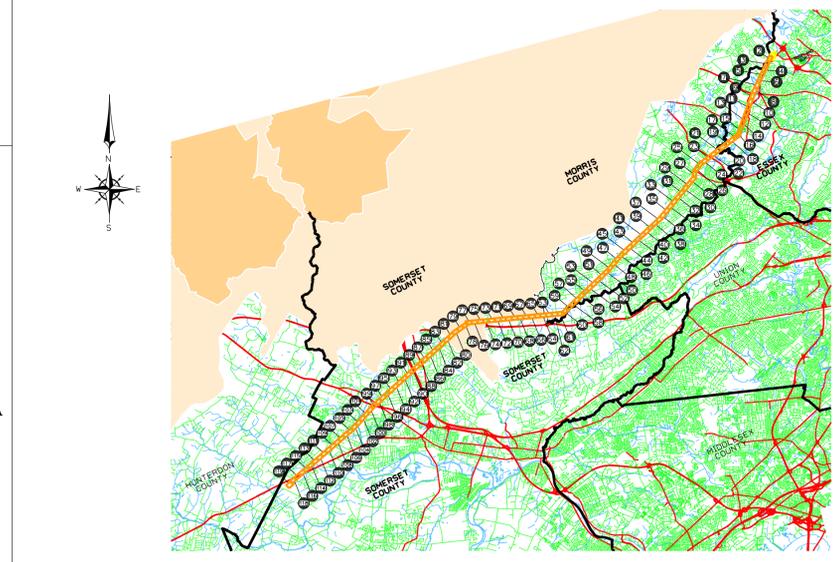
JOSHUA FINK 3 2 1



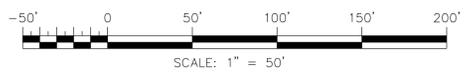
- NOTES:**
1. SEE DWG. TO-737087 SHEET 120 FOR TYPICAL ROAD SECTION.
 2. TYPE 1 SEDIMENT BARRIER SHALL BE REMOVED ONCE ACCESS ROAD IS STABILIZED.
 3. TYPE 2 AND 3 SEDIMENT BARRIER SHALL REMAIN IN PLACE UNTIL CONSTRUCTION ACTIVITIES ARE COMPLETE AND SITE IS STABILIZED.
 4. AREA UNDER TOWERS SHALL NOT BE ROCKED.
 5. WORK PADS AND PULL SITES SHALL HAVE AN AVERAGE GRADE OF 2% ACROSS PAD.
 6. SEDIMENT BARRIER SHALL BE PLACED INSIDE OF LIMITS OF DISTURBANCE INDICATED ON THE DRAWINGS.
 7. ANY DISTURBED AREAS THAT REQUIRE RESTORATION WILL BE TOP - SOILED PRIOR TO SEEDING FOR PERMANENT VEGETATION STABILIZATION.
 8. FOR GENERAL NOTES AND DETAILS SEE DRAWING TO-737087 SHEET 120 THRU SHEET 127.
 9. FOR SOIL CONSERVATION DISTRICT (SCD) REQUIREMENTS SEE DRAWING TO-737087 SHEET 125 THRU 127. FOR HUDSON-ESSEX-PASSAIC COUNTY SCD SEE SHT 125. FOR MORRIS COUNTY SCD SEE SHT 126. FOR SOMERSET-UNION COUNTY SCD SEE SHT 127.

- LEGEND:**
- MATTING
 - WORKPAD/PULL SITE LIMIT OF DISTURBANCE (LOD)
 - GUARD STRUCTURE LIMIT OF DISTURBANCE (LOD)
 - PERMANENT ROAD
 - TRACKING PAD
 - WATERBODIES
 - WETLANDS
 - PSEG RIGHT OF WAY (R.O.W.)
 - PSEG RIGHT OF WAY (R.O.W.) FEE EASEMENTS
 - PROPOSED ACCESS ROAD (LOD)
 - LAYDOWN AREA (NO SOIL DISTURBANCE)
 - EXISTING ACCESS ROAD (PAVED)
 - EXISTING DIRT / GRAVEL ROAD
 - WETLAND BUFFER
 - TYPE 1 SEDIMENT BARRIER (SILT FENCE/18" DIA. SILT SOCK)
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 - GAS PIPELINE (PIPE DIA.)
 - PETROLEUM PIPE
 - FIBER OPTIC CABLE
 - WATER PIPELINE
 - SANITARY/STORM SEWER
 - EXISTING FENCE
 - EXISTING RAILROAD
 - MINOR CONTOUR
 - MAJOR CONTOUR
 - PROPOSED TEMPORARY GRADING CONTOUR
 - FLOODWAY
 - FLOOD HAZARD
 - A.R. R/LA XXX-1 ACCESS ROAD NUMBER
 - NEW H-FRAME
 - NEW MONOPOLE FOUNDATION
 - EXISTING POLE
 - EXISTING LATTICE STRUCTURE
 - PROPOSED PERMANENT GRADING
 - STATE ENDANGERED WILLOW-LEAF ASTER

PLAN VIEW
SCALE: 1" = 50'



KEY PLAN



N.J. PROFESSIONAL ENGINEER
LIC. NO. 24GE04930400
C.O.A. NO. 24GE04813000

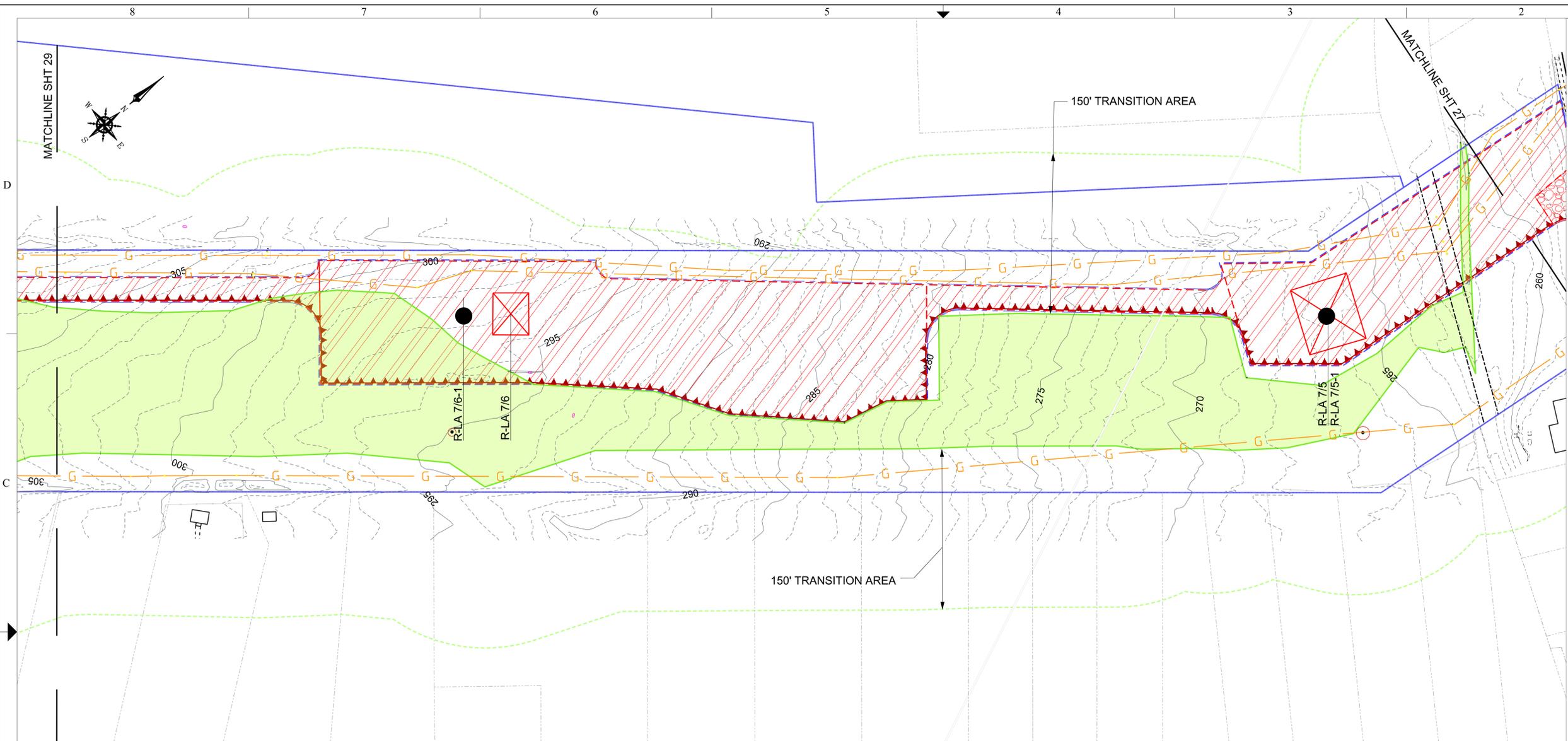
4/6/2020

JOSHUA FINK

| NO. | DESCRIPTION | DATE | APPROVED |
|-----|--|------------|----------|
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG |
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG |

| REVISIONS | | | |
|-----------|-----|-------------|------|
| ZONE | REV | DESCRIPTION | DATE |
| | | | |

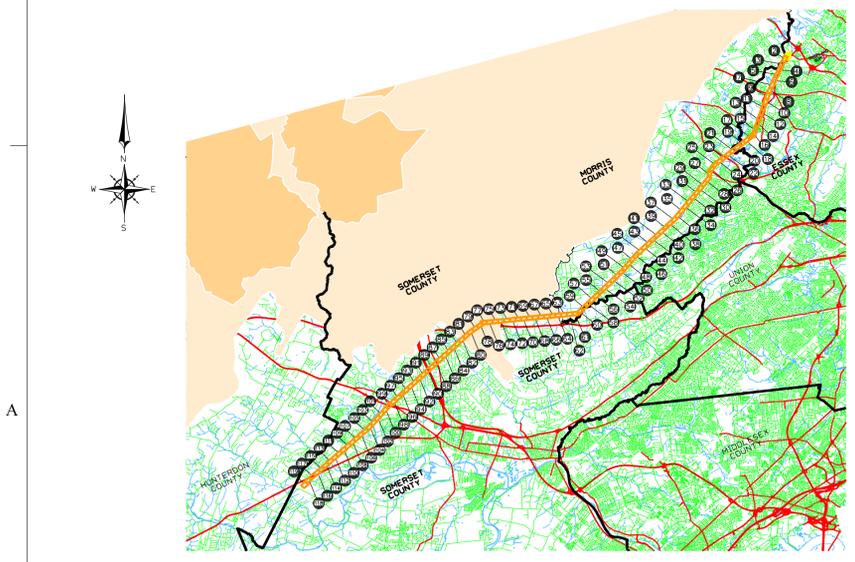
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| | | SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | |
| SCALE: 1" = 50' | CHECKED: J. FINK | APPROVED/TITLE: J. WANG / MANAGER | |
| DATE: 11/07/18 | DRAWN BY: J. GROSS | DISCIPLINE: CIVIL | |
| LOCATION: RPV | SIZE ARCH D | DWG. NO. TO-737087 | REV 1 SHEET 27 OF 127 |



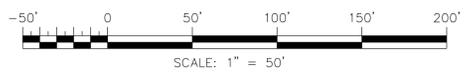
PLAN VIEW
SCALE: 1" = 50'

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 4. AREA UNDER TOWERS SHALL NOT BE ROCKED.
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- LEGEND:
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 - MAJOR CONTOUR
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 - FLOODWAY
 - FLOOD HAZARD
 - ACCESS ROAD NUMBER
 - NEW H-FRAME
 - NEW MONOPOLE FOUNDATION
 - EXISTING POLE
 - EXISTING LATTICE STRUCTURE
 - PROPOSED PERMANENT GRADING
 - STATE ENDANGERED WILLOW-LEAF ASTER



KEY PLAN

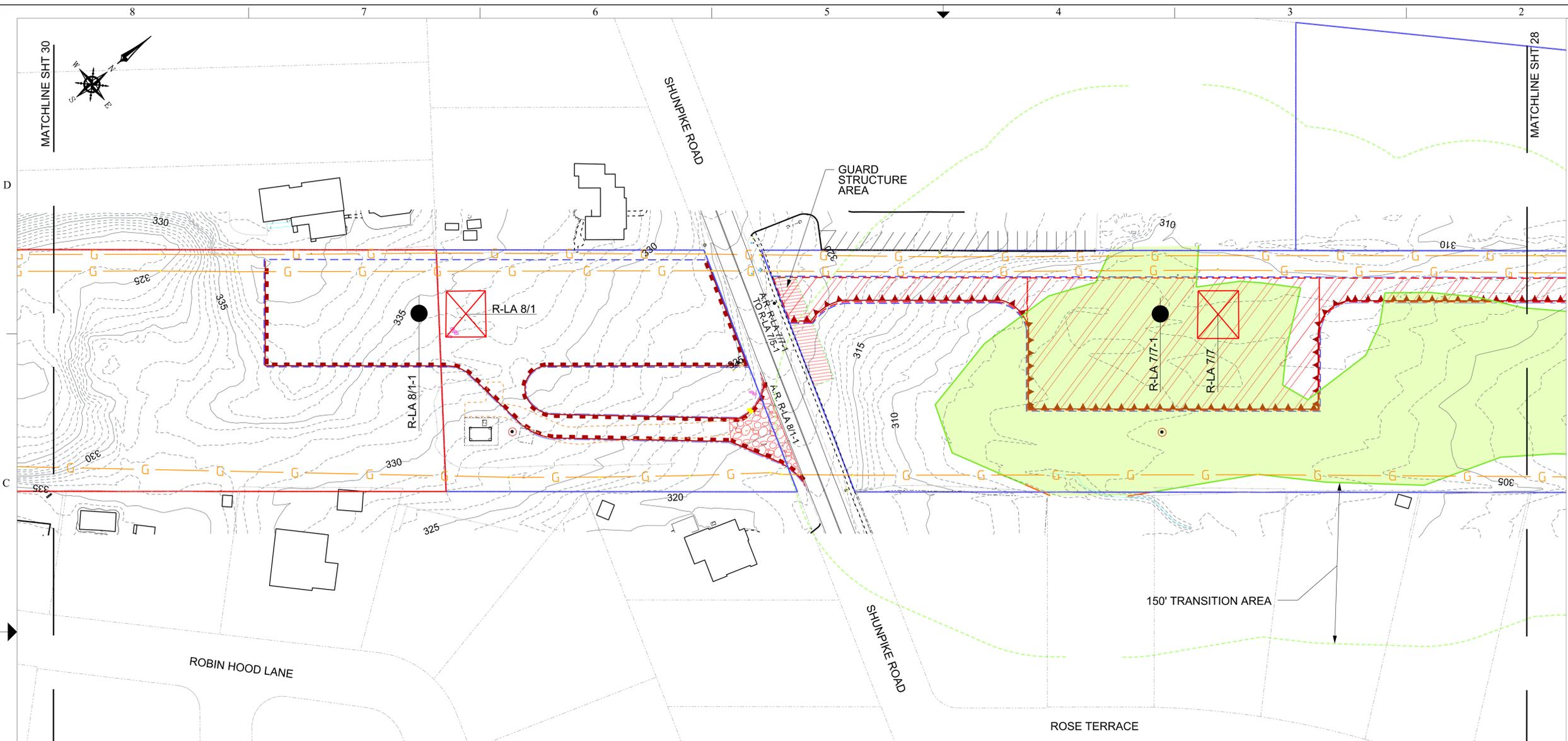


N.J. PROFESSIONAL ENGINEER
LIC. NO. 24GE04930400
C.O.A. NO. 24GE04813000

4/6/2020

JOSHUA FINK

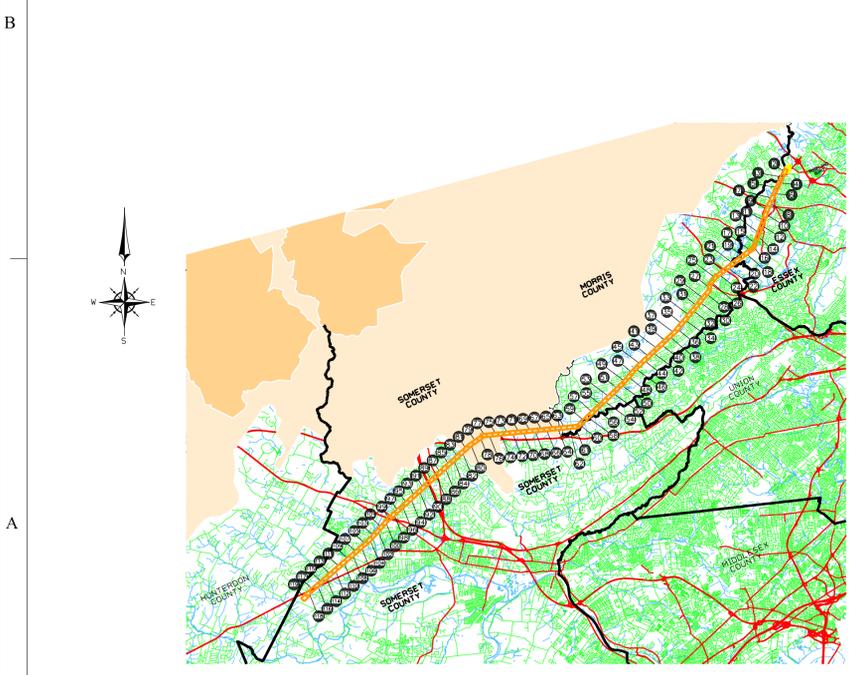
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG | | |
|---|--|-------------|----------|----------------|-------------------|
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG | | |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG | | |
| ZONE | REV | DESCRIPTION | DATE | APPROVED | |
| REVISIONS | | | | | |
| SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | | | | |
| SCALE | 1" = 50' | CHECKED | J. FINK | APPROVED/TITLE | J. WANG / MANAGER |
| DATE | 11/07/18 | DRAWN BY | J. GROSS | DISCIPLINE | CIVIL |
| LOCATION: | RPV | SIZE ARCH | D | DWG. NO. | TO-737087 |
| | | | | REV | 1 |
| | | | | SHEET | 28 OF 127 |



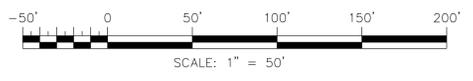
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KEY PLAN



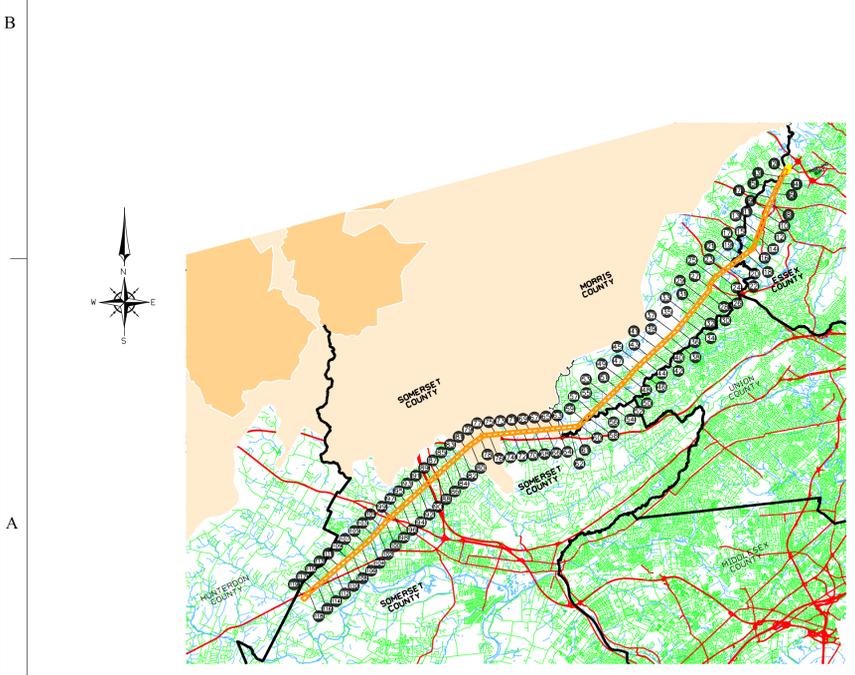
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|---|--|---|----------|----------------|-------------------|--|---|--|--|-------|----------|---------|---------|----------------|-------------------|------|----------|----------|----------|------------|-------|-----------|-----|-----------|---|----------|-----------|--|--|--|--|-----|---|--|--|--|--|-------|-----------|
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| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| ZONE | REV | DESCRIPTION | DATE | APPROVED | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <table border="1"> <tr> <td colspan="2"> </td> <td colspan="3"> SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS </td> </tr> <tr> <td>SCALE</td> <td>1" = 50'</td> <td>CHECKED</td> <td>J. FINK</td> <td>APPROVED/TITLE</td> <td>J. WANG / MANAGER</td> </tr> <tr> <td>DATE</td> <td>11/07/18</td> <td>DRAWN BY</td> <td>J. GROSS</td> <td>DISCIPLINE</td> <td>CIVIL</td> </tr> <tr> <td>LOCATION:</td> <td>RPV</td> <td>SIZE ARCH</td> <td>D</td> <td>DWG. NO.</td> <td>TO-737087</td> </tr> <tr> <td></td> <td></td> <td></td> <td></td> <td>REV</td> <td>1</td> </tr> <tr> <td></td> <td></td> <td></td> <td></td> <td>SHEET</td> <td>29 OF 127</td> </tr> </table> | | | | | | | SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | | SCALE | 1" = 50' | CHECKED | J. FINK | APPROVED/TITLE | J. WANG / MANAGER | DATE | 11/07/18 | DRAWN BY | J. GROSS | DISCIPLINE | CIVIL | LOCATION: | RPV | SIZE ARCH | D | DWG. NO. | TO-737087 | | | | | REV | 1 | | | | | SHEET | 29 OF 127 |
| | | SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| SCALE | 1" = 50' | CHECKED | J. FINK | APPROVED/TITLE | J. WANG / MANAGER | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| DATE | 11/07/18 | DRAWN BY | J. GROSS | DISCIPLINE | CIVIL | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| LOCATION: | RPV | SIZE ARCH | D | DWG. NO. | TO-737087 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | REV | 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | SHEET | 29 OF 127 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |



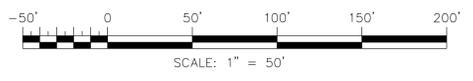
PLAN VIEW
SCALE: 1" = 50'

- NOTES:
- SEE DWG. TO-737087 SHEET 120 FOR TYPICAL ROAD SECTION.
 - TYPE 1 SEDIMENT BARRIER SHALL BE REMOVED ONCE ACCESS ROAD IS STABILIZED.
 - TYPE 2 AND 3 SEDIMENT BARRIER SHALL REMAIN IN PLACE UNTIL CONSTRUCTION ACTIVITIES ARE COMPLETE AND SITE IS STABILIZED.
 - AREA UNDER TOWERS SHALL NOT BE ROCKED.
 - WORK PADS AND PULL SITES SHALL HAVE AN AVERAGE GRADE OF 2% ACROSS PAD.
 - SEDIMENT BARRIER SHALL BE PLACED INSIDE OF LIMITS OF DISTURBANCE INDICATED ON THE DRAWINGS.
 - ANY DISTURBED AREAS THAT REQUIRE RESTORATION WILL BE TOP-SOILED PRIOR TO SEEDING FOR PERMANENT VEGETATION STABILIZATION.
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 - FOR SOIL CONSERVATION DISTRICT (SCD) REQUIREMENTS SEE DRAWING TO-737087 SHEET 125 THRU 127. FOR HUDSON-ESSEX-PASSAIC COUNTY SCD SEE SHT 125. FOR MORRIS COUNTY SCD SEE SHT 126. FOR SOMERSET-UNION COUNTY SCD SEE SHT 127.

- LEGEND:
- MATTING
 - WORKPAD/PULL SITE LIMIT OF DISTURBANCE (LOD)
 - GUARD STRUCTURE LIMIT OF DISTURBANCE (LOD)
 - PERMANENT ROAD
 - TRACKING PAD
 - WATERBODIES
 - WETLANDS
 - PSEG RIGHT OF WAY (R.O.W.)
 - PSEG RIGHT OF WAY (R.O.W.) FEE EASEMENTS
 - PROPOSED ACCESS ROAD (LOD)
 - LAYDOWN AREA (NO SOIL DISTURBANCE)
 - EXISTING ACCESS ROAD (PAVED)
 - EXISTING DIRT / GRAVEL ROAD
 - WETLAND BUFFER
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 - GAS PIPELINE (PIPE DIA.)
 - PETROLEUM PIPE
 - FIBER OPTIC CABLE
 - WATER PIPELINE
 - SANITARY/STORM SEWER
 - EXISTING FENCE
 - EXISTING RAILROAD
 - MINOR CONTOUR
 - MAJOR CONTOUR
 - PROPOSED TEMPORARY GRADING CONTOUR
 - FLOODWAY
 - FLOOD HAZARD
 - ACCESS ROAD NUMBER
 - NEW H-FRAME
 - NEW MONOPOLE FOUNDATION
 - EXISTING POLE
 - EXISTING LATTICE STRUCTURE
 - PROPOSED PERMANENT GRADING
 - STATE ENDANGERED WILLOW-LEAF ASTER



KEY PLAN



N.J. PROFESSIONAL ENGINEER
LIC. NO. 24GE04930400
C.O.A. NO. 24G104813000

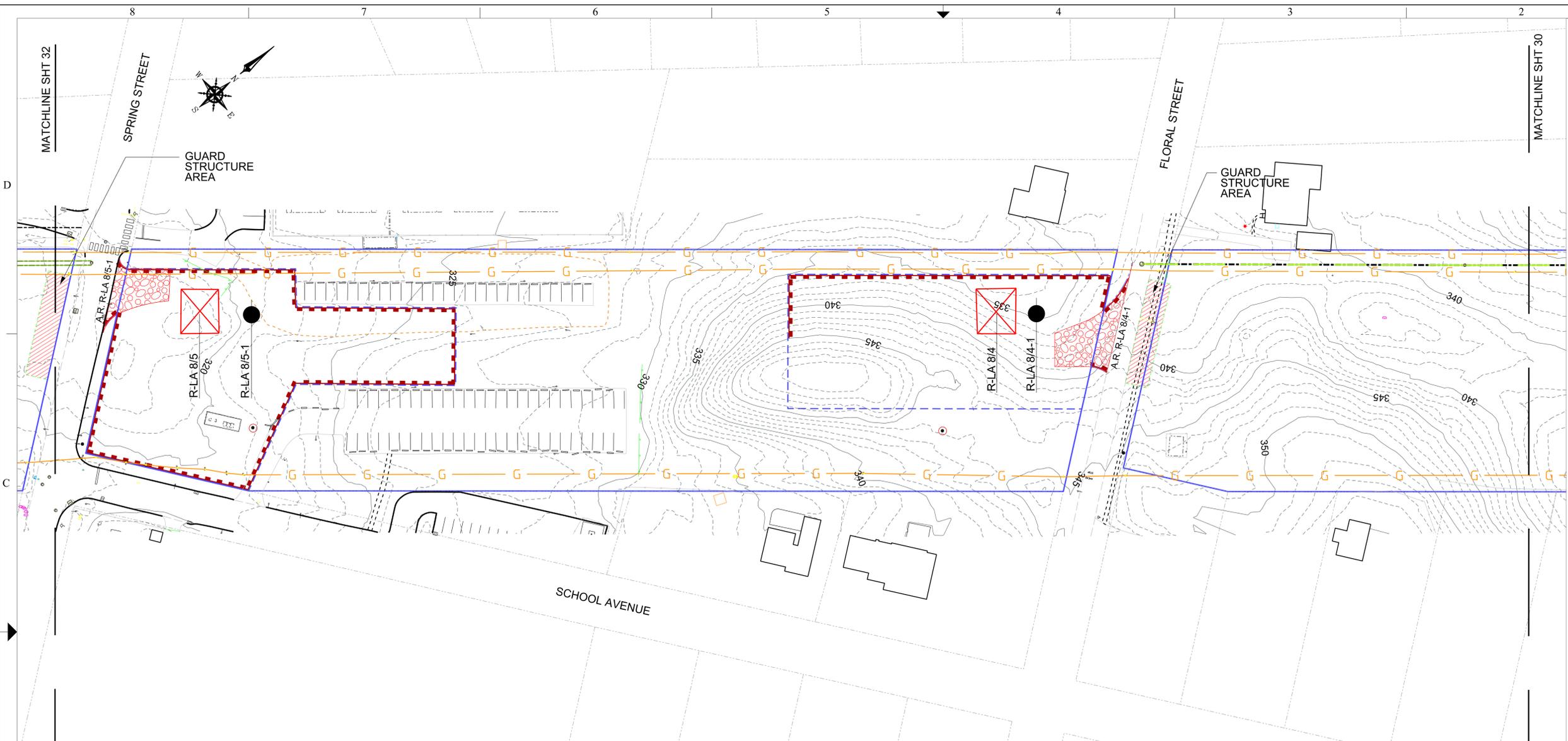
4/6/2020

JOSHUA FINK

| ZONE | REV | DESCRIPTION | DATE | APPROVED |
|------|-----|--|------------|----------|
| 2 | | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG |
| 1 | | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG |
| 0 | | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG |

| REVISIONS | | | |
|----------------|-------------------|------------|----------|
| SCALE | 1" = 50' | CHECKED | J. FINK |
| DATE | 11/07/18 | DRAWN BY | J. GROSS |
| APPROVED/TITLE | J. WANG / MANAGER | DISCIPLINE | CIVIL |

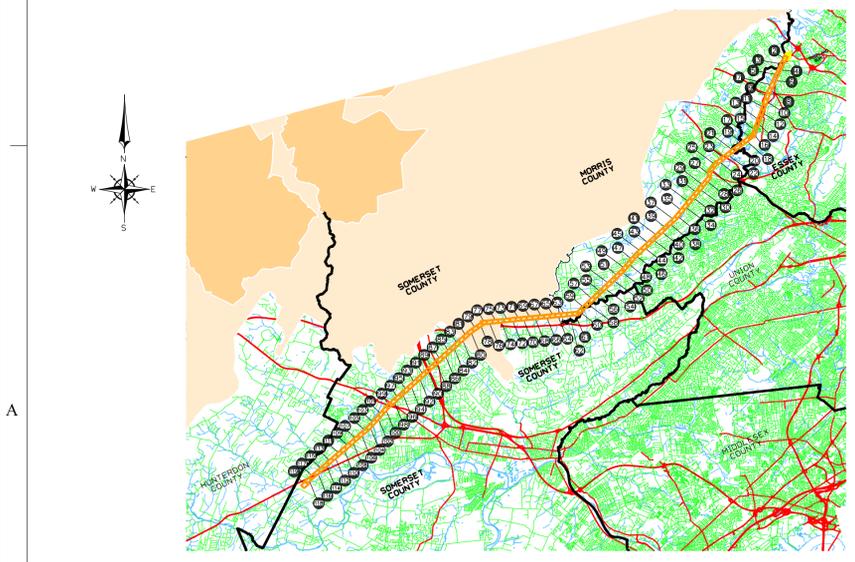
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|-----------|-----|-----------|---|----------|-----------|-----|---|-------|-----------|
| LOCATION: | RPV | SIZE ARCH | D | DWG. NO. | TO-737087 | REV | 1 | SHEET | 30 OF 127 |
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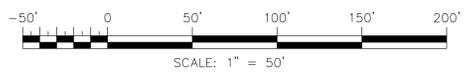
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- LEGEND:**
- MATting
 - WORKPAD/PULL SITE LIMIT OF DISTURBANCE (LOD)
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 - MINOR CONTOUR
 - MAJOR CONTOUR
 - PROPOSED TEMPORARY GRADING CONTOUR
 - FLOODWAY
 - FLOOD HAZARD
 - ACCESS ROAD NUMBER
 - NEW H-FRAME
 - NEW MONOPOLE FOUNDATION
 - EXISTING POLE
 - EXISTING LATTICE STRUCTURE
 - PROPOSED PERMANENT GRADING
 - STATE ENDANGERED WILLOW-LEAF ASTER

PLAN VIEW
SCALE: 1" = 50'



KEY PLAN



N.J. PROFESSIONAL ENGINEER
LIC. NO. 24GE04930400
C.O.A. NO. 24GI04813000

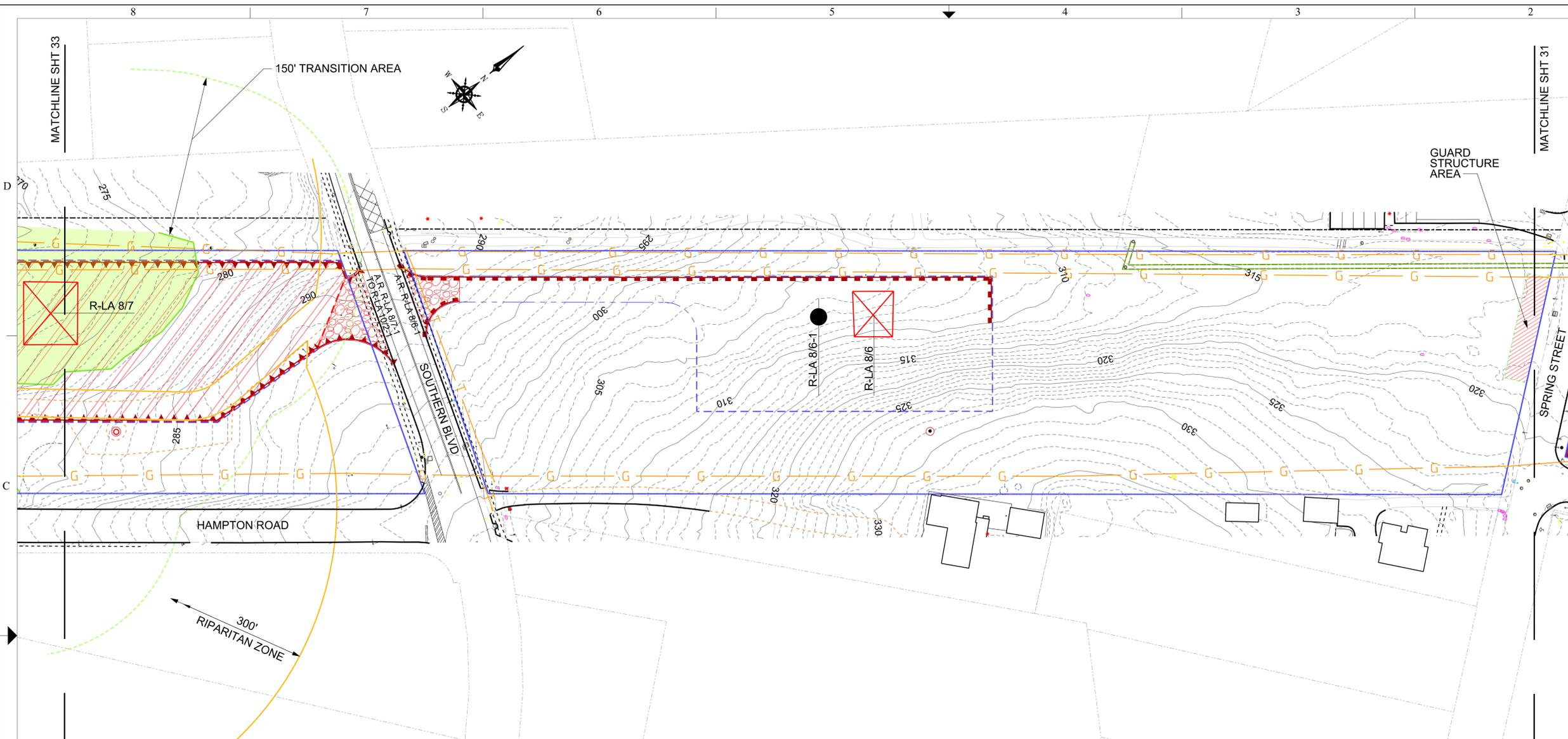
4/6/2020

JOSHUA FINK

| ZONE | REV | DESCRIPTION | DATE | APPROVED |
|------|-----|--|------------|----------|
| 2 | | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG |
| 1 | | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG |
| 0 | | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG |

| REVISIONS | | | |
|----------------|-------------------|------------|----------|
| SCALE | 1" = 50' | CHECKED | J. FINK |
| DATE | 11/07/18 | DRAWN BY | J. GROSS |
| APPROVED/TITLE | J. WANG / MANAGER | DISCIPLINE | CIVIL |

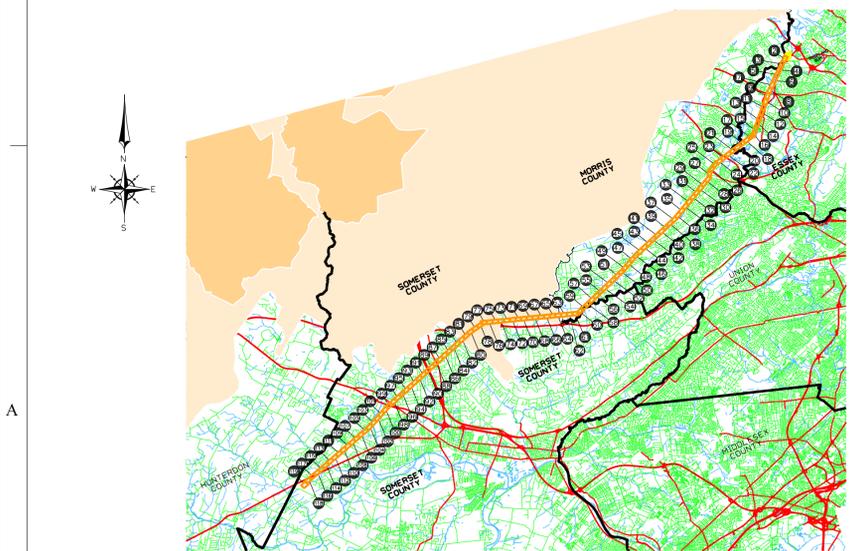
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|-----------|-----|-----------|---|----------|-----------|-----|---|-------|-----------|
| LOCATION: | RPV | SIZE ARCH | D | DWG. NO. | TO-737087 | REV | 1 | SHEET | 31 OF 127 |
|-----------|-----|-----------|---|----------|-----------|-----|---|-------|-----------|



- NOTES:**
1. SEE DWG. TO-737087 SHEET 120 FOR TYPICAL ROAD SECTION.
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- LEGEND:**
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 - MAJOR CONTOUR
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 - FLOODWAY
 - FLOOD HAZARD
 - A.R. R-LA XXX-1 ACCESS ROAD NUMBER
 - NEW H-FRAME
 - NEW MONOPOLE FOUNDATION
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 - PROPOSED PERMANENT GRADING
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PLAN VIEW
SCALE: 1" = 50'



KEY PLAN

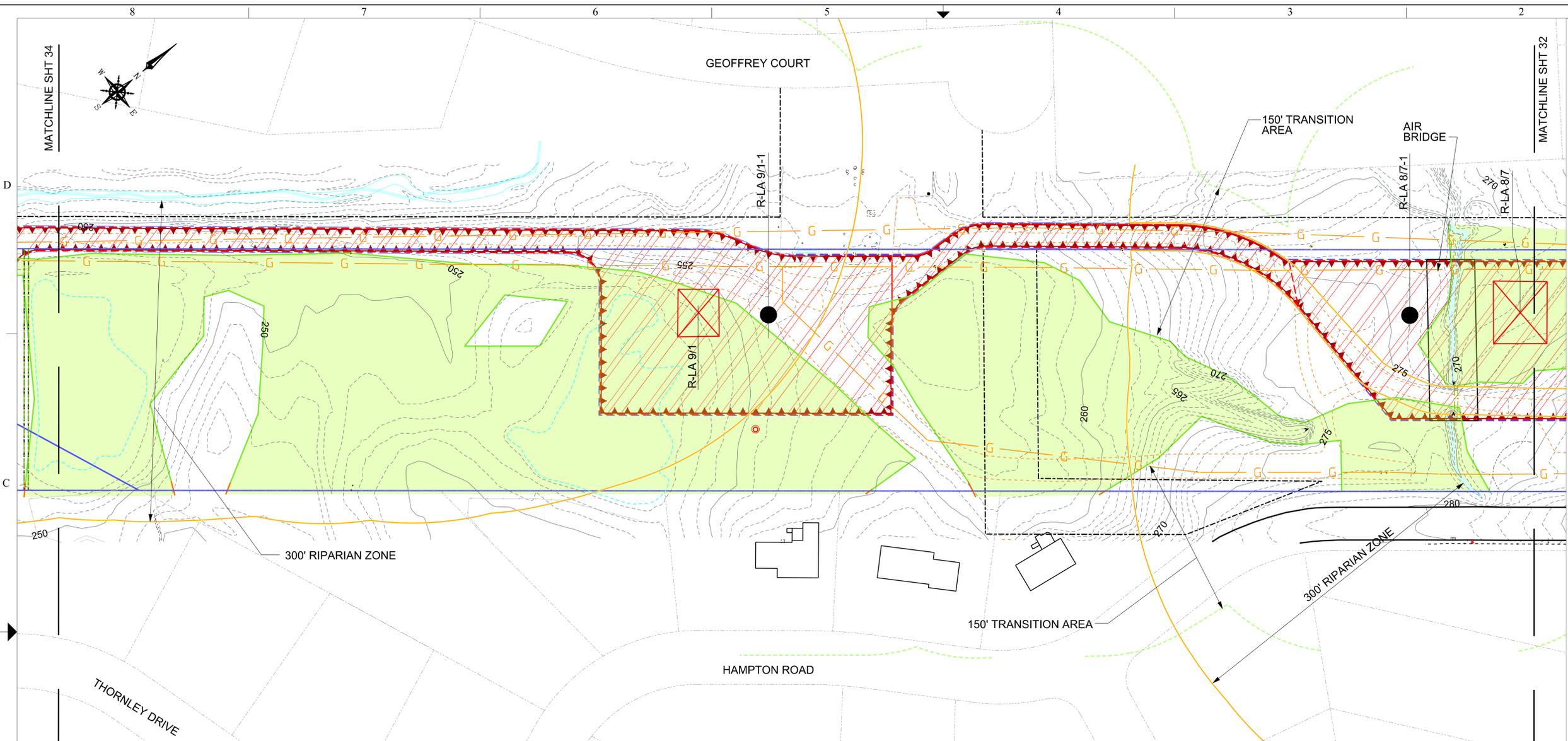


N.J. PROFESSIONAL ENGINEER
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C.O.A. NO. 24GE04813000

4/6/2020

JOSHUA FINK

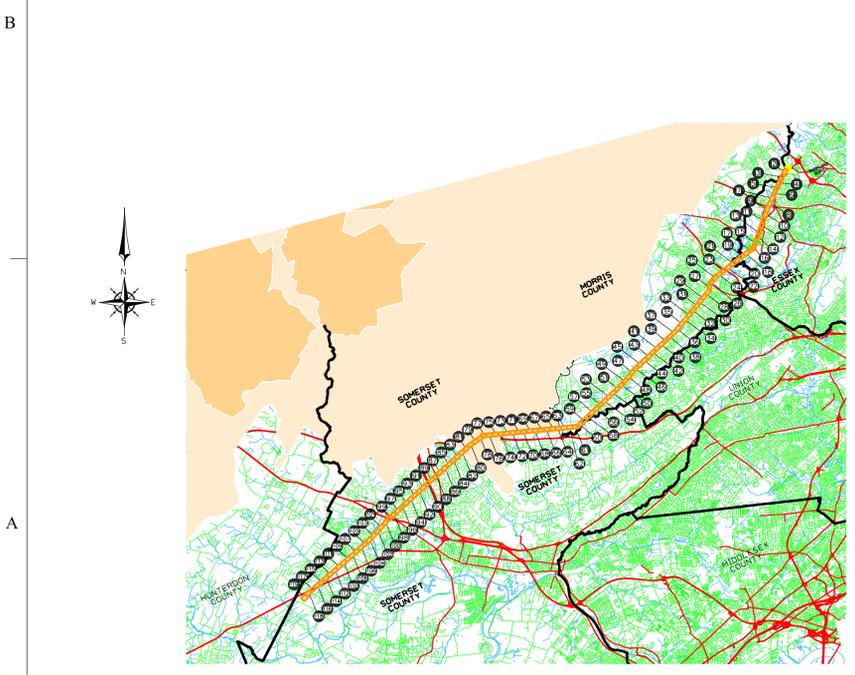
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG | |
|---|--|--------------------|-----------------------------------|-------------------------|
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG | |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG | |
| ZONE | REV | DESCRIPTION | DATE | APPROVED |
| REVISIONS | | | | |
| SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | | | |
| SCALE: 1" = 50' | | CHECKED: J. FINK | APPROVED/TITLE: J. WANG / MANAGER | |
| DATE: 11/07/18 | | DRAWN BY: J. GROSS | DISCIPLINE: CIVIL | |
| LOCATION: RPV | | SIZE ARCH: D | DWG. NO.: TO-737087 | REV: 1 SHEET: 32 OF 127 |



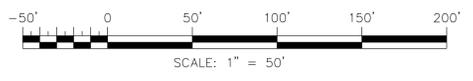
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- LEGEND:**
- MATTING
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 - FLOODWAY
 - FLOOD HAZARD
 - ACCESS ROAD NUMBER
 - NEW H-FRAME
 - NEW MONOPOLE FOUNDATION
 - EXISTING POLE
 - EXISTING LATTICE STRUCTURE
 - PROPOSED PERMANENT GRADING
 - STATE ENDANGERED WILLOW-LEAF ASTER

PLAN VIEW
SCALE: 1" = 50'



KEY PLAN

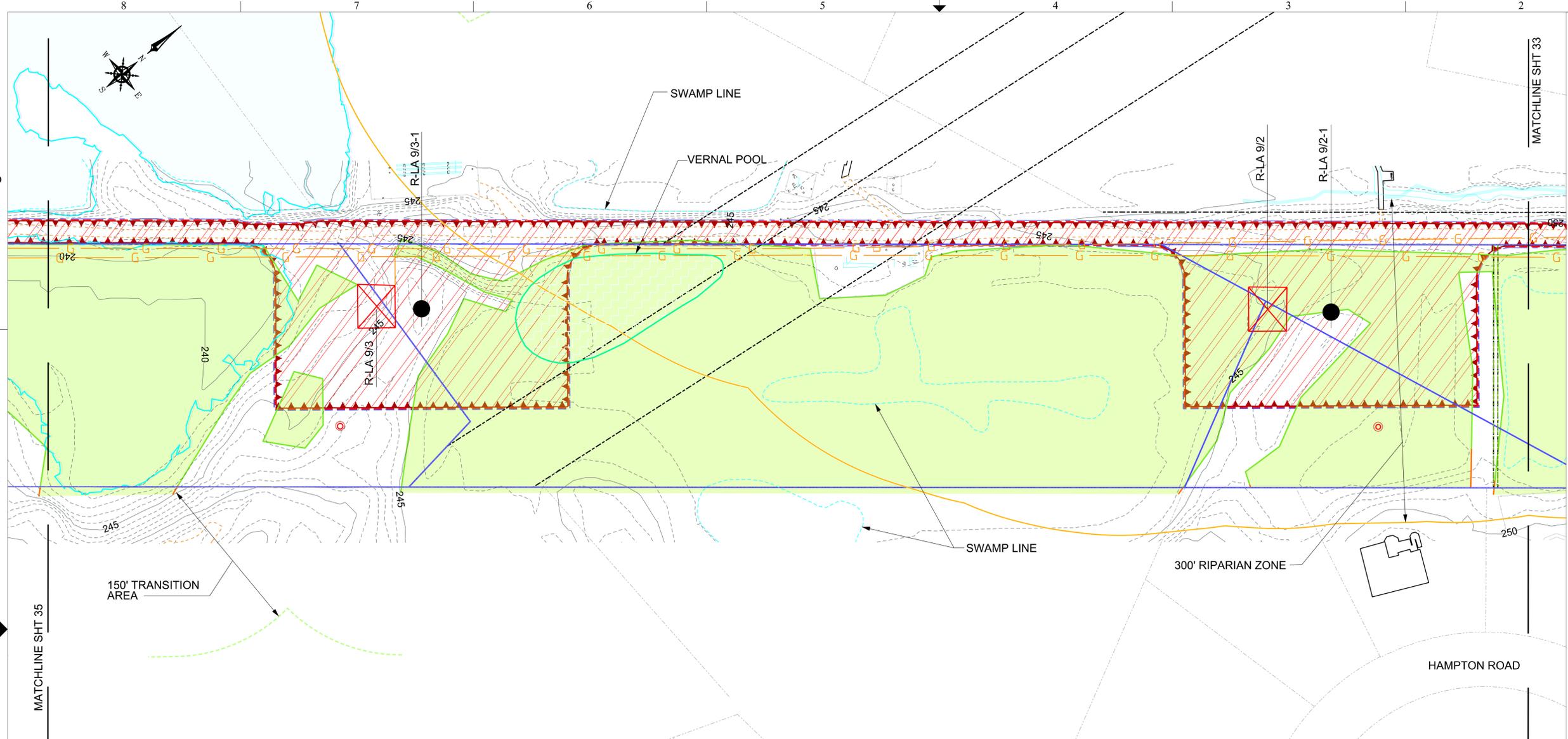


N.J. PROFESSIONAL ENGINEER
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JOSHUA FINK

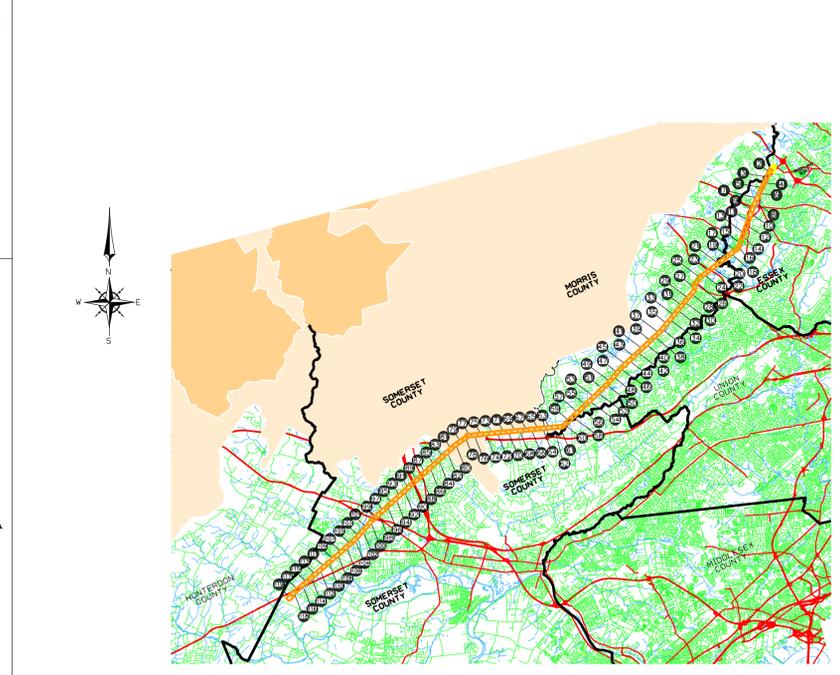
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG | |
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| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG | |
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| ZONE | REV | DESCRIPTION | DATE | APPROVED |
| REVISIONS | | | | |
| | | SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | |
| SCALE: 1" = 50' | | CHECKED: J. FINK | APPROVED/TITLE: J. WANG / MANAGER | |
| DATE: 11/07/18 | | DRAWN BY: J. GROSS | DISCIPLINE: CIVIL | |
| LOCATION: RPV | | SIZE ARCH: D | DWG. NO.: TO-737087 | REV: 1 SHEET: 33 OF 127 |



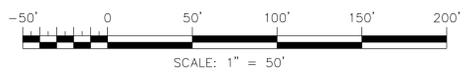
PLAN VIEW
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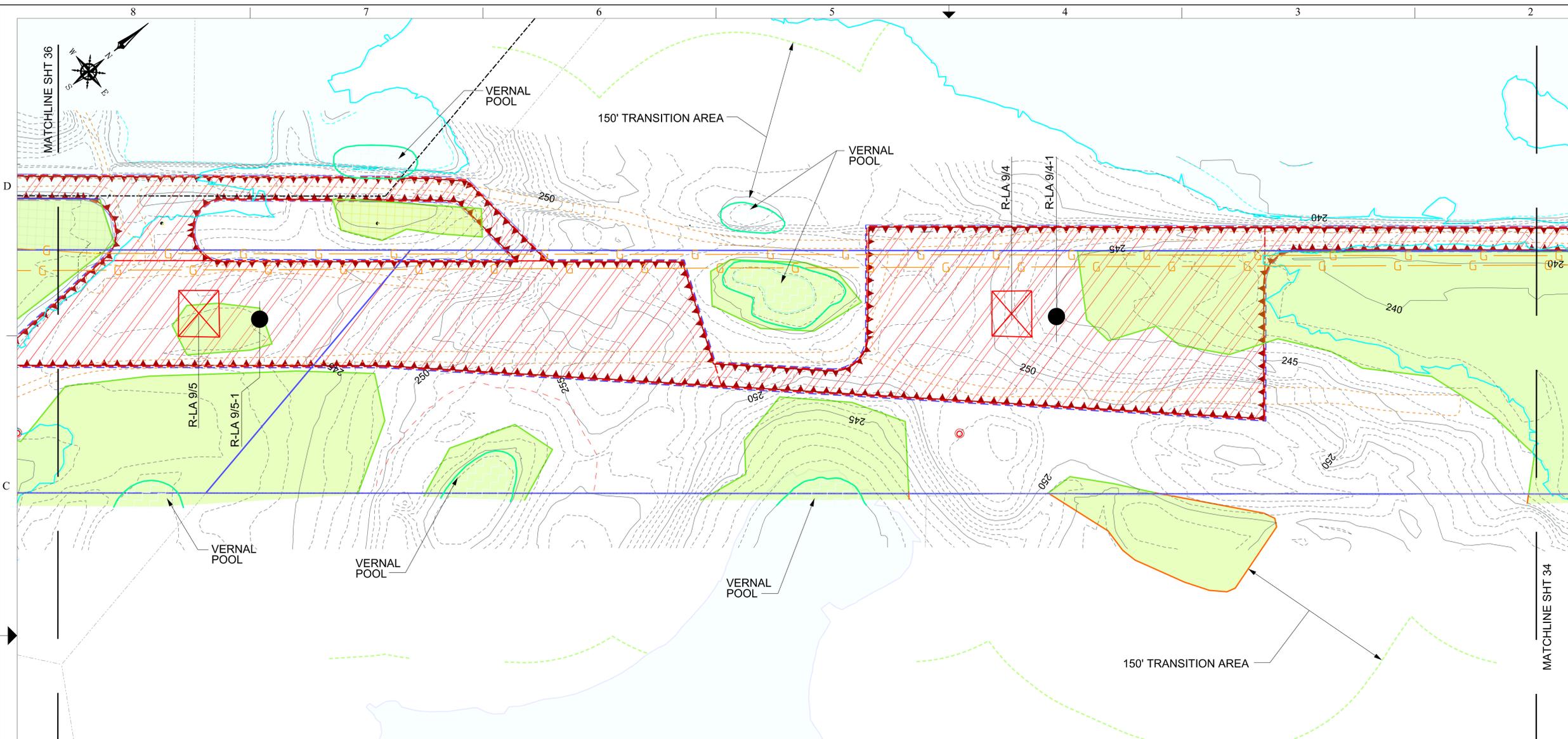
KEY PLAN



| ZONE | REV | DESCRIPTION | DATE | APPROVED |
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| 2 | | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG |
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| REVISIONS | | | |
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| 1 | 11/07/18 | J. GROSS | ISSUED FOR CONSTRUCTION |

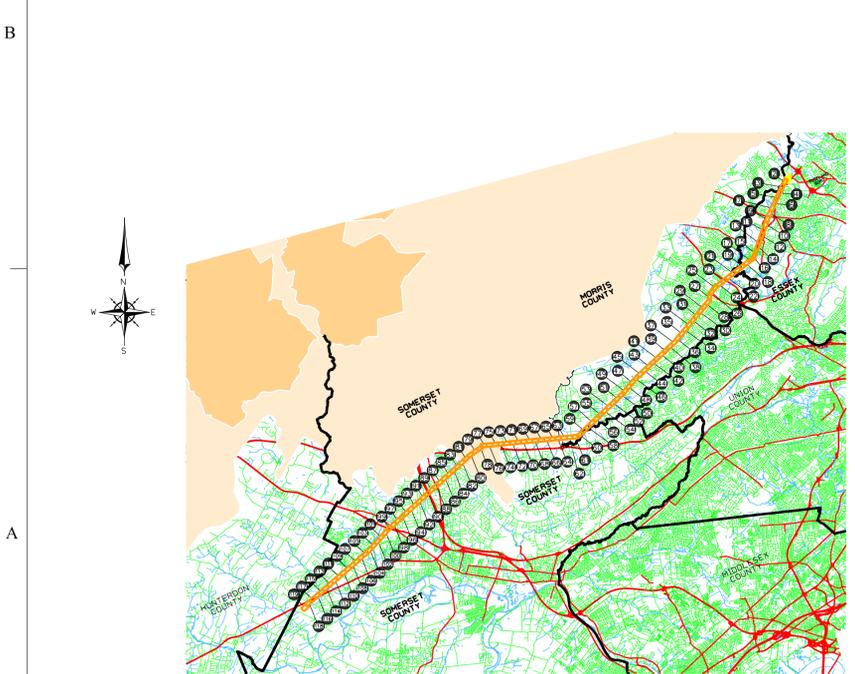
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| | | SUBJECT: RPV (SEGMENT 1) | |
| ASSET MANAGEMENT | | SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | |
| SCALE: 1" = 50' | CHECKED: J. FINK | APPROVED/TITLE: J. WANG / MANAGER | |
| DATE: 11/07/18 | DRAWN BY: J. GROSS | DISCIPLINE: CIVIL | |
| LOCATION: RPV | SIZE ARCH D | DWG. NO. TO-737087 | REV 1 SHEET 34 OF 127 |



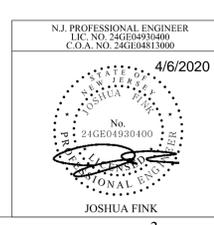
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 6. SEDIMENT BARRIER SHALL BE PLACED INSIDE OF LIMITS OF DISTURBANCE INDICATED ON THE DRAWINGS.
 7. ANY DISTURBED AREAS THAT REQUIRE RESTORATION WILL BE TOP-SOILED PRIOR TO SEEDING FOR PERMANENT VEGETATION STABILIZATION.
 8. FOR GENERAL NOTES AND DETAILS SEE DRAWING TO-737087 SHEET 120 THRU SHEET 127.
 9. FOR SOIL CONSERVATION DISTRICT (SCD) REQUIREMENTS SEE DRAWING TO-737087 SHEET 125 THRU 127. FOR HUDSON-ESSEX-PASSAIC COUNTY SCD SEE SHT 125. FOR MORRIS COUNTY SCD SEE SHT 126. FOR SOMERSET-UNION COUNTY SCD SEE SHT 127.

- LEGEND:**
- MATTING
 - WORKPAD/PULL SITE LIMIT OF DISTURBANCE (LOD)
 - GUARD STRUCTURE LIMIT OF DISTURBANCE (LOD)
 - PERMANENT ROAD
 - TRACKING PAD
 - WATERBODIES
 - WETLANDS
 - PSEG RIGHT OF WAY (R.O.W.)
 - PSEG RIGHT OF WAY (R.O.W.) FEE EASEMENTS
 - PROPOSED ACCESS ROAD (LOD)
 - LAYDOWN AREA (NO SOIL DISTURBANCE)
 - EXISTING ACCESS ROAD (PAVED)
 - EXISTING DIRT / GRAVEL ROAD
 - WETLAND BUFFER
 - TYPE 1 SEDIMENT BARRIER (SILT FENCE/18" DIA. SILT SOCK)
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 - GAS PIPELINE (PIPE DIA.)
 - PETROLEUM PIPE
 - FIBER OPTIC CABLE
 - WATER PIPELINE
 - SANITARY/STORM SEWER
 - EXISTING FENCE
 - EXISTING RAILROAD
 - MINOR CONTOUR
 - MAJOR CONTOUR
 - PROPOSED TEMPORARY GRADING CONTOUR
 - FLOODWAY
 - FLOOD HAZARD
 - ACCESS ROAD NUMBER
 - NEW H-FRAME
 - NEW MONOPOLE FOUNDATION
 - EXISTING POLE
 - EXISTING LATTICE STRUCTURE
 - PROPOSED PERMANENT GRADING
 - STATE ENDANGERED WILLOW-LEAF ASTER

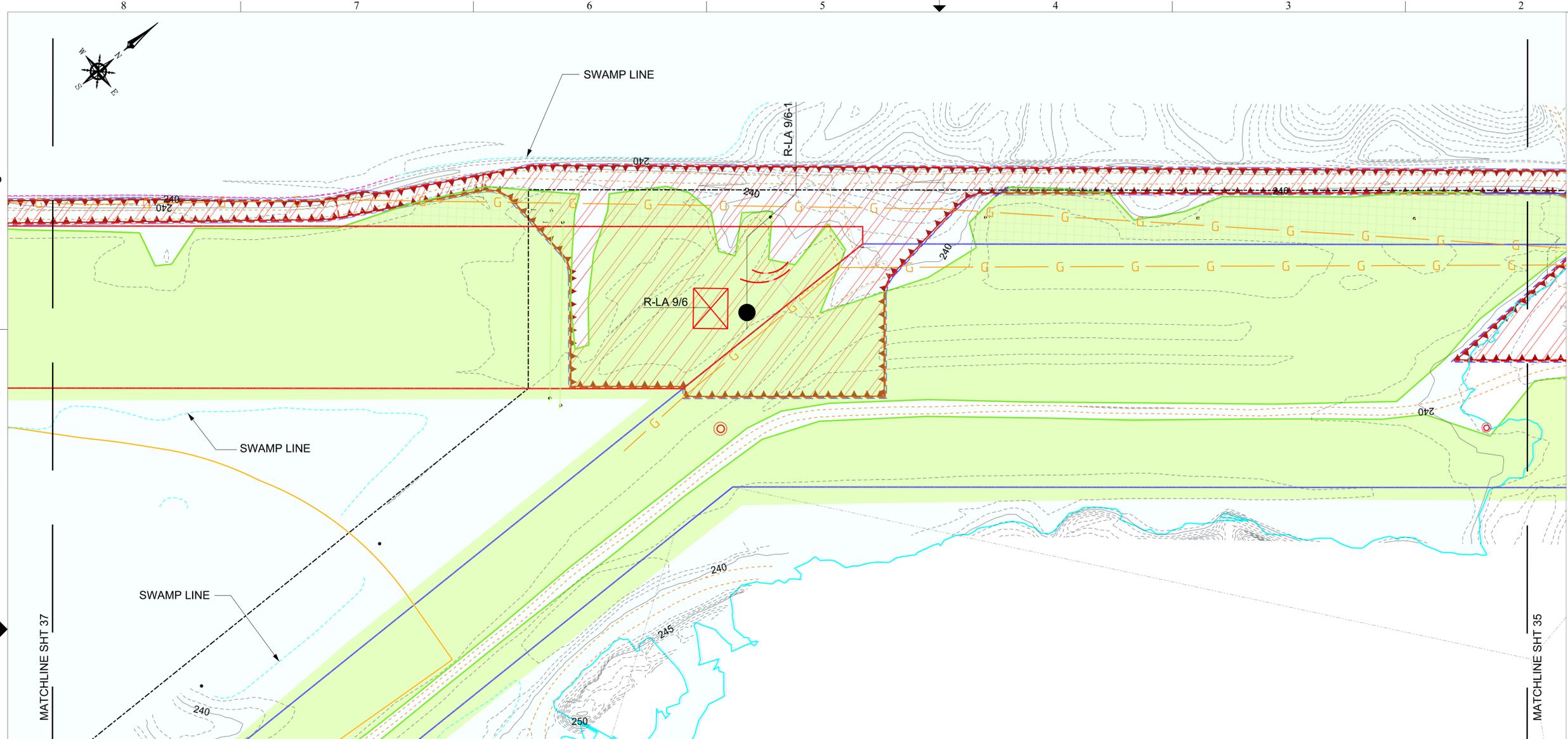
PLAN VIEW
SCALE: 1" = 50'



KEY PLAN



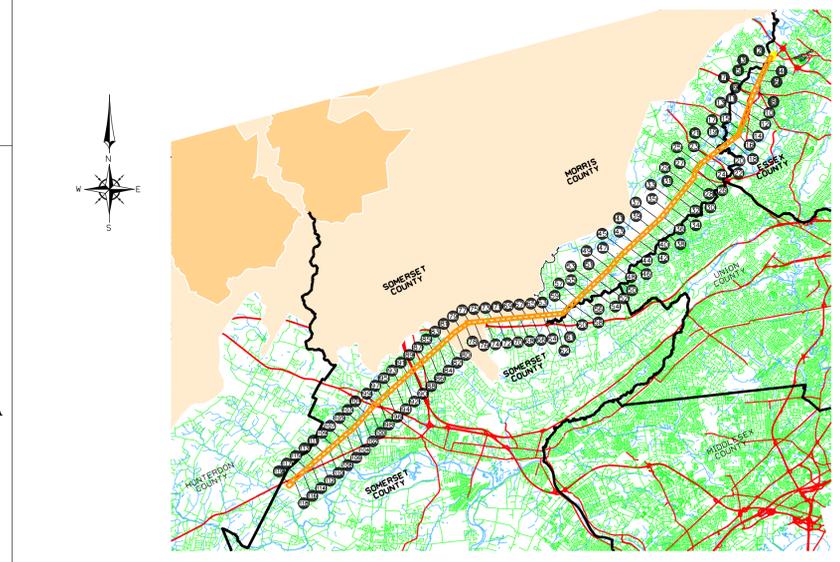
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG | | |
|---|--|-------------|----------|----------------|-------------------|
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG | | |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG | | |
| ZONE | REV | DESCRIPTION | DATE | APPROVED | |
| REVISIONS | | | | | |
| SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | | | | |
| SCALE | 1" = 50' | CHECKED | J. FINK | APPROVED/TITLE | J. WANG / MANAGER |
| DATE | 11/07/18 | DRAWN BY | J. GROSS | DISCIPLINE | CIVIL |
| LOCATION: | RPV | SIZE ARCH | D | DWG. NO. | TO-737087 |
| | | | | REV | 1 |
| | | | | SHEET | 35 OF 127 |



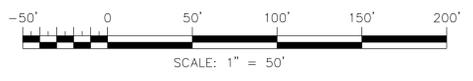
- NOTES:**
1. SEE DWG. TO-737087 SHEET 120 FOR TYPICAL ROAD SECTION.
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 3. TYPE 2 AND 3 SEDIMENT BARRIER SHALL REMAIN IN PLACE UNTIL CONSTRUCTION ACTIVITIES ARE COMPLETE AND SITE IS STABILIZED.
 4. AREA UNDER TOWERS SHALL NOT BE ROCKED.
 5. WORK PADS AND PULL SITES SHALL HAVE AN AVERAGE GRADE OF 2% ACROSS PAD.
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- LEGEND:**
- MATTING
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 - MINOR CONTOUR
 - MAJOR CONTOUR
 - PROPOSED TEMPORARY GRADING CONTOUR
 - FLOODWAY
 - FLOOD HAZARD
 - ACCESS ROAD NUMBER
 - NEW H-FRAME
 - NEW MONOPOLE FOUNDATION
 - EXISTING POLE
 - EXISTING LATTICE STRUCTURE
 - PROPOSED PERMANENT GRADING
 - STATE ENDANGERED WILLOW-LEAF ASTER

PLAN VIEW
SCALE: 1" = 50'



KEY PLAN

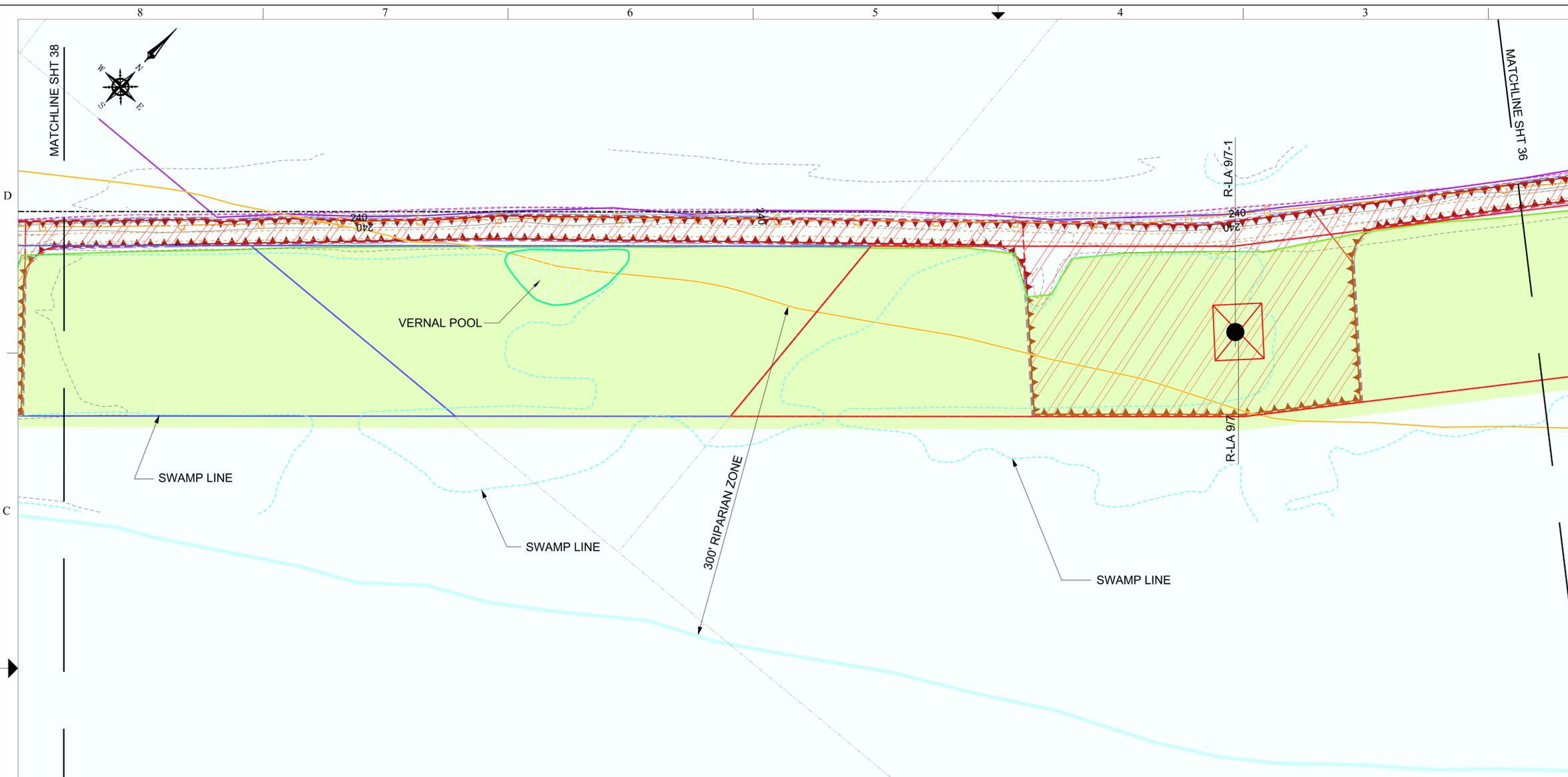


N.J. PROFESSIONAL ENGINEER
LIC. NO. 24GE04930400
C.O.A. NO. 24GI04813000

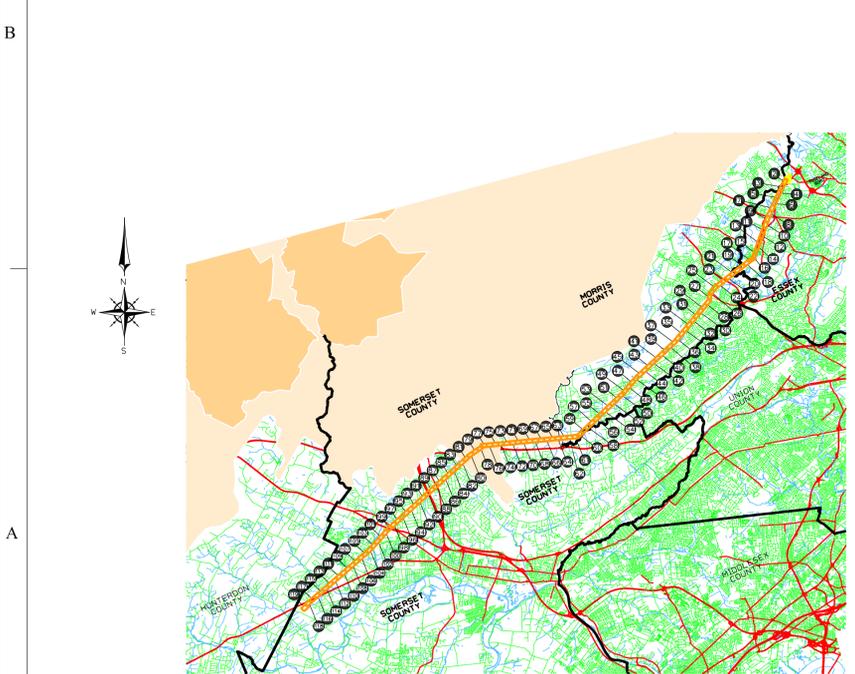
4/6/2020

JOSHUA FINK

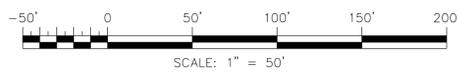
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG | |
|-----------------|--|---|-----------------------------------|-----------------------|
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG | |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG | |
| ZONE | REV | DESCRIPTION | DATE | APPROVED |
| REVISIONS | | | | |
| | | SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | |
| SCALE: 1" = 50' | | CHECKED: J. FINK | APPROVED/TITLE: J. WANG / MANAGER | |
| DATE: 11/07/18 | | DRAWN BY: J. GROSS | DISCIPLINE: CIVIL | |
| LOCATION: RPV | | SIZE ARCH D | DWG. NO. TO-737087 | REV 1 SHEET 36 OF 127 |



PLAN VIEW
SCALE: 1" = 50'



KEY PLAN



- NOTES:
- SEE DWG. TO-737087 SHEET 120 FOR TYPICAL ROAD SECTION.
 - TYPE 1 SEDIMENT BARRIER SHALL BE REMOVED ONCE ACCESS ROAD IS STABILIZED.
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 - AREA UNDER TOWERS SHALL NOT BE ROCKED.
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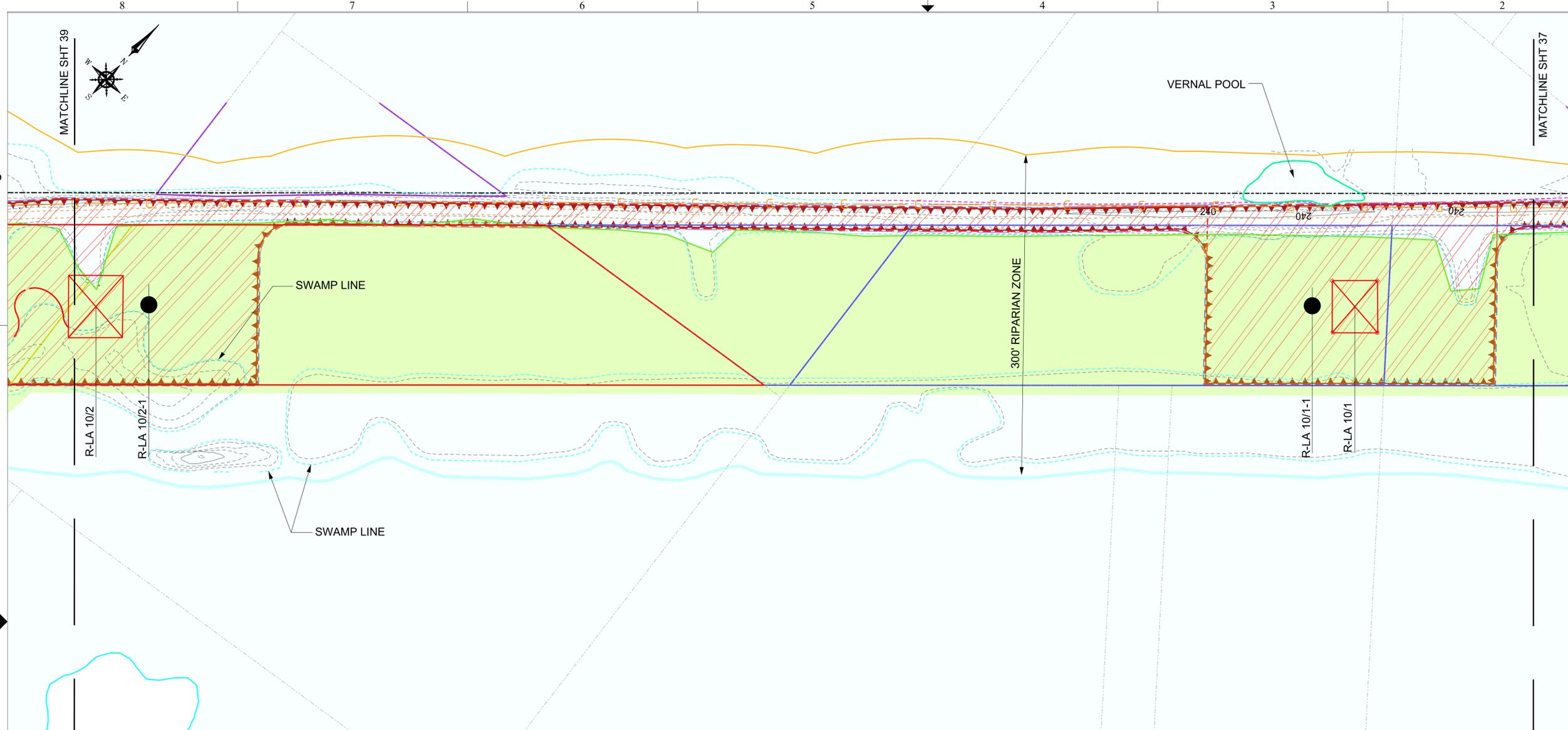
- LEGEND:
- MATting
 - WORKPAD/PULL SITE LIMIT OF DISTURBANCE (LOD)
 - GUARD STRUCTURE LIMIT OF DISTURBANCE (LOD)
 - PERMANENT ROAD
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 - PSEG RIGHT OF WAY (R.O.W.)
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 - MINOR CONTOUR
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 - FLOOD HAZARD
 - A.R. R-LA XXX-1 ACCESS ROAD NUMBER
 - NEW H-FRAME
 - NEW MONOPOLE FOUNDATION
 - EXISTING POLE
 - EXISTING LATTICE STRUCTURE
 - PROPOSED PERMANENT GRADING
 - STATE ENDANGERED WILLOW-LEAF ASTER

| NO. | DESCRIPTION | DATE | APPROVED |
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| SCALE: 1" = 50' | CHECKED: J. FINK | APPROVED/TITLE: J. WANG / MANAGER | |
| DATE: 11/07/18 | DRAWN BY: J. GROSS | DISCIPLINE: CIVIL | |
| LOCATION: RPV | SIZE ARCH D | DWG. NO. TO-737087 | REV 1 SHEET 37 OF 127 |

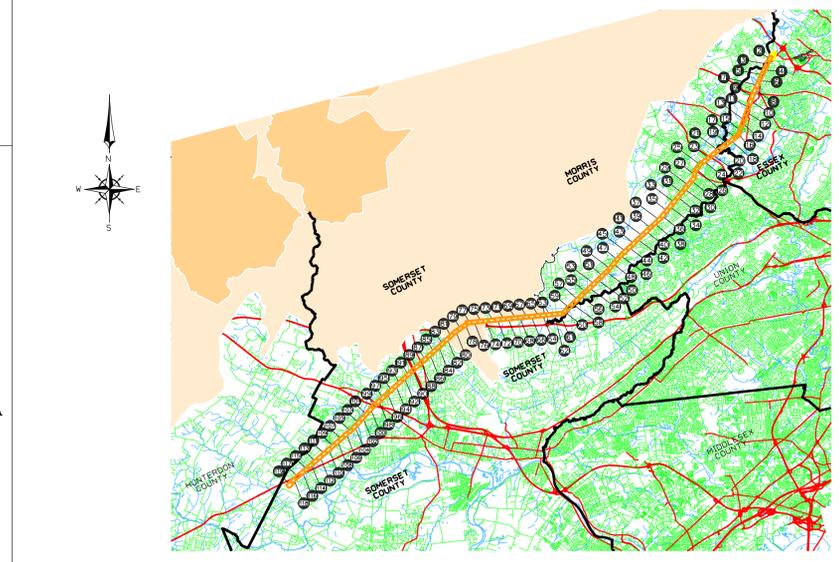




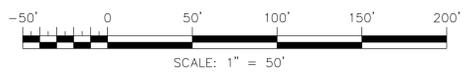
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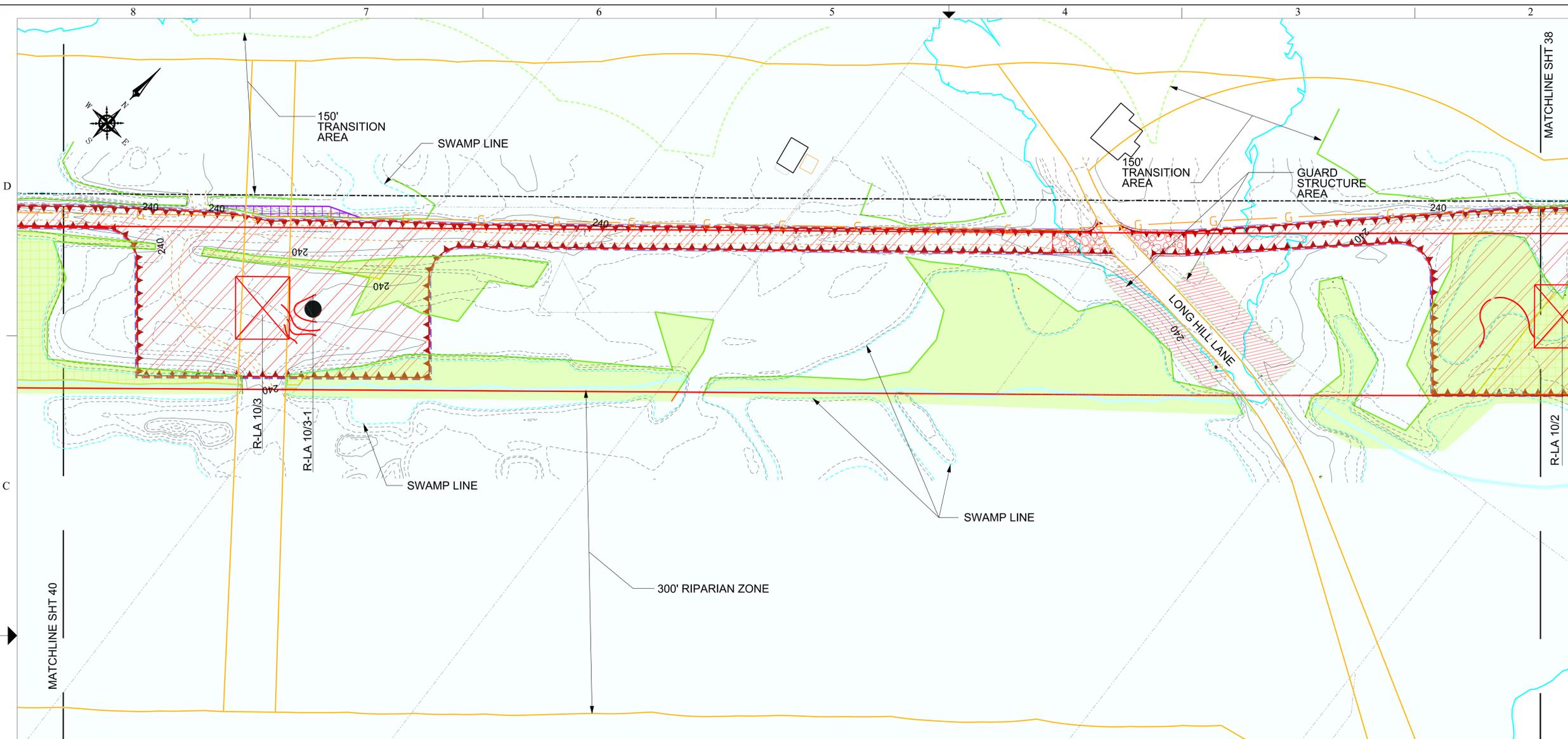
PLAN VIEW
SCALE: 1" = 50'



KEY PLAN



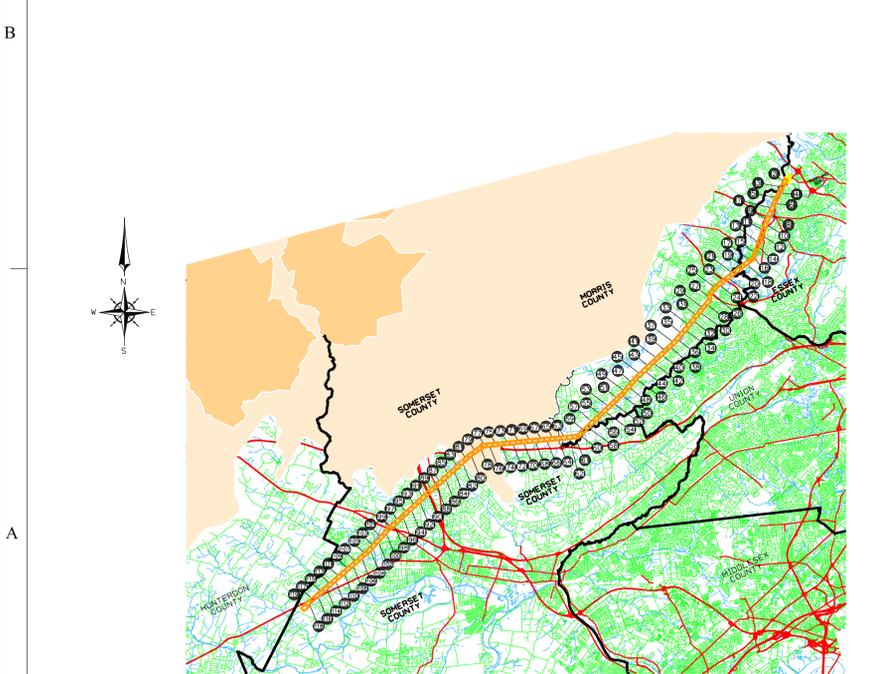
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| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG | |
| ZONE | REV | DESCRIPTION | DATE | APPROVED |
| REVISIONS | | | | |
| | | SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | |
| SCALE: 1" = 50' | | CHECKED: J. FINK | APPROVED/TITLE: J. WANG / MANAGER | |
| DATE: 11/07/18 | | DRAWN BY: J. GROSS | DISCIPLINE: CIVIL | |
| LOCATION: RPV | | SIZE ARCH: D | DWG. NO.: TO-737087 | REV: 1 SHEET: 38 OF 127 |



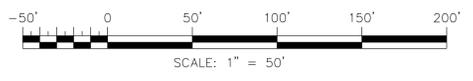
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- LEGEND:**
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PLAN VIEW
SCALE: 1" = 50'



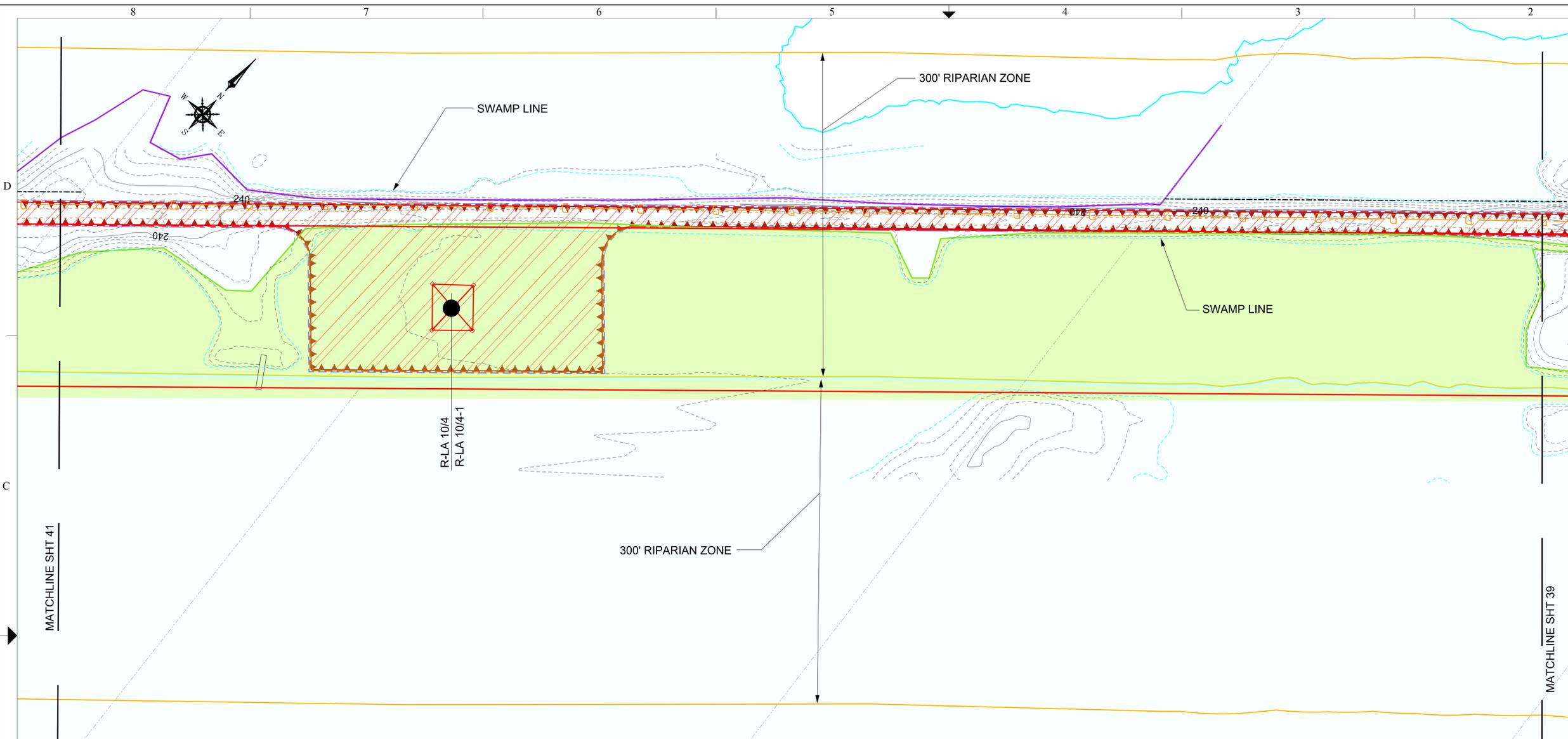
KEY PLAN



| NO. | DESCRIPTION | DATE | APPROVED |
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| REVISIONS | | | |
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| ZONE | REV | DESCRIPTION | DATE |
| | | | |

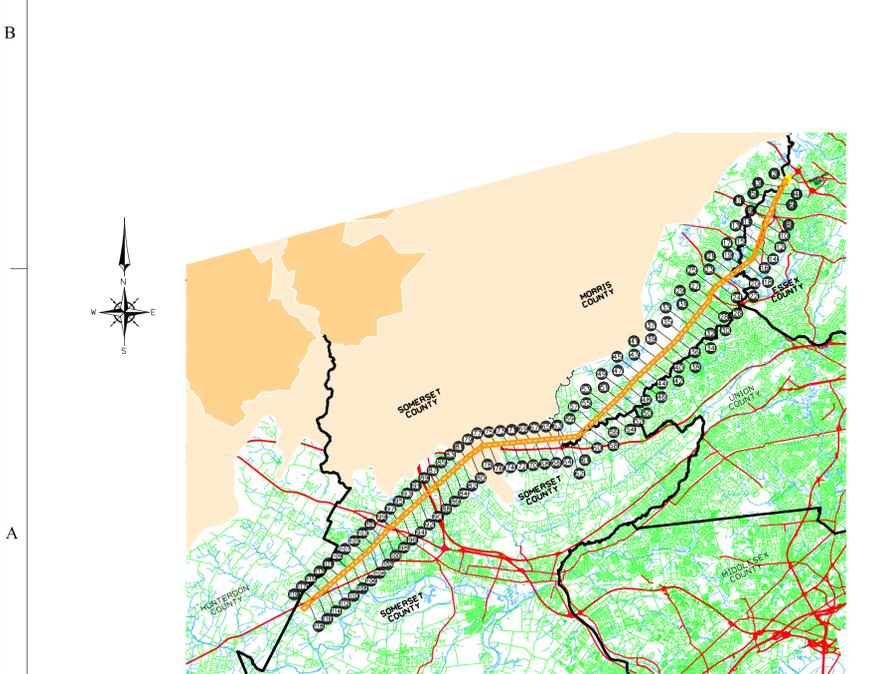
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|---|---|---------------------|-----------------------------------|
| <p>PSEG ASSET MANAGEMENT ELECTRIC TRANSMISSION ENGINEERING</p> | SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | APPROVED/TITLE: J. WANG / MANAGER |
| | SCALE: 1" = 50' | CHECKED: J. FINK | DISCIPLINE: CIVIL |
| LOCATION: RPV | DATE: 11/07/18 | DRAWN BY: J. GROSS | REV: 1 |
| | SIZE: ARCH D | DWG. NO.: TO-737087 | SHEET: 39 OF 127 |



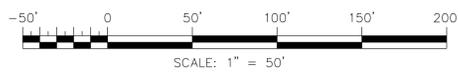
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 - STATE ENDANGERED WILLOW-LEAF ASTER

PLAN VIEW
SCALE: 1" = 50'



KEY PLAN



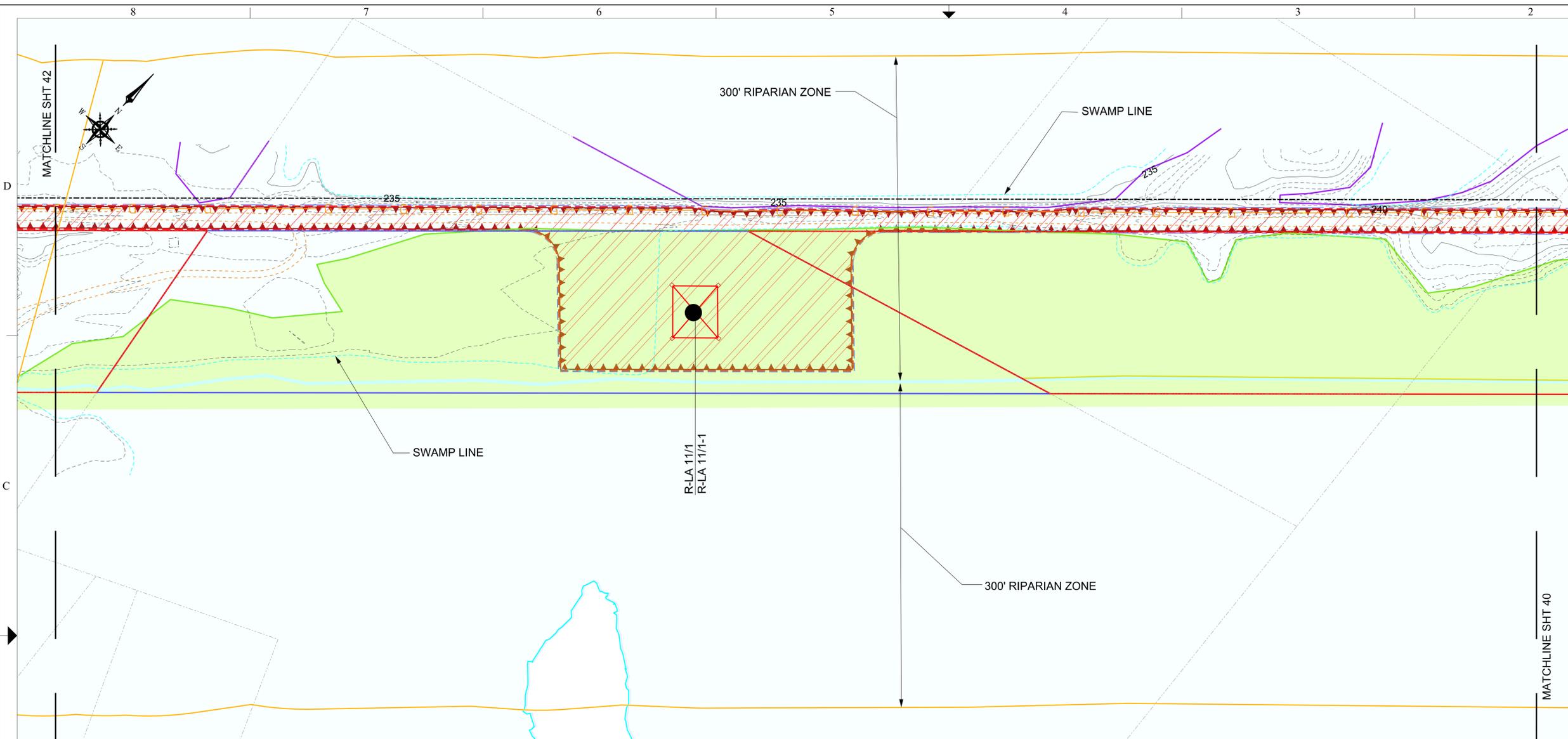
N.J. PROFESSIONAL ENGINEER
LIC. NO. 24GE04930400
C.O.A. NO. 24GI04813000

4/6/2020

No. 24GE04930400

JOSHUA FINK

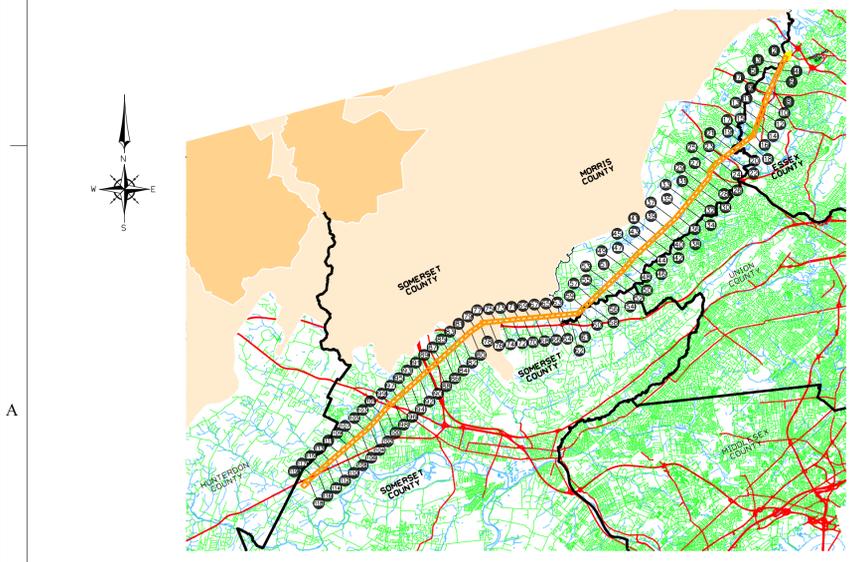
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| DATE: 11/07/18 | | DRAWN BY: J. GROSS | DISCIPLINE: CIVIL | |
| LOCATION: RPV | | SIZE ARCH D | DWG. NO. TO-737087 | REV 1 SHEET 40 OF 127 |



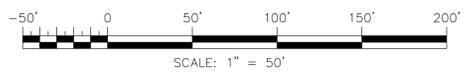
PLAN VIEW
SCALE: 1" = 50'

- NOTES:
- SEE DWG. TO-737087 SHEET 120 FOR TYPICAL ROAD SECTION.
 - TYPE 1 SEDIMENT BARRIER SHALL BE REMOVED ONCE ACCESS ROAD IS STABILIZED.
 - TYPE 2 AND 3 SEDIMENT BARRIER SHALL REMAIN IN PLACE UNTIL CONSTRUCTION ACTIVITIES ARE COMPLETE AND SITE IS STABILIZED.
 - AREA UNDER TOWERS SHALL NOT BE ROCKED.
 - WORK PADS AND PULL SITES SHALL HAVE AN AVERAGE GRADE OF 2% ACROSS PAD.
 - SEDIMENT BARRIER SHALL BE PLACED INSIDE OF LIMITS OF DISTURBANCE INDICATED ON THE DRAWINGS.
 - ANY DISTURBED AREAS THAT REQUIRE RESTORATION WILL BE TOP-SOILED PRIOR TO SEEDING FOR PERMANENT VEGETATION STABILIZATION.
 - FOR GENERAL NOTES AND DETAILS SEE DRAWING TO-737087 SHEET 120 THRU SHEET 127.
 - FOR SOIL CONSERVATION DISTRICT (SCD) REQUIREMENTS SEE DRAWING TO-737087 SHEET 125 THRU 127. FOR HUDSON-ESSEX-PASSAIC COUNTY SCD SEE SHT 125. FOR MORRIS COUNTY SCD SEE SHT 126. FOR SOMERSET-UNION COUNTY SCD SEE SHT 127.

- LEGEND:
- MATTING
 - WORKPAD/PULL SITE LIMIT OF DISTURBANCE (LOD)
 - GUARD STRUCTURE LIMIT OF DISTURBANCE (LOD)
 - PERMANENT ROAD
 - TRACKING PAD
 - WATERBODIES
 - WETLANDS
 - PSEG RIGHT OF WAY (R.O.W.)
 - PSEG RIGHT OF WAY (R.O.W.) FEE EASEMENTS
 - PROPOSED ACCESS ROAD (LOD)
 - LAYDOWN AREA (NO SOIL DISTURBANCE)
 - EXISTING ACCESS ROAD (PAVED)
 - EXISTING DIRT / GRAVEL ROAD
 - WETLAND BUFFER
 - TYPE 1 SEDIMENT BARRIER (SILT FENCE/18" DIA. SILT SOCK)
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 - ELECTRIC
 - GAS PIPELINE (PIPE DIA.)
 - PETROLEUM PIPE
 - FIBER OPTIC CABLE
 - WATER PIPELINE
 - SANITARY/STORM SEWER
 - EXISTING FENCE
 - EXISTING RAILROAD
 - MINOR CONTOUR
 - MAJOR CONTOUR
 - PROPOSED TEMPORARY GRADING CONTOUR
 - FLOODWAY
 - FLOOD HAZARD
 - A.R. R-LA XXX-1 ACCESS ROAD NUMBER
 - NEW H-FRAME
 - NEW MONOPOLE FOUNDATION
 - EXISTING POLE
 - EXISTING LATTICE STRUCTURE
 - PROPOSED PERMANENT GRADING
 - STATE ENDANGERED WILLOW-LEAF ASTER



KEY PLAN

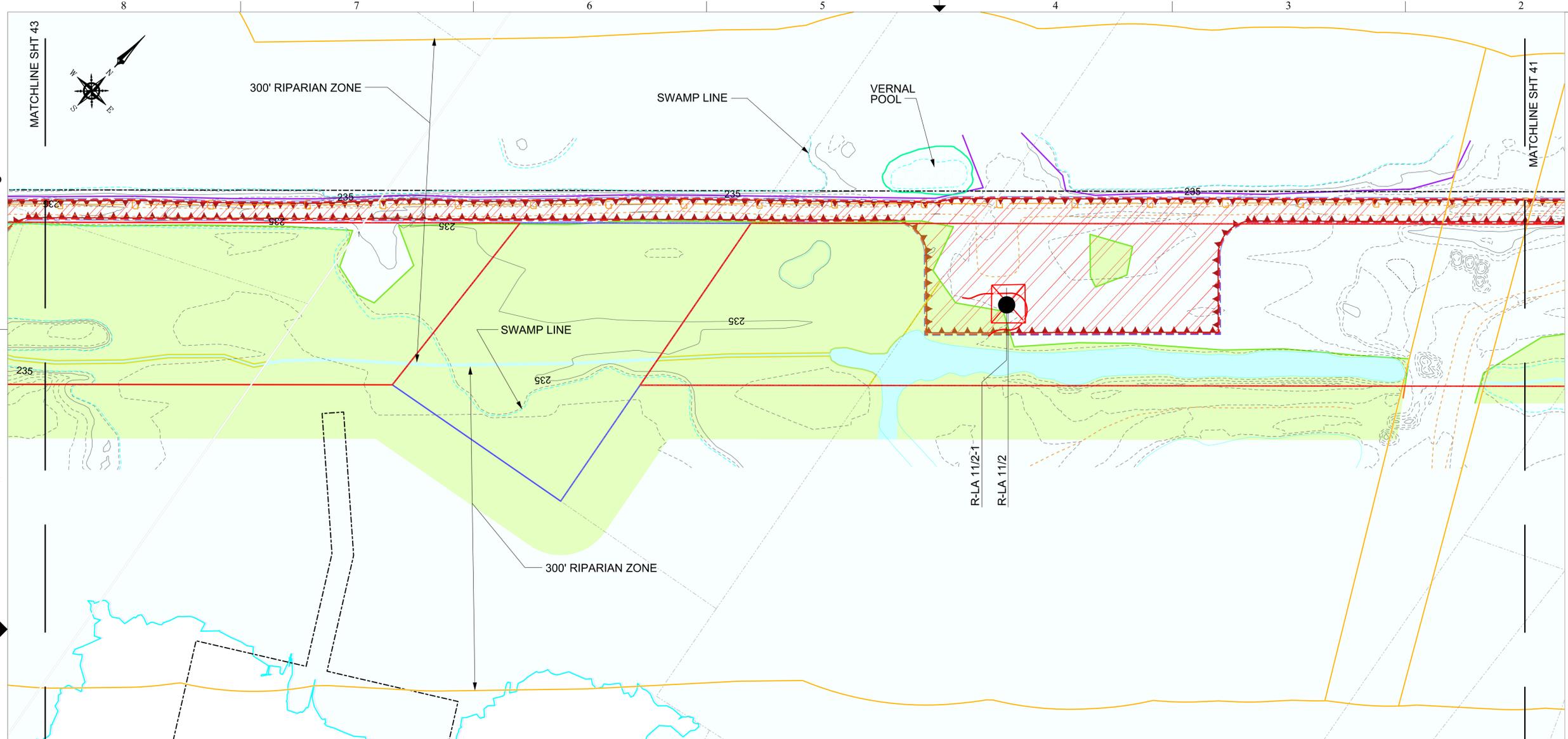


N.J. PROFESSIONAL ENGINEER
LIC. NO. 24GE04930400
C.O.A. NO. 24GI04813000

4/6/2020

JOSHUA FINK

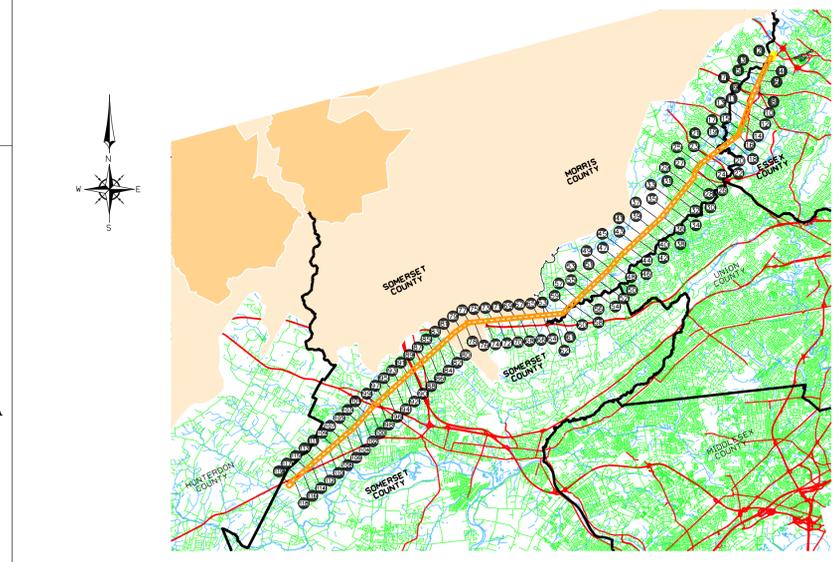
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG | | |
|-----------|--|---|----------|----------------|-------------------|
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG | | |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG | | |
| ZONE | REV | DESCRIPTION | DATE | APPROVED | |
| REVISIONS | | | | | |
| | | SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | | |
| SCALE | 1" = 50' | CHECKED | J. FINK | APPROVED/TITLE | J. WANG / MANAGER |
| DATE | 11/07/18 | DRAWN BY | J. GROSS | DISCIPLINE | CIVIL |
| LOCATION: | RPV | SIZE ARCH | D | DWG. NO. | TO-737087 |
| | | | | REV | 1 |
| | | | | SHEET | 41 OF 127 |



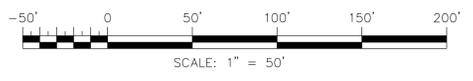
- NOTES:**
1. SEE DWG. TO-737087 SHEET 120 FOR TYPICAL ROAD SECTION.
 2. TYPE 1 SEDIMENT BARRIER SHALL BE REMOVED ONCE ACCESS ROAD IS STABILIZED.
 3. TYPE 2 AND 3 SEDIMENT BARRIER SHALL REMAIN IN PLACE UNTIL CONSTRUCTION ACTIVITIES ARE COMPLETE AND SITE IS STABILIZED.
 4. AREA UNDER TOWERS SHALL NOT BE ROCKED.
 5. WORK PADS AND PULL SITES SHALL HAVE AN AVERAGE GRADE OF 2% ACROSS PAD.
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 8. FOR GENERAL NOTES AND DETAILS SEE DRAWING TO-737087 SHEET 120 THRU SHEET 127.
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- LEGEND:**
- MATTING
 - WORKPAD/PULL SITE LIMIT OF DISTURBANCE (LOD)
 - GUARD STRUCTURE LIMIT OF DISTURBANCE (LOD)
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 - MAJOR CONTOUR
 - PROPOSED TEMPORARY GRADING CONTOUR
 - FLOODWAY
 - FLOOD HAZARD
 - ACCESS ROAD NUMBER
 - NEW H-FRAME
 - NEW MONOPOLE FOUNDATION
 - EXISTING POLE
 - EXISTING LATTICE STRUCTURE
 - PROPOSED PERMANENT GRADING
 - STATE ENDANGERED WILLOW-LEAF ASTER

PLAN VIEW
SCALE: 1" = 50'



KEY PLAN

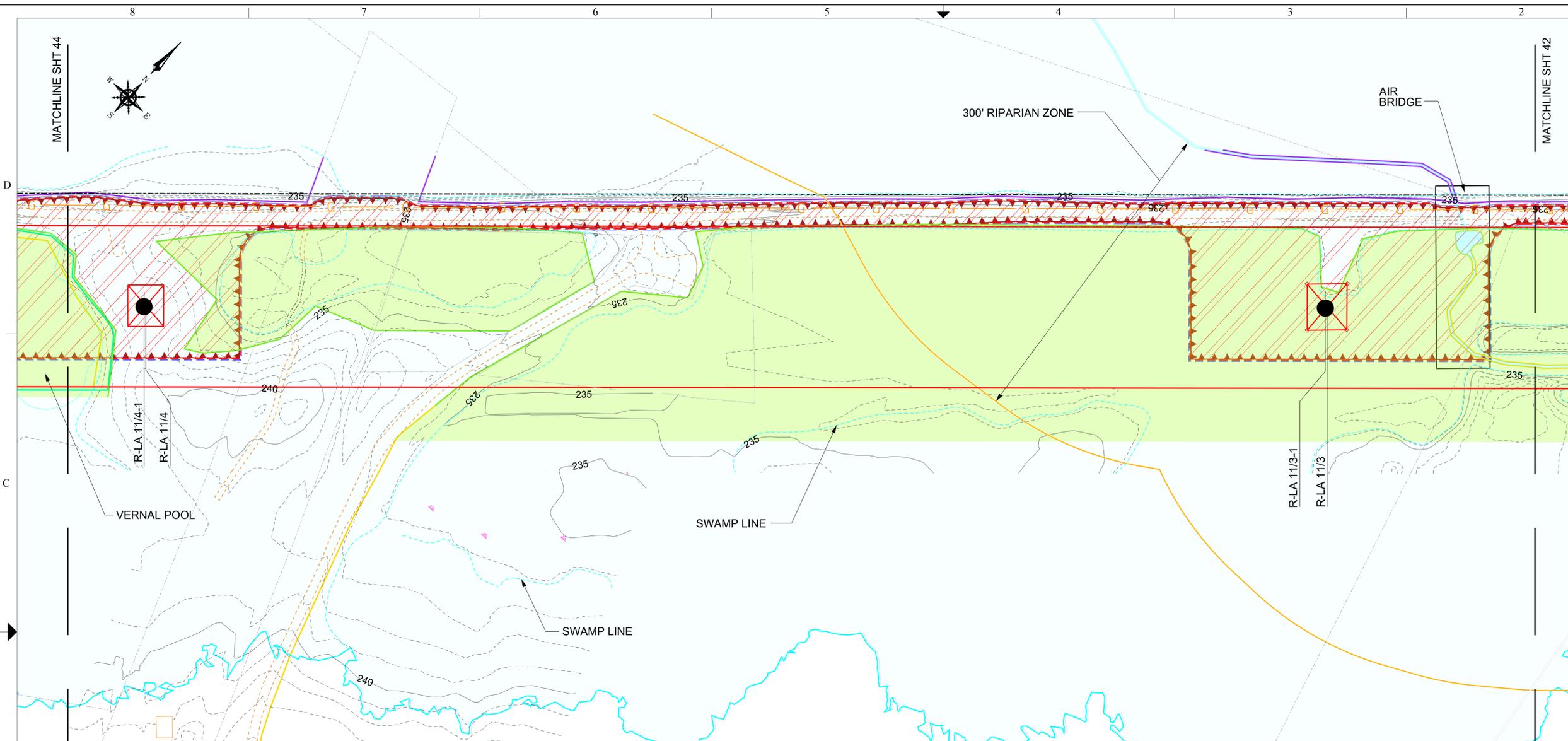


N.J. PROFESSIONAL ENGINEER
LIC. NO. 24GE04930400
C.O.A. NO. 24GI04813000

4/6/2020

JOSHUA FINK

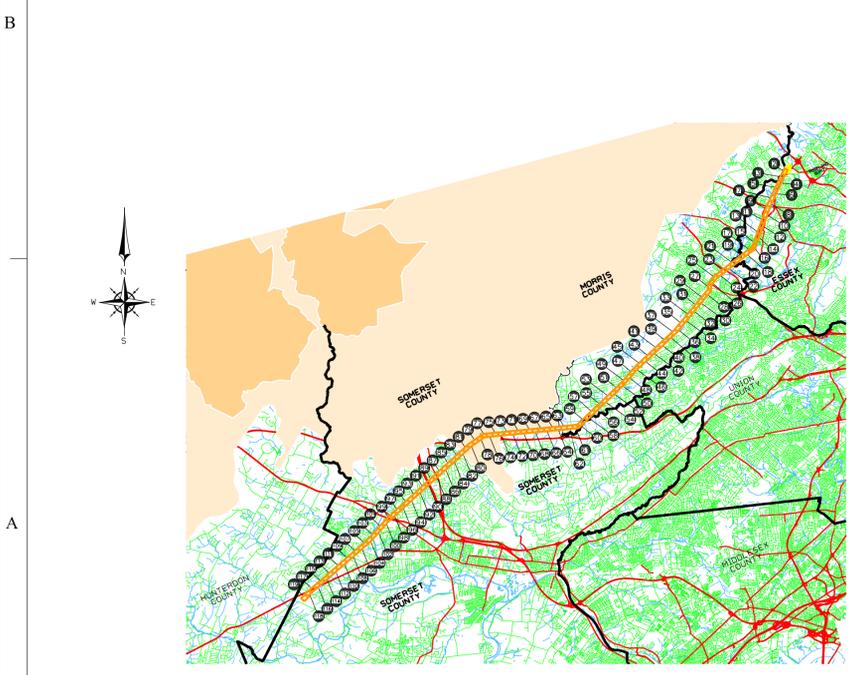
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG | |
|-----------------|--|---|-----------------------------------|-----------------------|
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG | |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG | |
| ZONE | REV | DESCRIPTION | DATE | APPROVED |
| REVISIONS | | | | |
| | | SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | |
| SCALE: 1" = 50' | | CHECKED: J. FINK | APPROVED/TITLE: J. WANG / MANAGER | |
| DATE: 11/07/18 | | DRAWN BY: J. GROSS | DISCIPLINE: CIVIL | |
| LOCATION: RPV | | SIZE ARCH D | DWG. NO. TO-737087 | REV 1 SHEET 42 OF 127 |



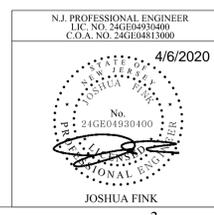
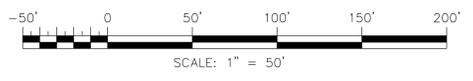
PLAN VIEW
SCALE: 1" = 50'

- NOTES:
- SEE DWG. TO-737087 SHEET 120 FOR TYPICAL ROAD SECTION.
 - TYPE 1 SEDIMENT BARRIER SHALL BE REMOVED ONCE ACCESS ROAD IS STABILIZED.
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- LEGEND:
- MATTING
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 - PSEG RIGHT OF WAY (R.O.W.)
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 - FLOODWAY
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 - ACCESS ROAD NUMBER
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 - NEW MONOPOLE FOUNDATION
 - EXISTING POLE
 - EXISTING LATTICE STRUCTURE
 - PROPOSED PERMANENT GRADING
 - STATE ENDANGERED WILLOW-LEAF ASTER



KEY PLAN



| NO. | DESCRIPTION | DATE | APPROVED |
|-----|--|------------|----------|
| 2 | ISSUED FOR CONSTRUCTION (SDC SUBMISSION) | 03/06/2020 | J. WANG |
| 1 | RE-ISSUED FOR CONSTRUCTION | 01/24/2020 | J. WANG |
| 0 | ISSUED FOR CONSTRUCTION | 06/13/19 | J. WANG |

| REVISIONS | | | |
|-----------|-----|-------------|------|
| ZONE | REV | DESCRIPTION | DATE |
| | | | |

| | | | |
|---|---|---------------------|-----------------------------------|
| <p>ASSET MANAGEMENT ELECTRIC TRANSMISSION ENGINEERING</p> | SUBJECT: RPV (SEGMENT 1) SOIL EROSION AND SEDIMENT CONTROL PLANS AND DETAILS | | APPROVED/TITLE: J. WANG / MANAGER |
| | SCALE: 1" = 50' | CHECKED: J. FINK | DISCIPLINE: CIVIL |
| LOCATION: RPV | DATE: 11/07/18 | DRAWN BY: J. GROSS | REV: 1 |
| | SIZE: ARCH D | DWG. NO.: TO-737087 | SHEET: 43 OF 127 |

ATTACHMENT G

FAA DETERMINATION OF NO HAZARD TO AIR NAVIGATION LETTERS



Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2019-AEA-7393-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
 PSE&G
 4000 Hadley Road
 South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Transmission Line R-LA 7/6-1
 Location: Roseland Borough, NJ
 Latitude: 40-44-26.31N NAD 83
 Longitude: 74-23-56.72W
 Heights: 298 feet site elevation (SE)
 118 feet above ground level (AGL)
 416 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7393-OE.

Signature Control No: 409906199-414256264

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

Additional information for ASN 2019-AEA-7393-OE

Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7393-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7394-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 7/7-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-44-20.83N NAD 83 |
| Longitude: | 74-24-02.76W |
| Heights: | 308 feet site elevation (SE) 118 feet above ground level (AGL) 426 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7394-OE.

Signature Control No: 409906200-414202494

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

Additional information for ASN 2019-AEA-7394-OE

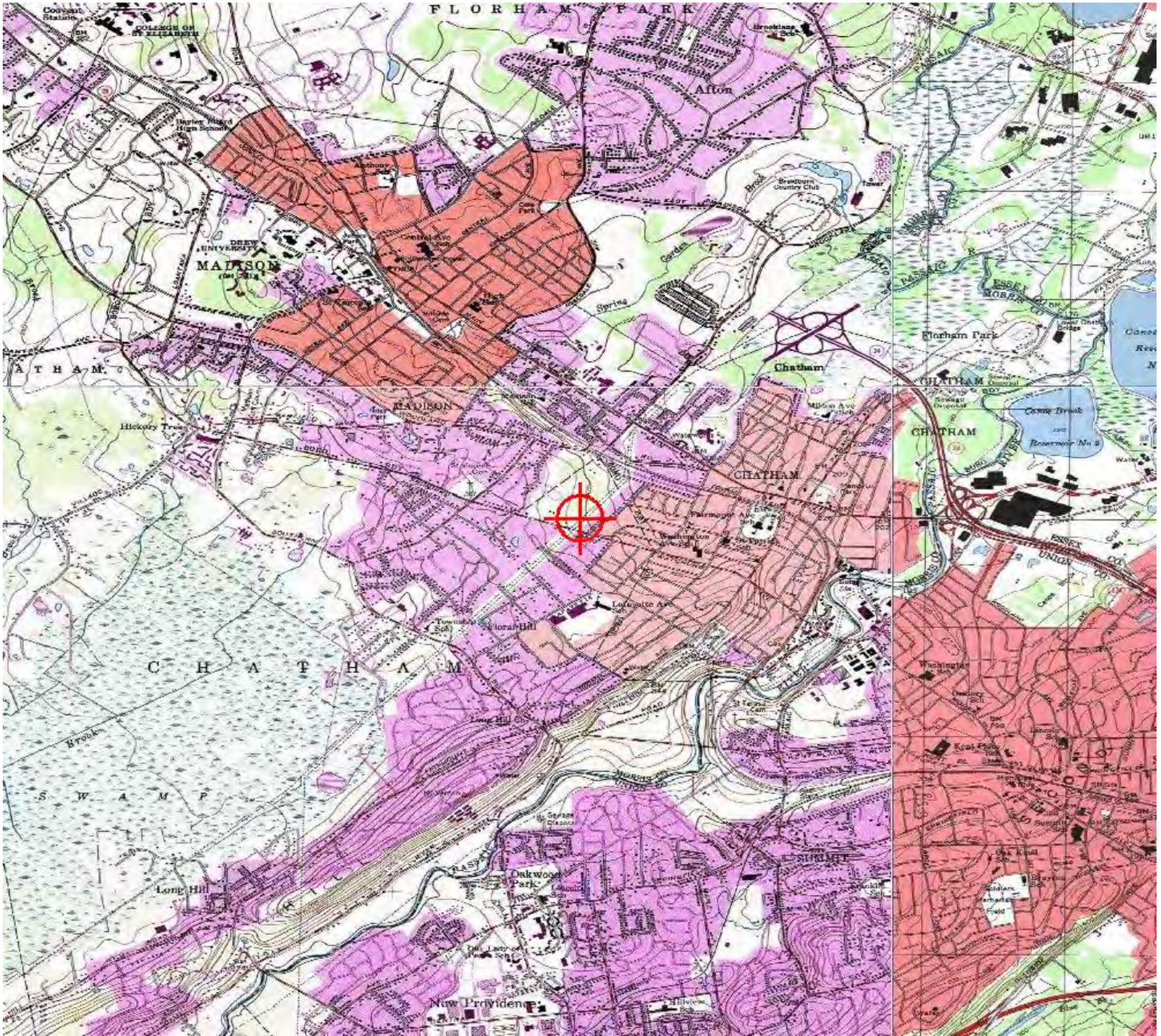
Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7394-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.



TOPO Map for ASN 2019-AEA-7394-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7395-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 8/1-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-44-15.57N NAD 83 |
| Longitude: | 74-24-08.54W |
| Heights: | 334 feet site elevation (SE) 118 feet above ground level (AGL) 452 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
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- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7395-OE.

Signature Control No: 409906201-414202502

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

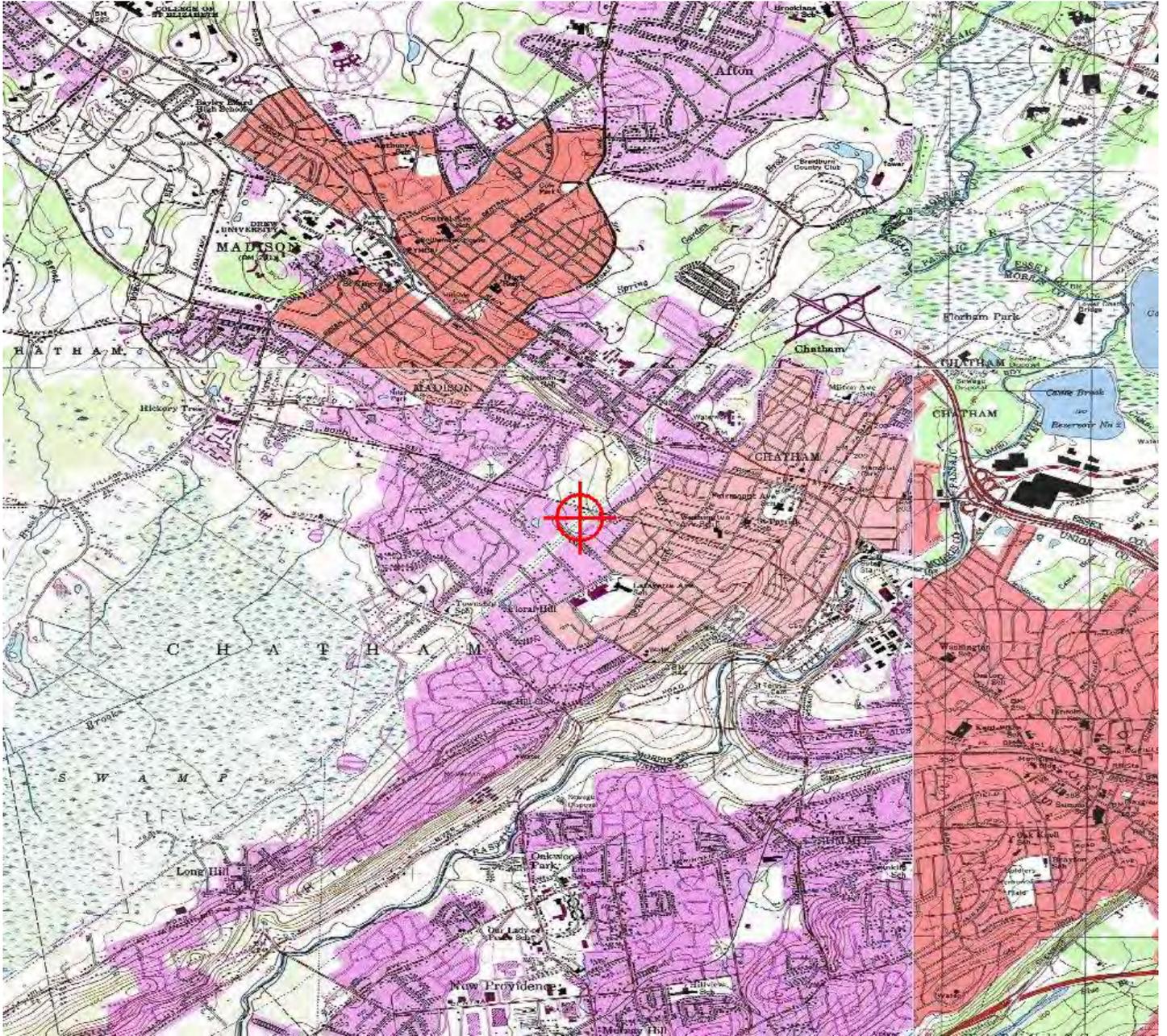
Additional information for ASN 2019-AEA-7395-OE

Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7395-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7396-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 8/2-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-44-10.12N NAD 83 |
| Longitude: | 74-24-14.51W |
| Heights: | 362 feet site elevation (SE) 108 feet above ground level (AGL) 470 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7396-OE.

Signature Control No: 409906202-414202493

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

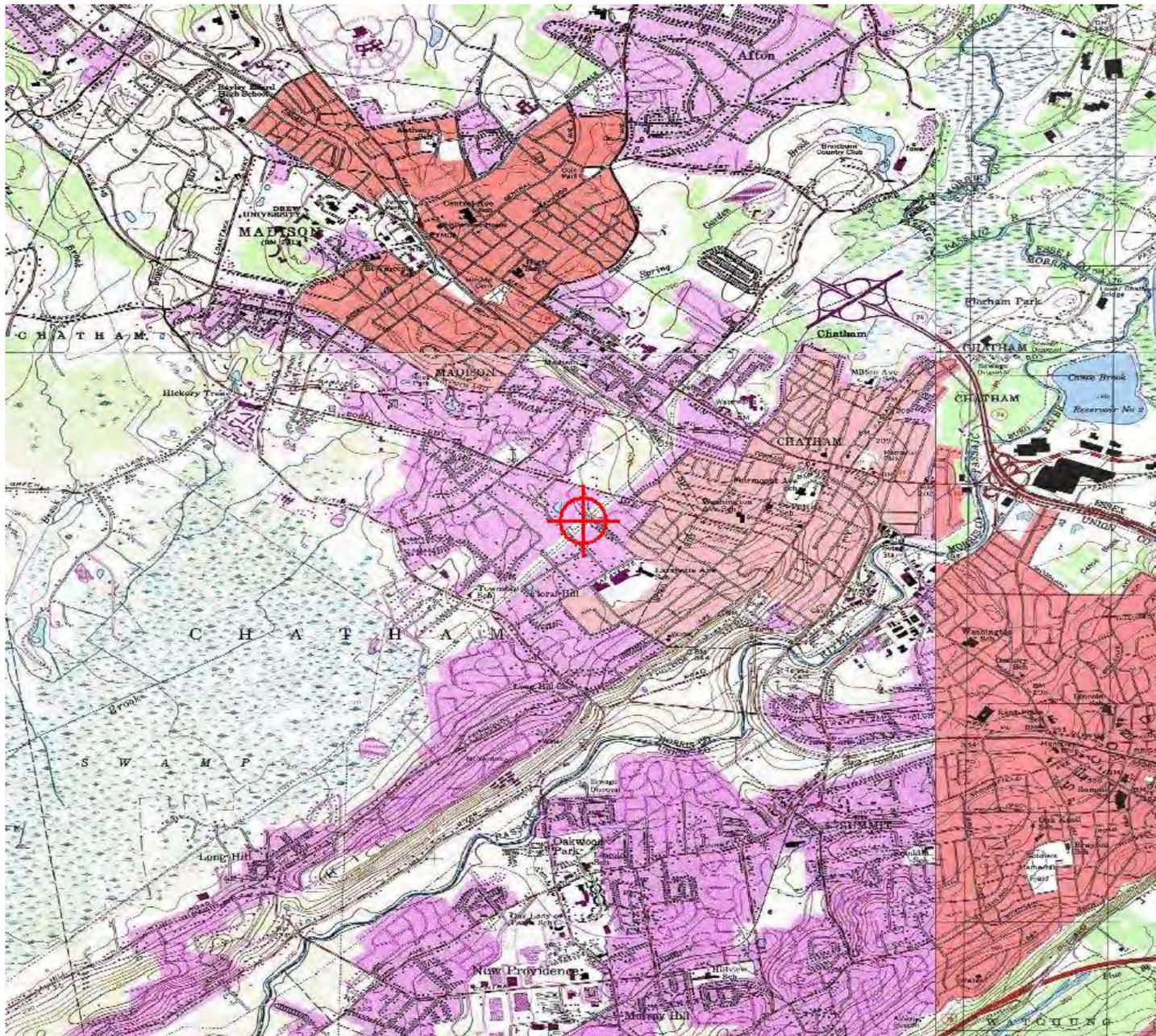
Additional information for ASN 2019-AEA-7396-OE

Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7396-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
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Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7397-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 8/3-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-44-04.93N NAD 83 |
| Longitude: | 74-24-20.17W |
| Heights: | 340 feet site elevation (SE) 123 feet above ground level (AGL) 463 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7397-OE.

Signature Control No: 409906203-414202496

(DNE)

Darin Clipper
Specialist

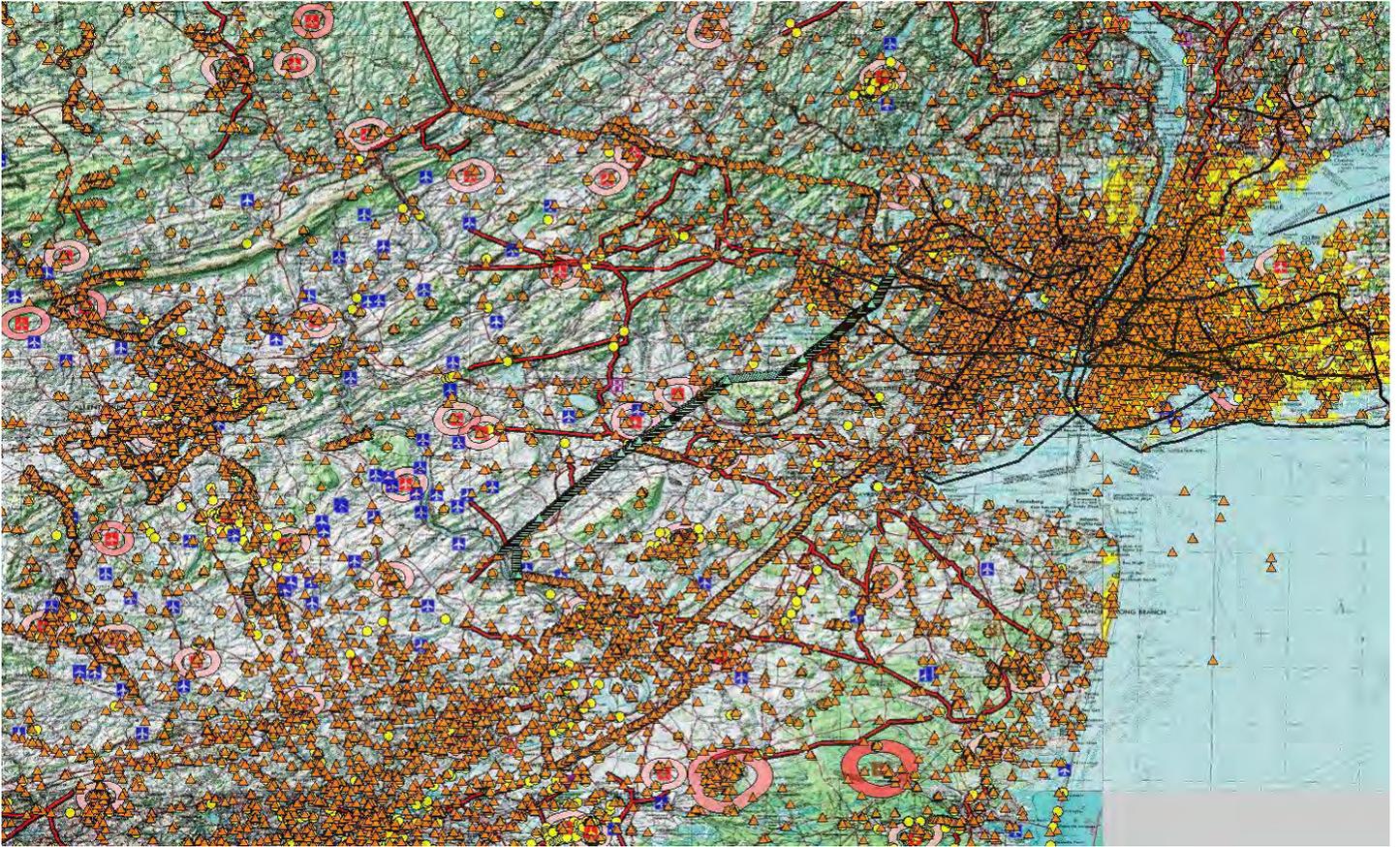
Attachment(s)
Additional Information
Case Description
Map(s)

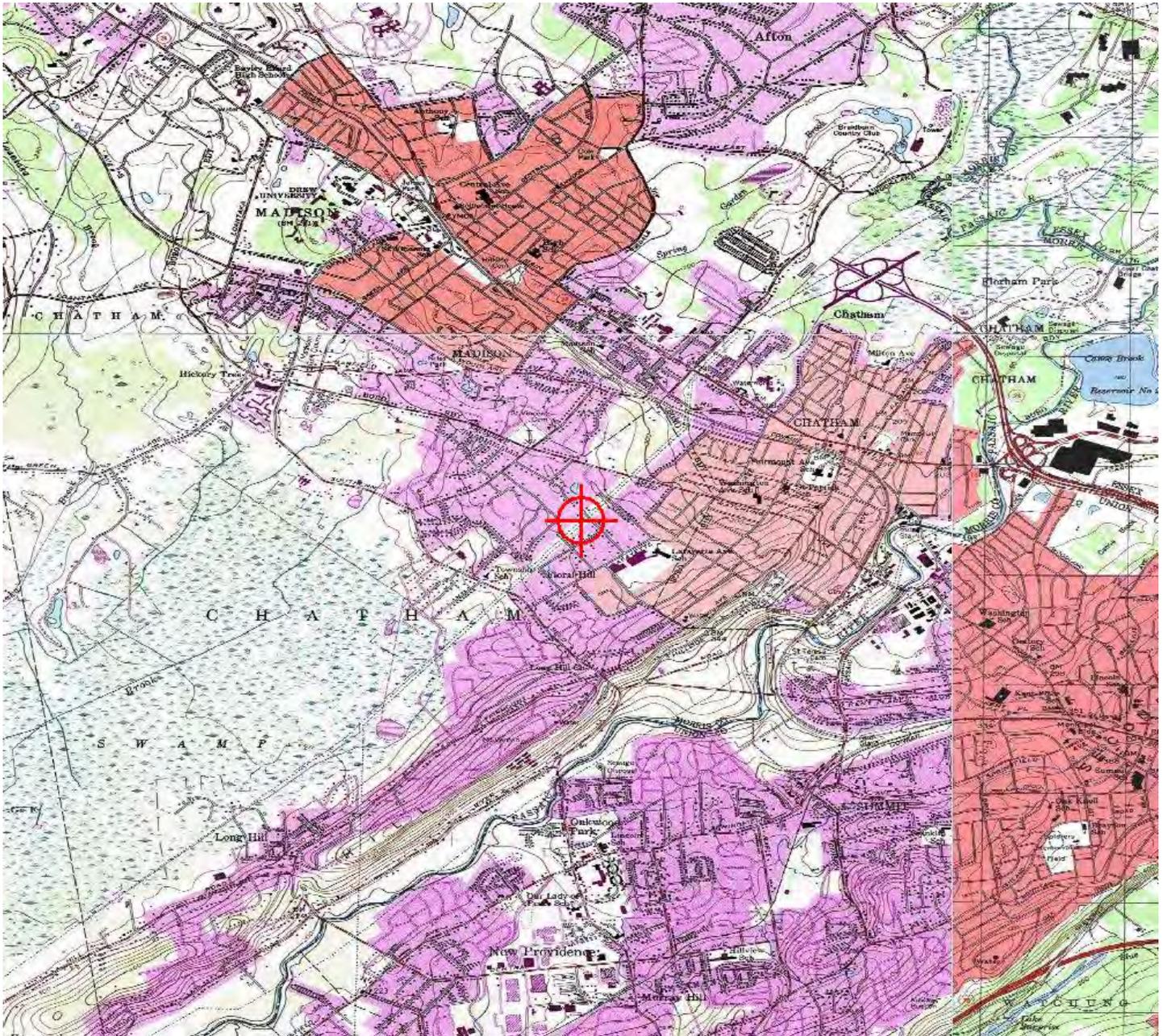
Additional information for ASN 2019-AEA-7397-OE

Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7397-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.







Mail Processing Center
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Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7398-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 8/4-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-43-59.08N NAD 83 |
| Longitude: | 74-24-26.61W |
| Heights: | 335 feet site elevation (SE) 123 feet above ground level (AGL) 458 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7398-OE.

Signature Control No: 409906204-414202491

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

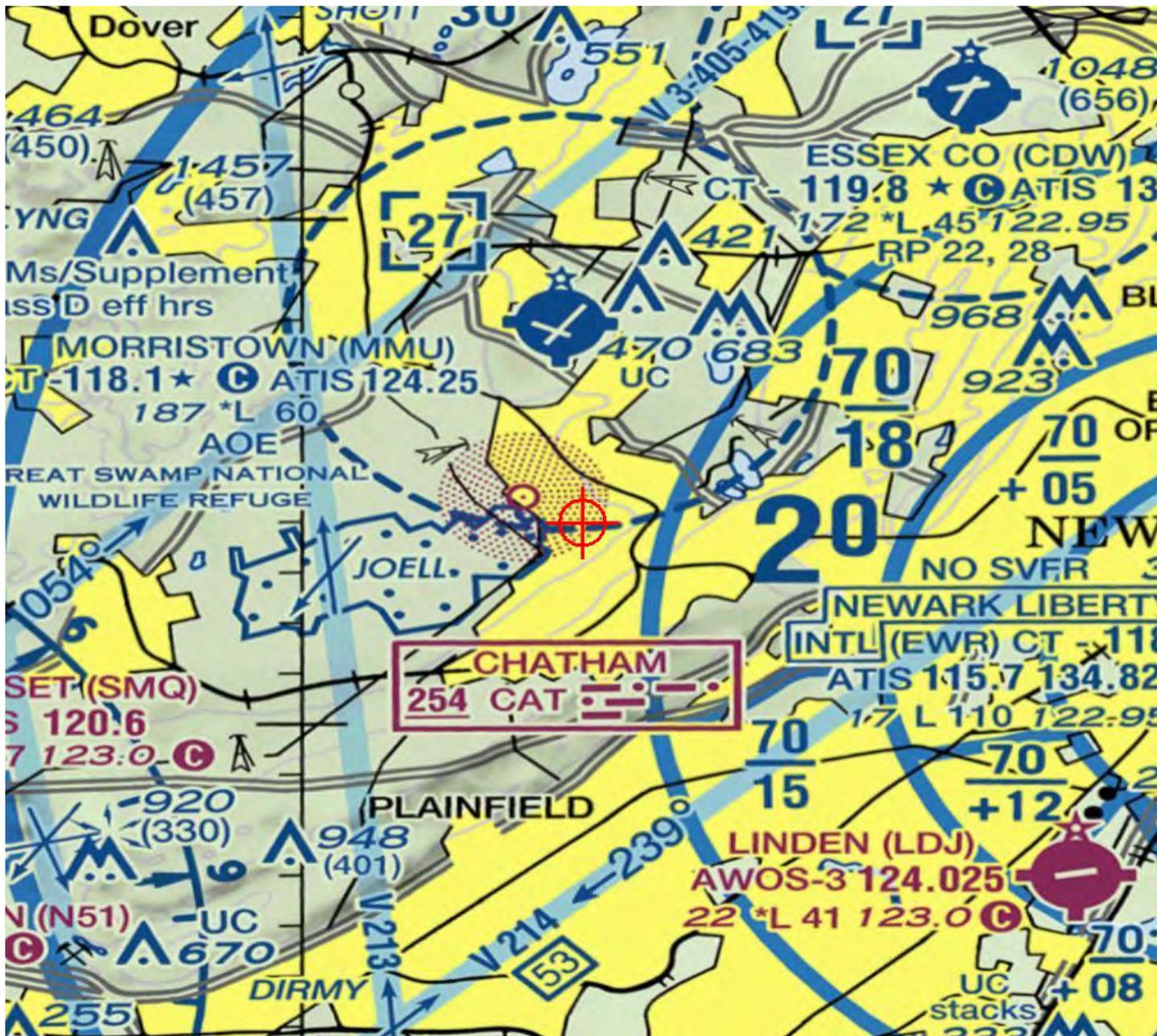
Additional information for ASN 2019-AEA-7398-OE

Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7398-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.







Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7399-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 8/5-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-43-53.50N NAD 83 |
| Longitude: | 74-24-32.71W |
| Heights: | 321 feet site elevation (SE) 133 feet above ground level (AGL) 454 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7399-OE.

Signature Control No: 409906205-414202505

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

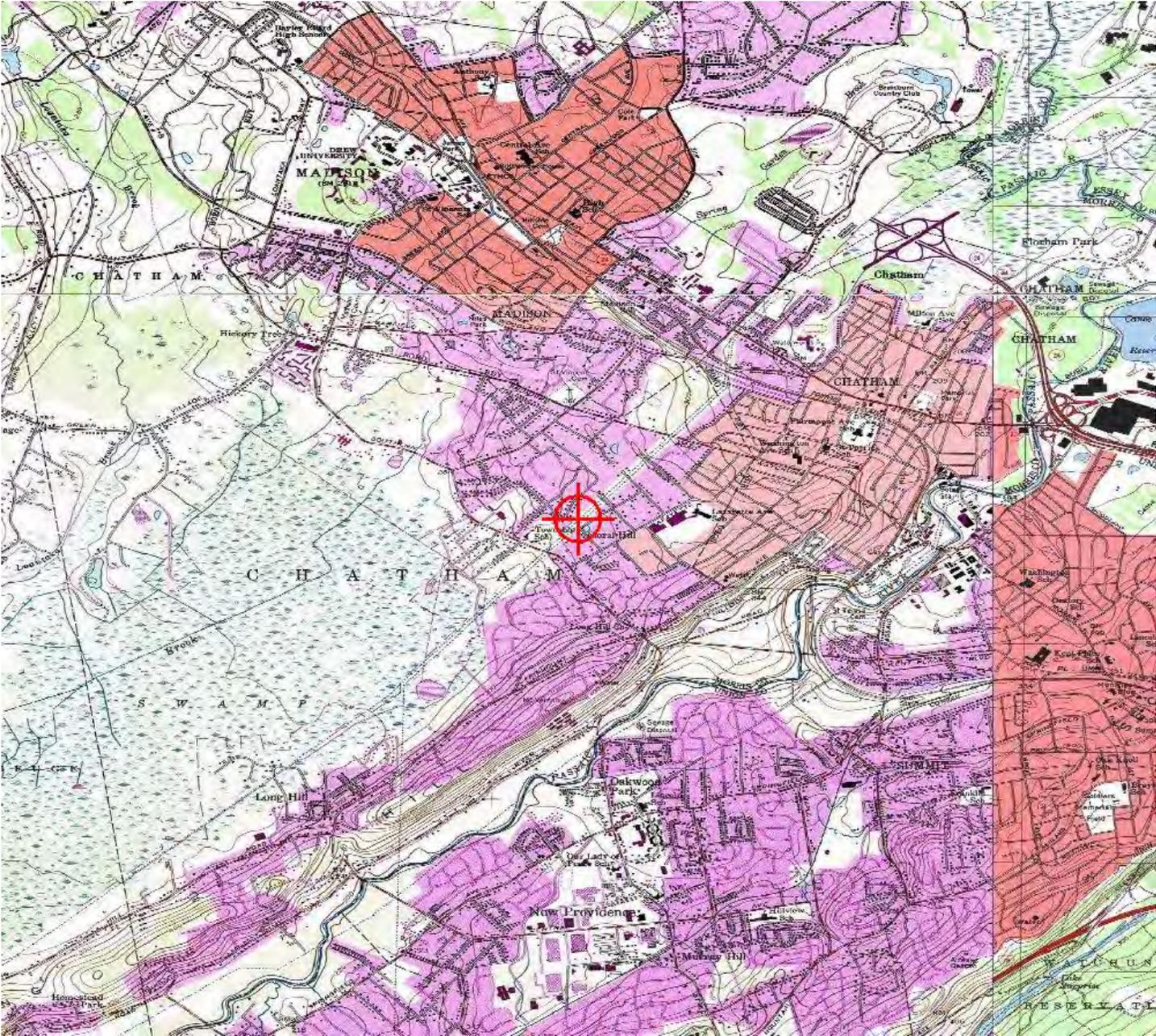
Additional information for ASN 2019-AEA-7399-OE

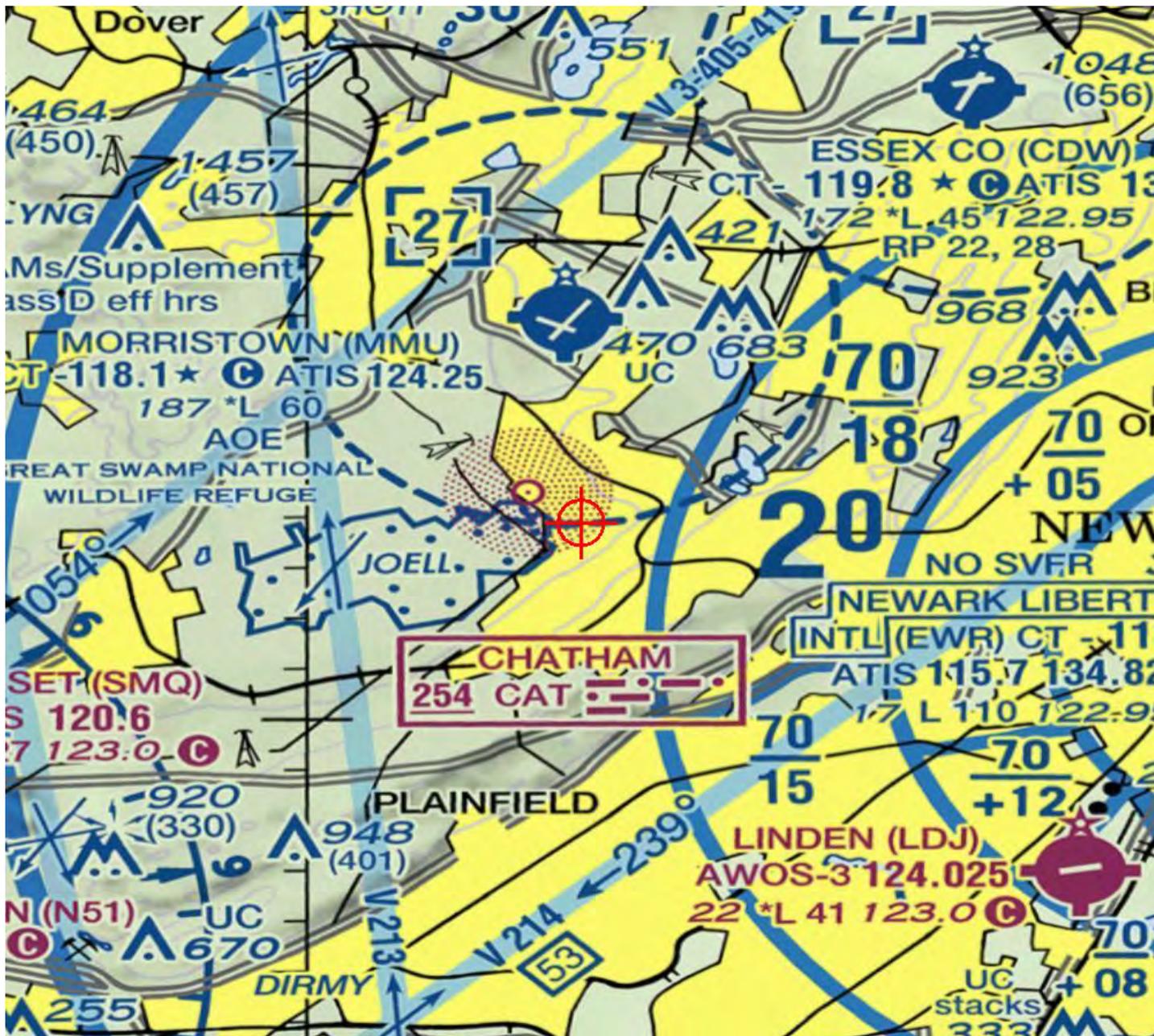
Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7399-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7400-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 8/6-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-43-47.06N NAD 83 |
| Longitude: | 74-24-39.77W |
| Heights: | 307 feet site elevation (SE) 133 feet above ground level (AGL) 440 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
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- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7400-OE.

Signature Control No: 409906206-414202510

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

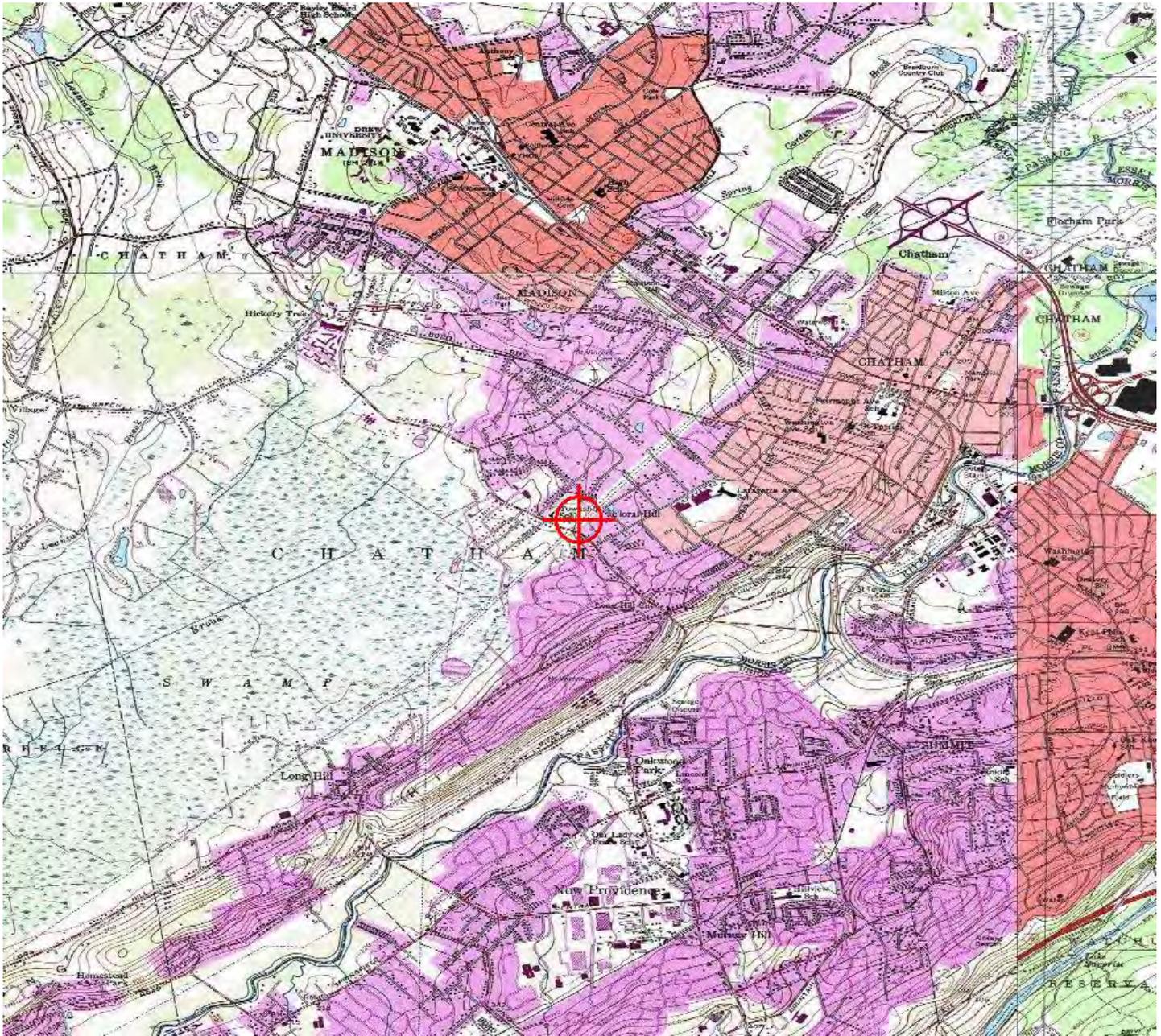
Additional information for ASN 2019-AEA-7400-OE

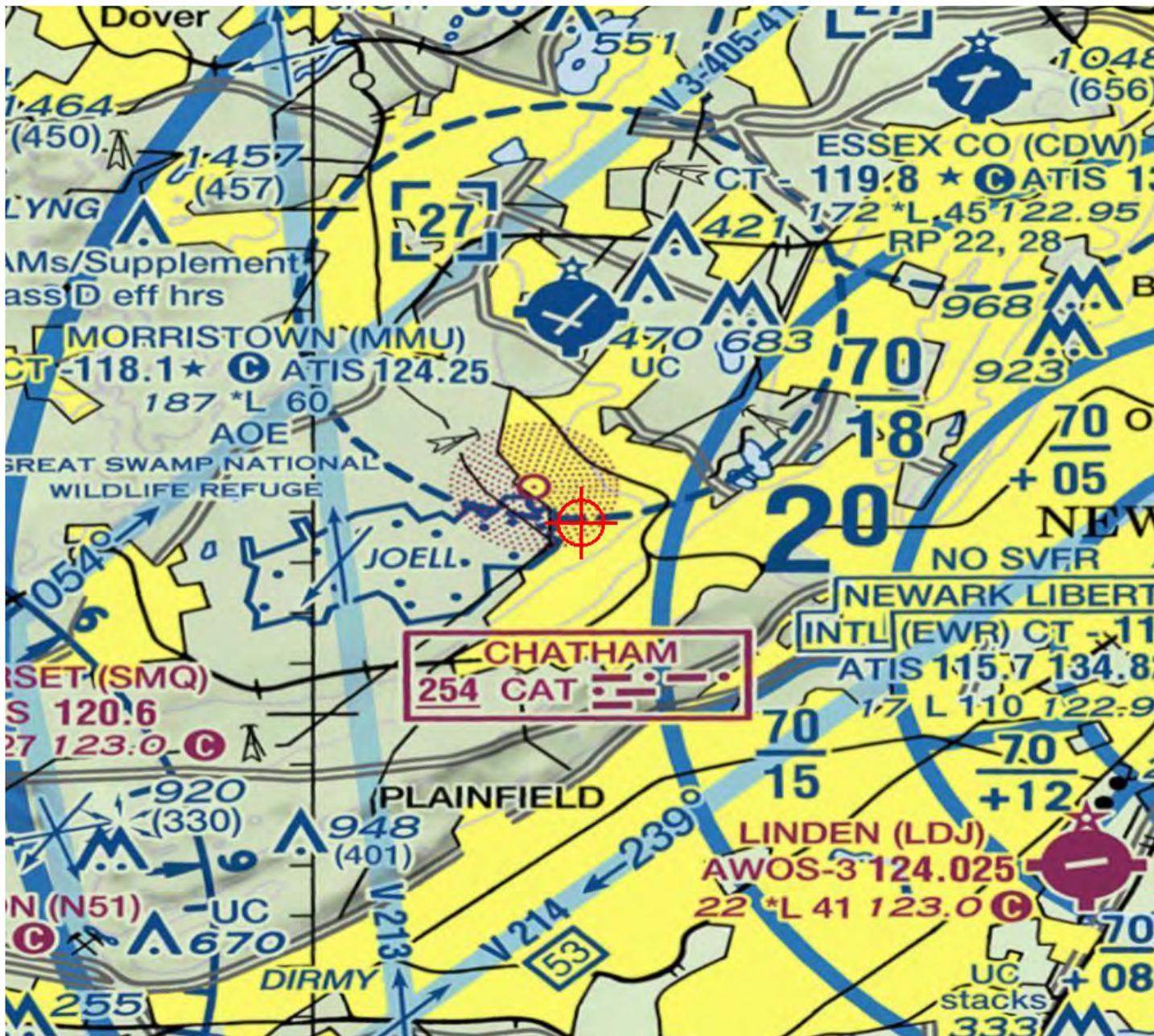
Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7400-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7401-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 8/7-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-43-40.86N NAD 83 |
| Longitude: | 74-24-46.58W |
| Heights: | 274 feet site elevation (SE) 118 feet above ground level (AGL) 392 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7401-OE.

Signature Control No: 409906207-414202506

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

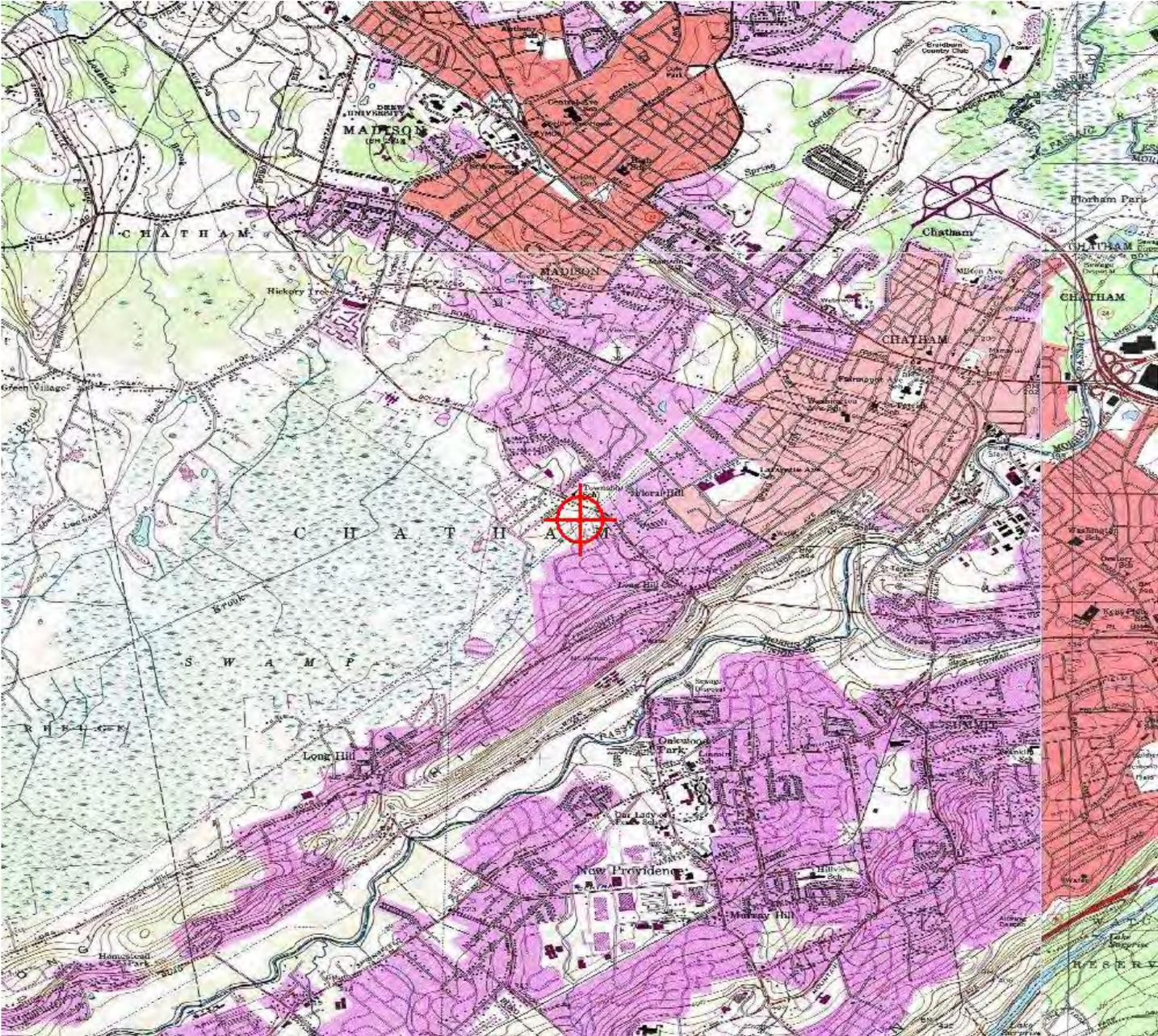
Additional information for ASN 2019-AEA-7401-OE

Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7401-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7402-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 9/1-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-43-36.30N NAD 83 |
| Longitude: | 74-24-51.57W |
| Heights: | 257 feet site elevation (SE) 118 feet above ground level (AGL) 375 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7402-OE.

Signature Control No: 409906208-414202501

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

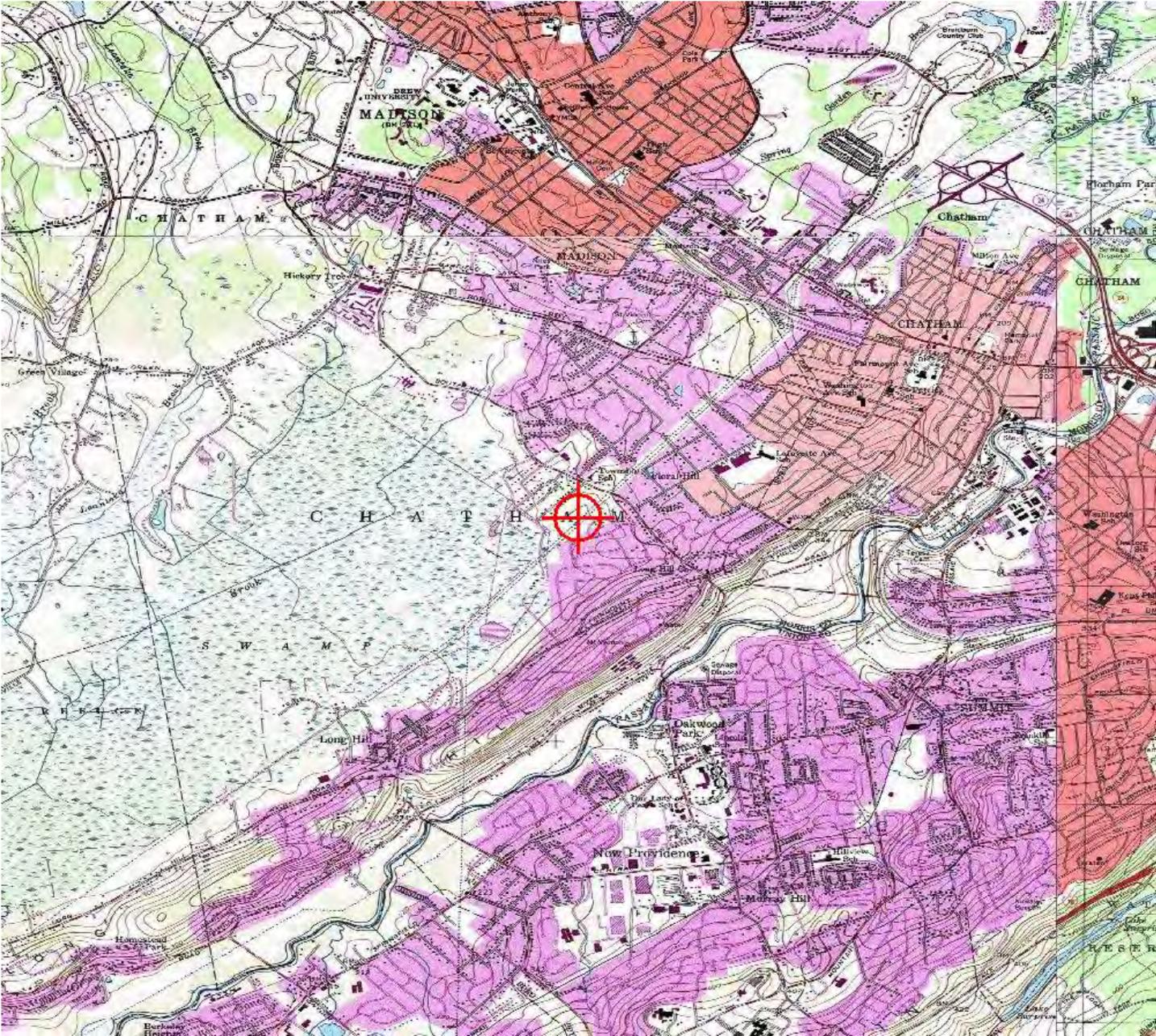
Additional information for ASN 2019-AEA-7402-OE

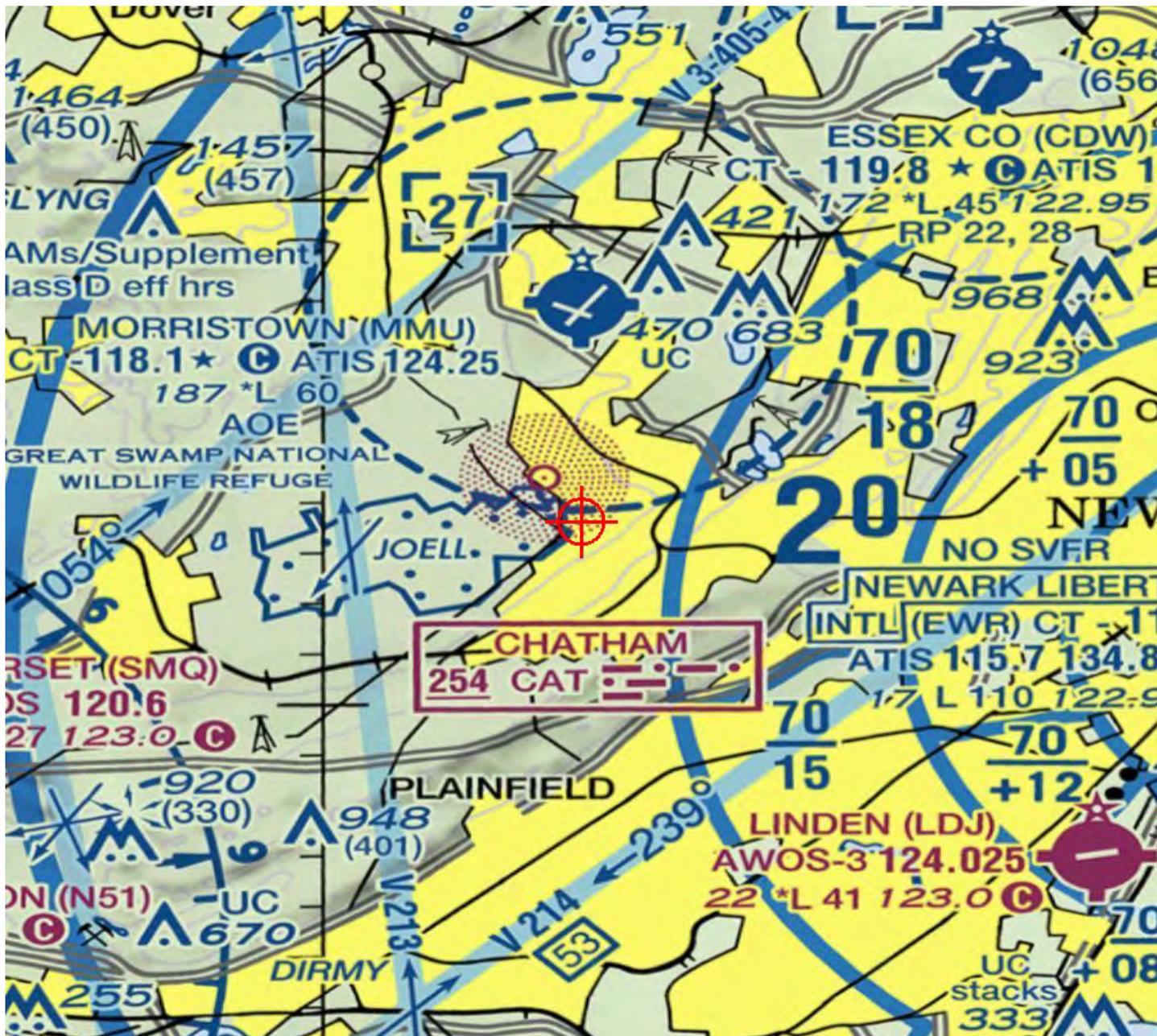
Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7402-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7403-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 9/2-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-43-29.87N NAD 83 |
| Longitude: | 74-24-58.61W |
| Heights: | 247 feet site elevation (SE) 123 feet above ground level (AGL) 370 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7403-OE.

Signature Control No: 409906209-414202499

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

Additional information for ASN 2019-AEA-7403-OE

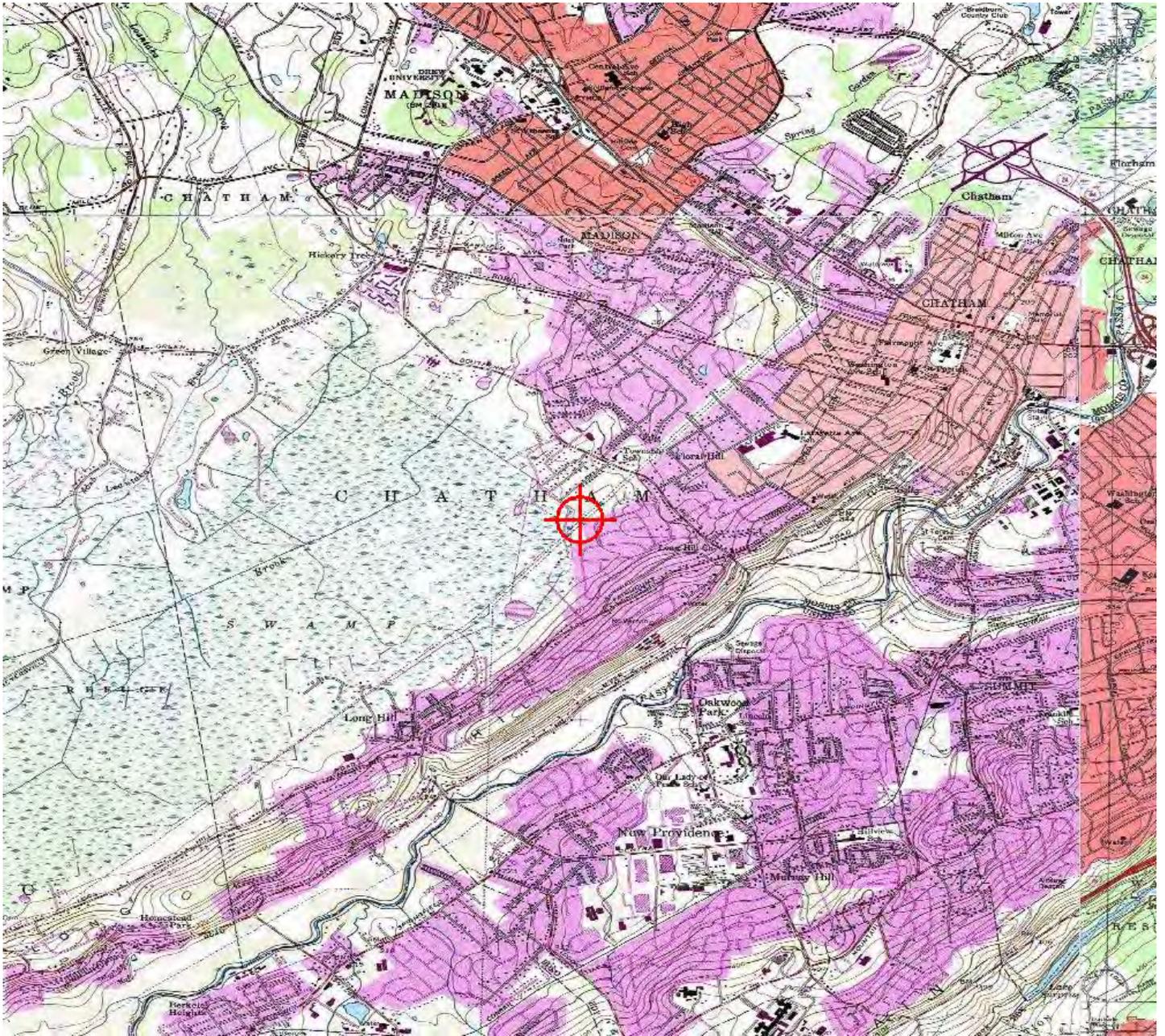
Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7403-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.



TOPO Map for ASN 2019-AEA-7403-OE





Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7404-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 9/3-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-43-23.46N NAD 83 |
| Longitude: | 74-25-05.67W |
| Heights: | 248 feet site elevation (SE) 113 feet above ground level (AGL) 361 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7404-OE.

Signature Control No: 409906210-414202508

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

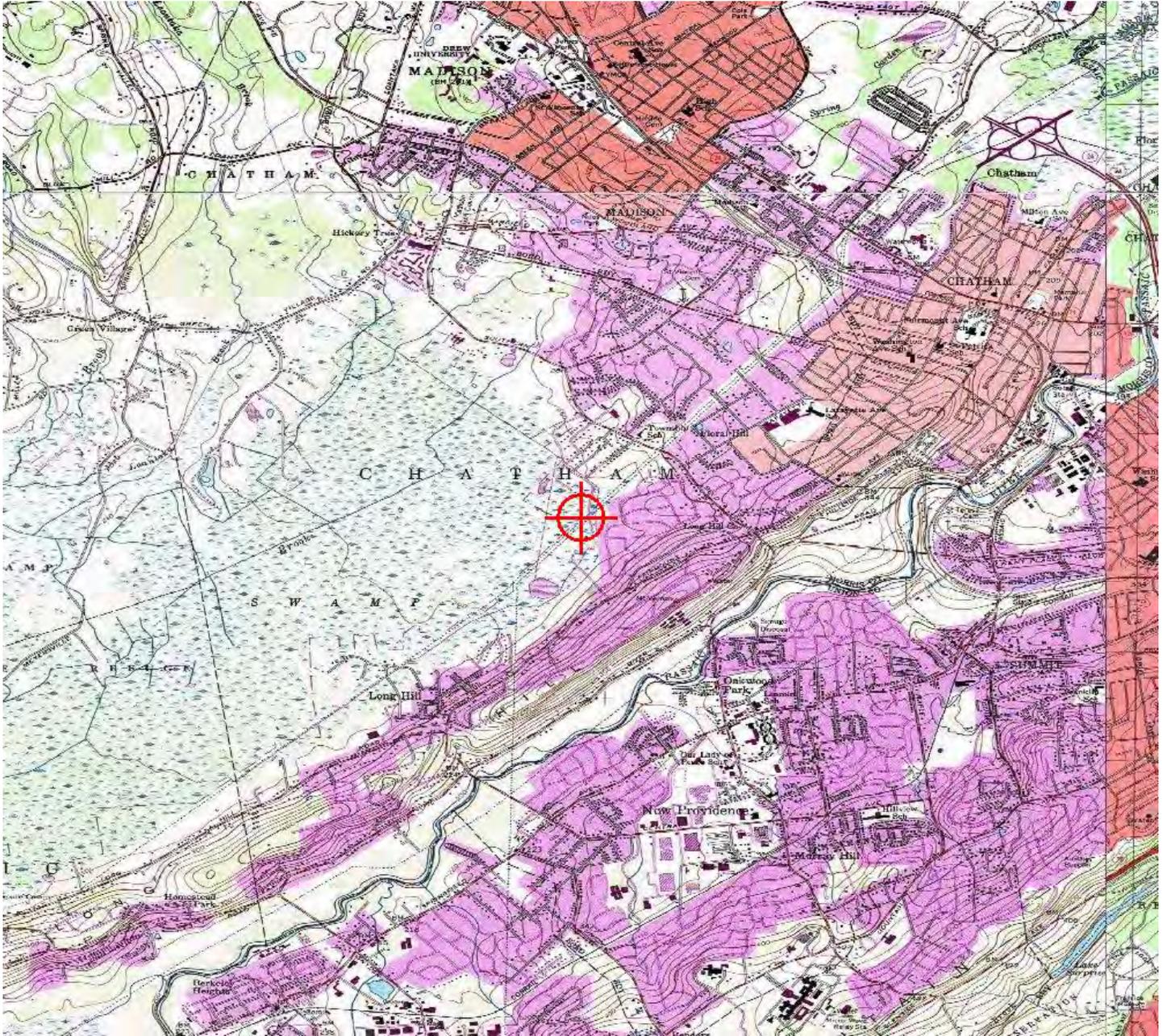
Additional information for ASN 2019-AEA-7404-OE

Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7404-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.







Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7405-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 9/4-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-43-17.44N NAD 83 |
| Longitude: | 74-25-12.26W |
| Heights: | 246 feet site elevation (SE) 123 feet above ground level (AGL) 369 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7405-OE.

Signature Control No: 409906211-414202503

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

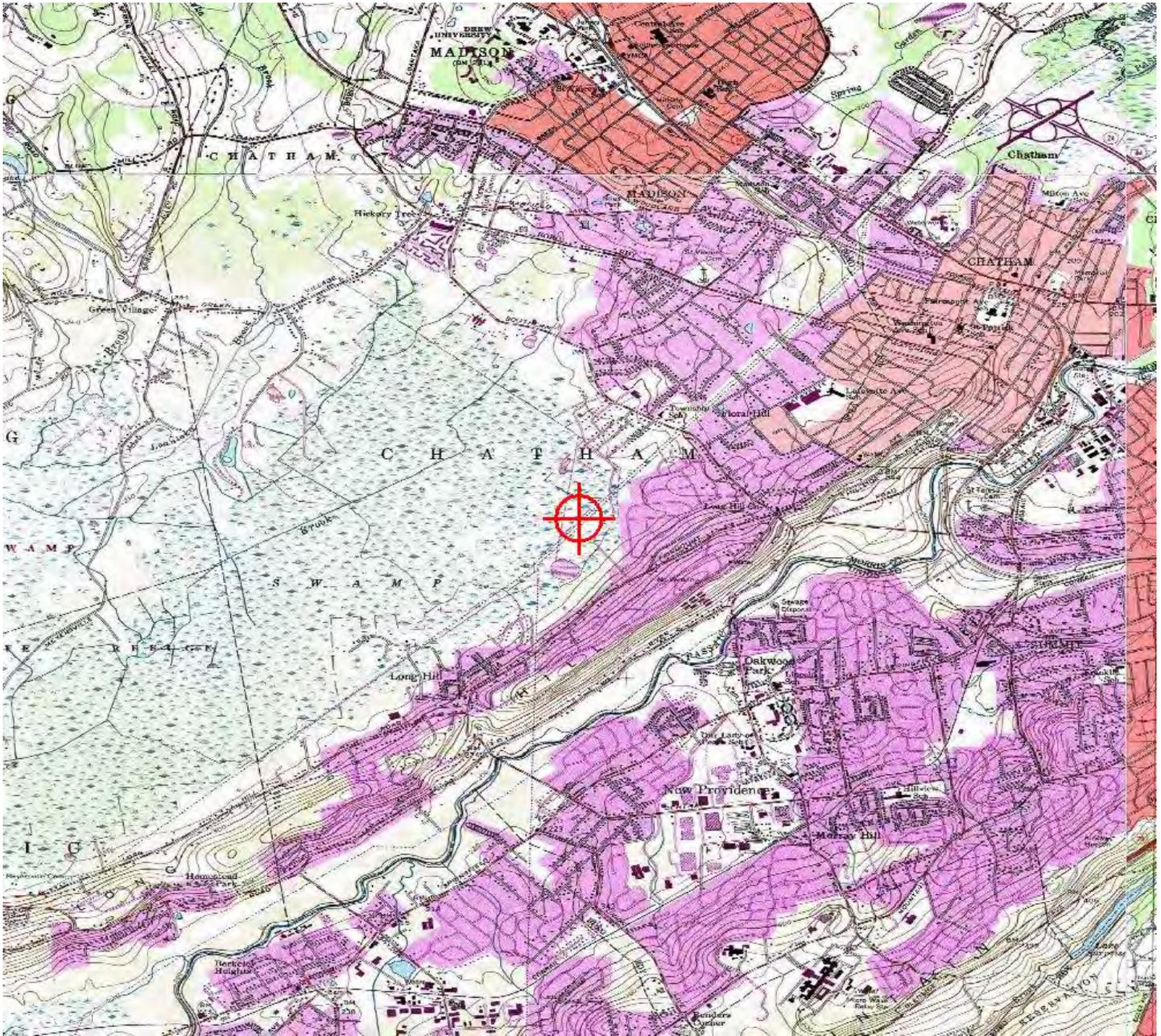
Additional information for ASN 2019-AEA-7405-OE

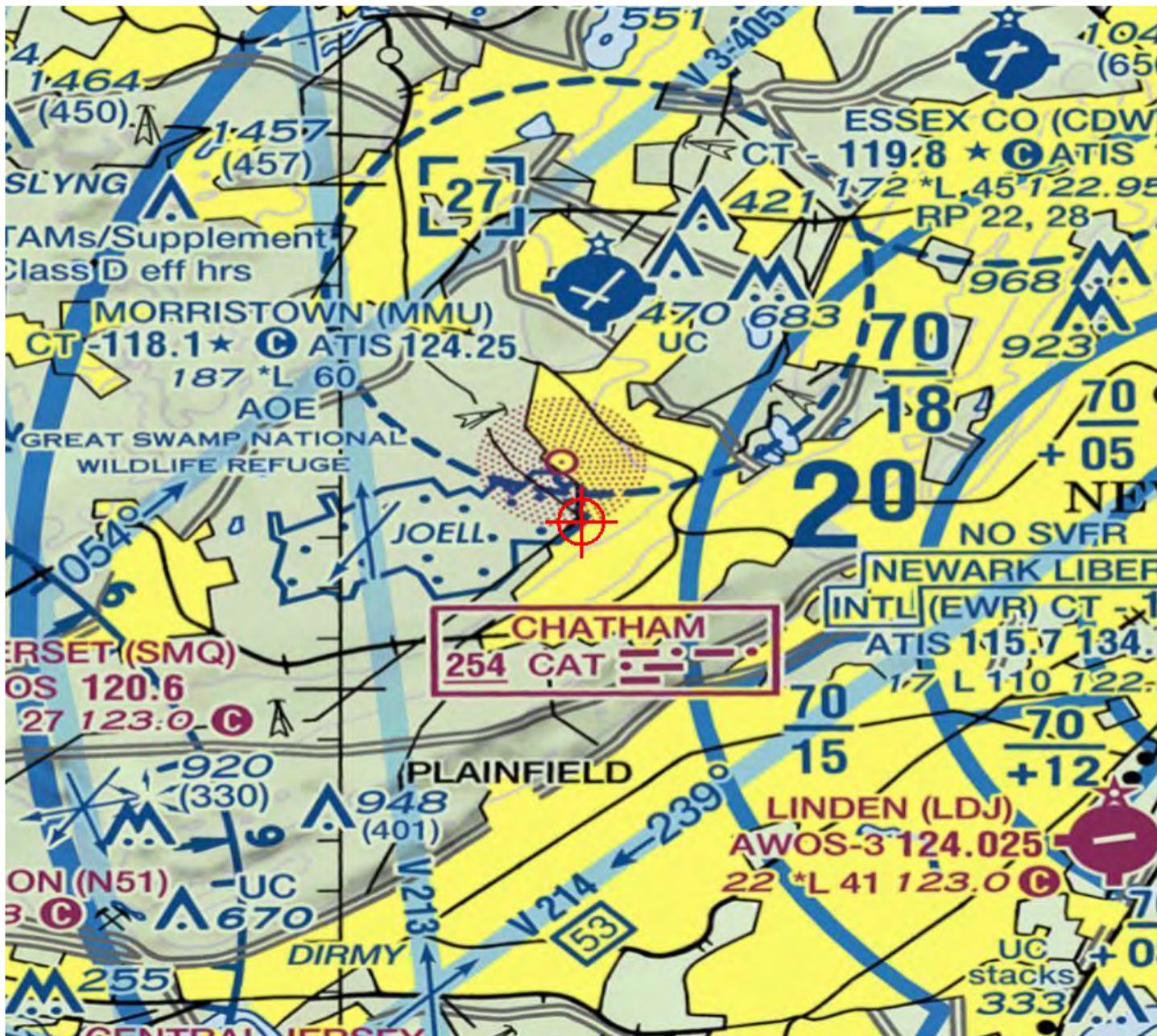
Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7405-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7406-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 9/5-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-43-11.79N NAD 83 |
| Longitude: | 74-25-18.39W |
| Heights: | 244 feet site elevation (SE) 123 feet above ground level (AGL) 367 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7406-OE.

Signature Control No: 409906212-414202497

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

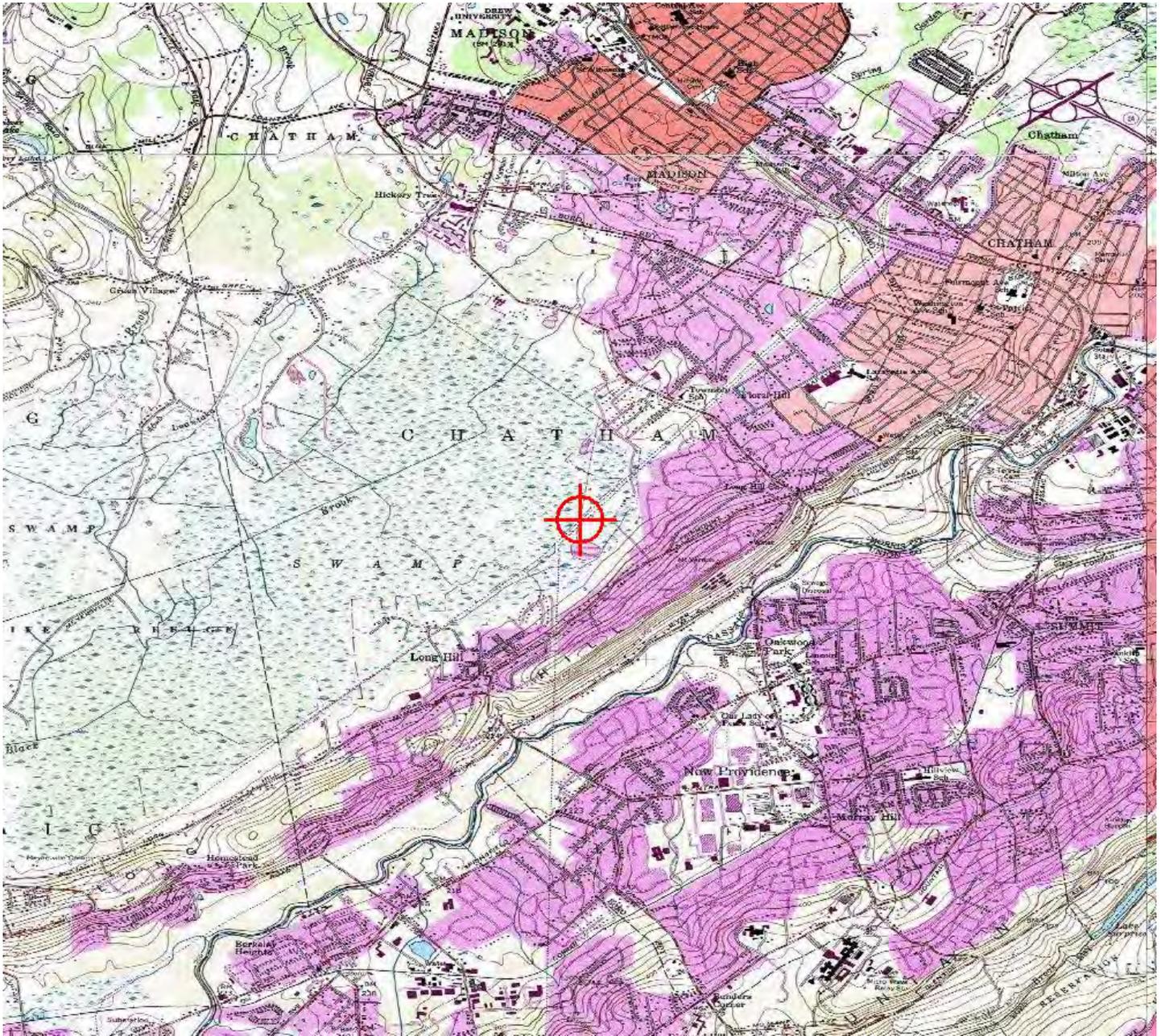
Additional information for ASN 2019-AEA-7406-OE

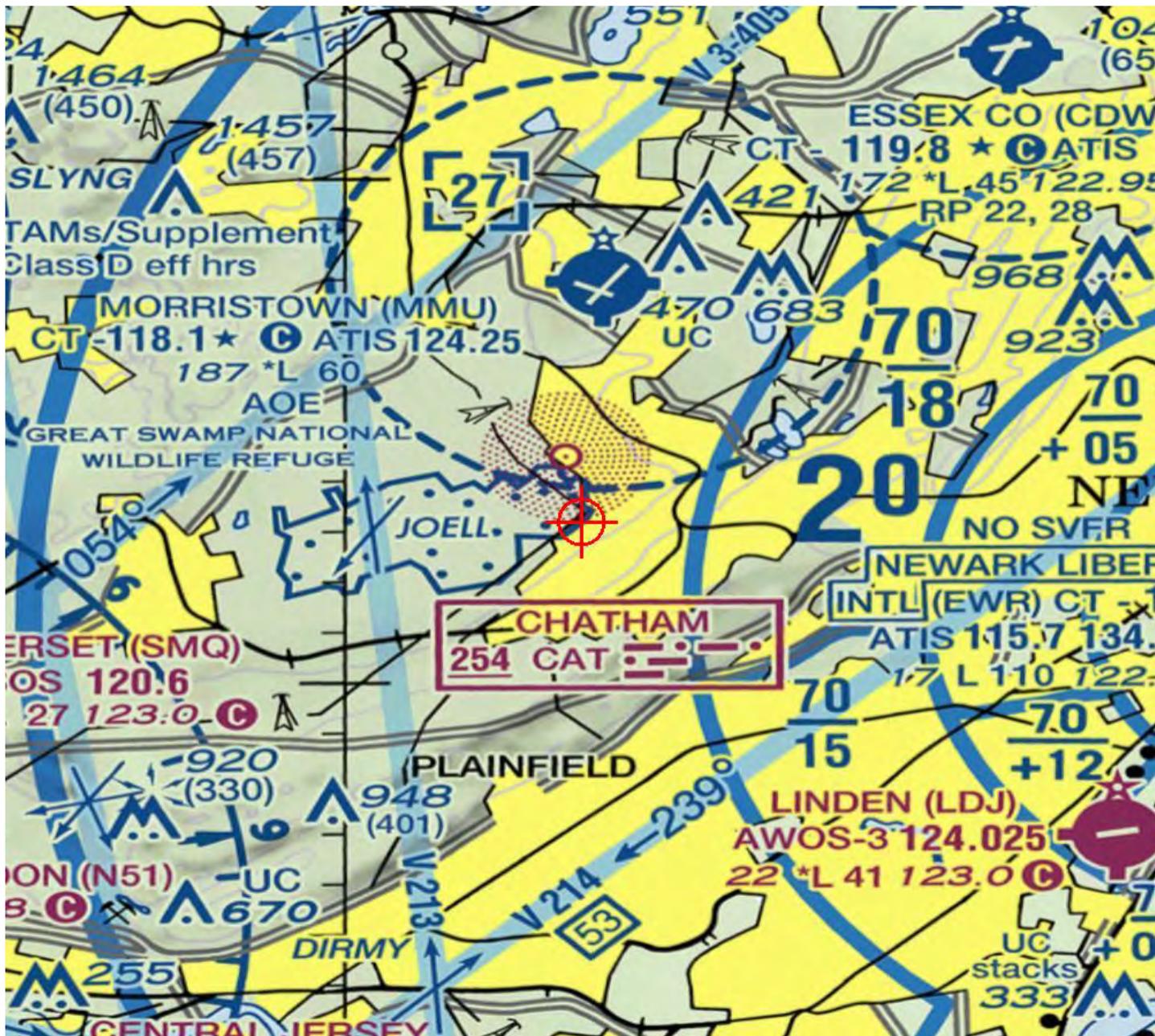
Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7406-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7407-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 9/6-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-43-04.87N NAD 83 |
| Longitude: | 74-25-25.99W |
| Heights: | 238 feet site elevation (SE) 123 feet above ground level (AGL) 361 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7407-OE.

Signature Control No: 409906213-414202504

(DNE)

Darin Clipper
Specialist

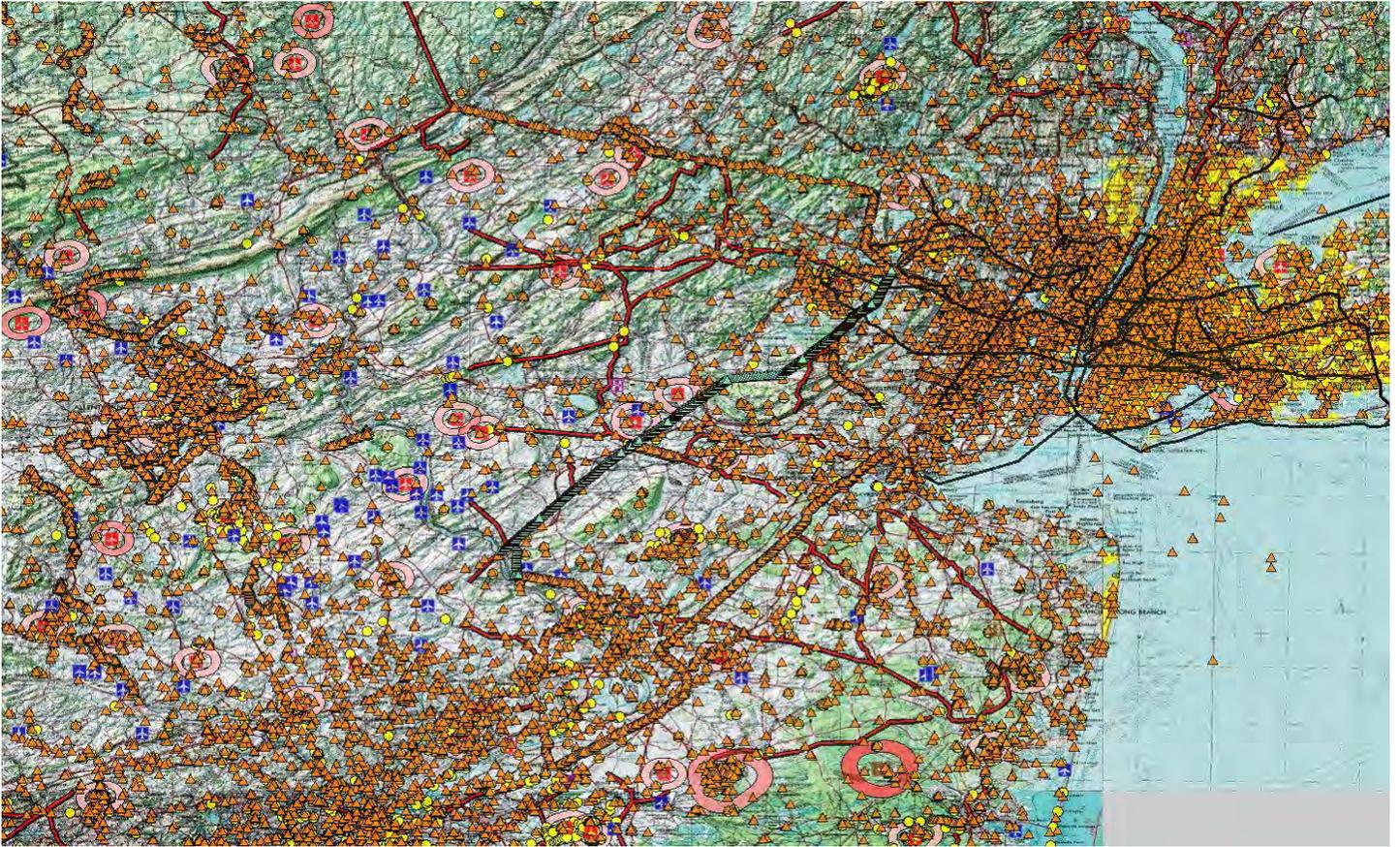
Attachment(s)
Additional Information
Case Description
Map(s)

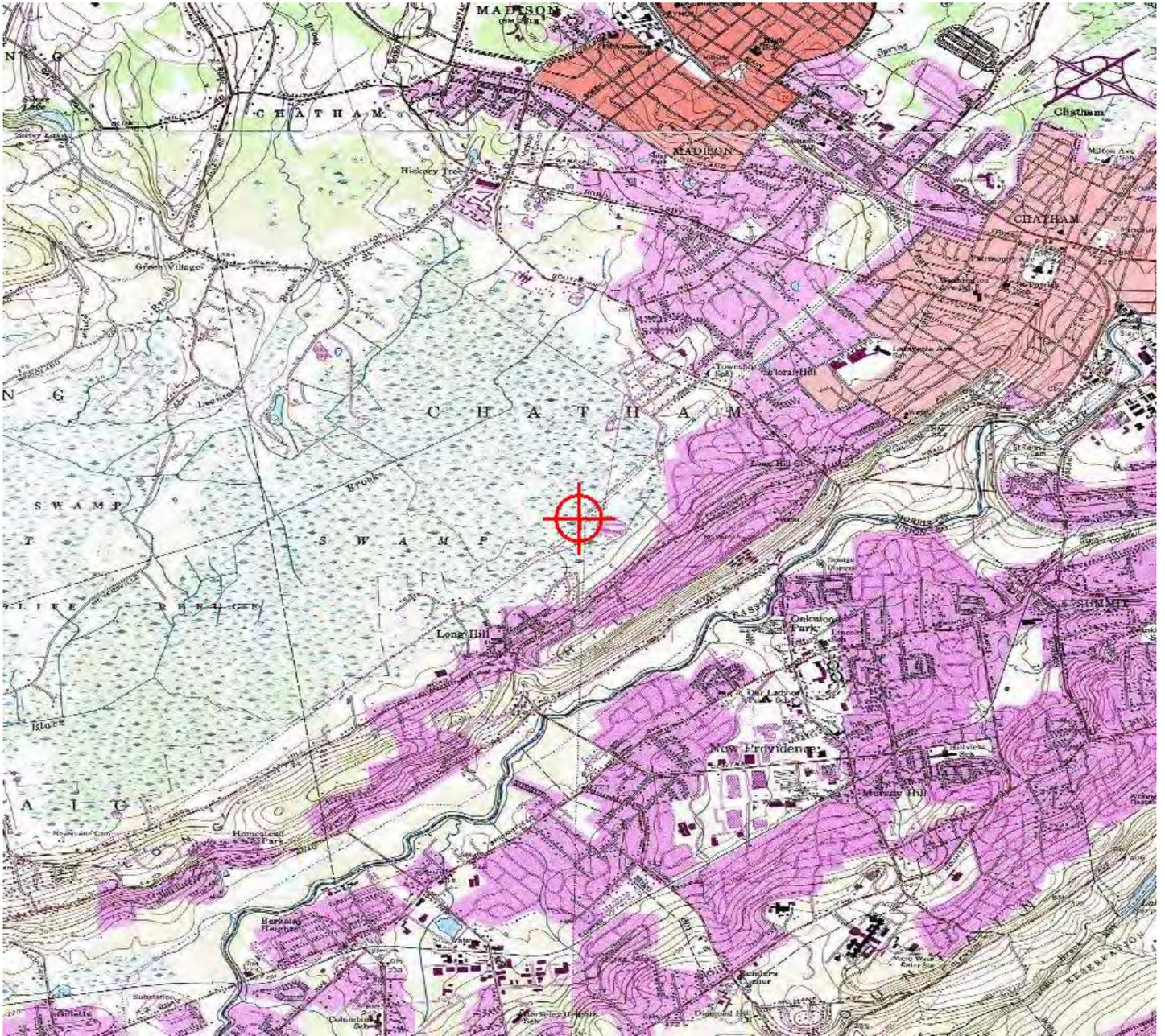
Additional information for ASN 2019-AEA-7407-OE

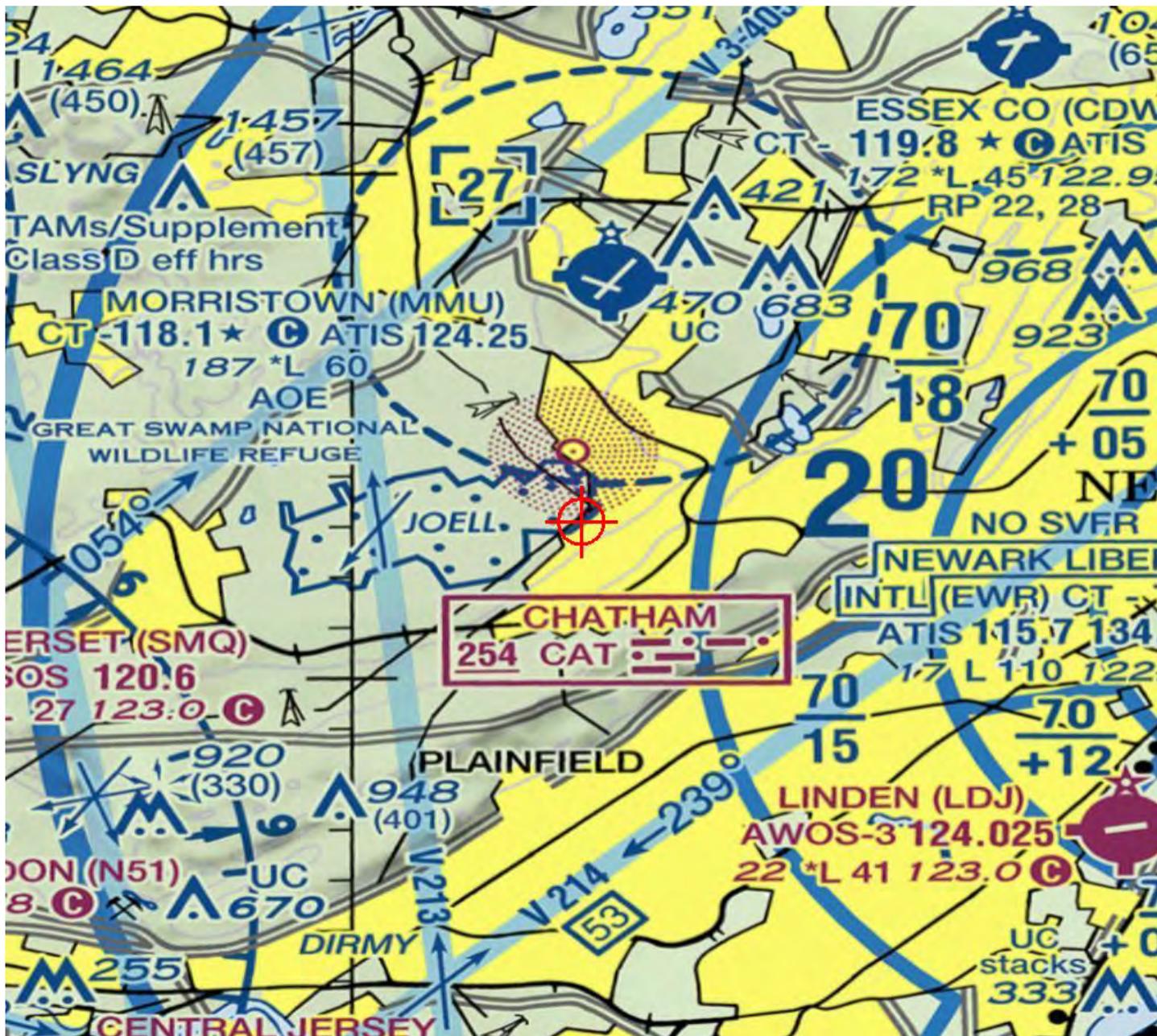
Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7407-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7408-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 9/7-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-42-57.99N NAD 83 |
| Longitude: | 74-25-33.60W |
| Heights: | 238 feet site elevation (SE) 148 feet above ground level (AGL) 386 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7408-OE.

Signature Control No: 409906214-414202500

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

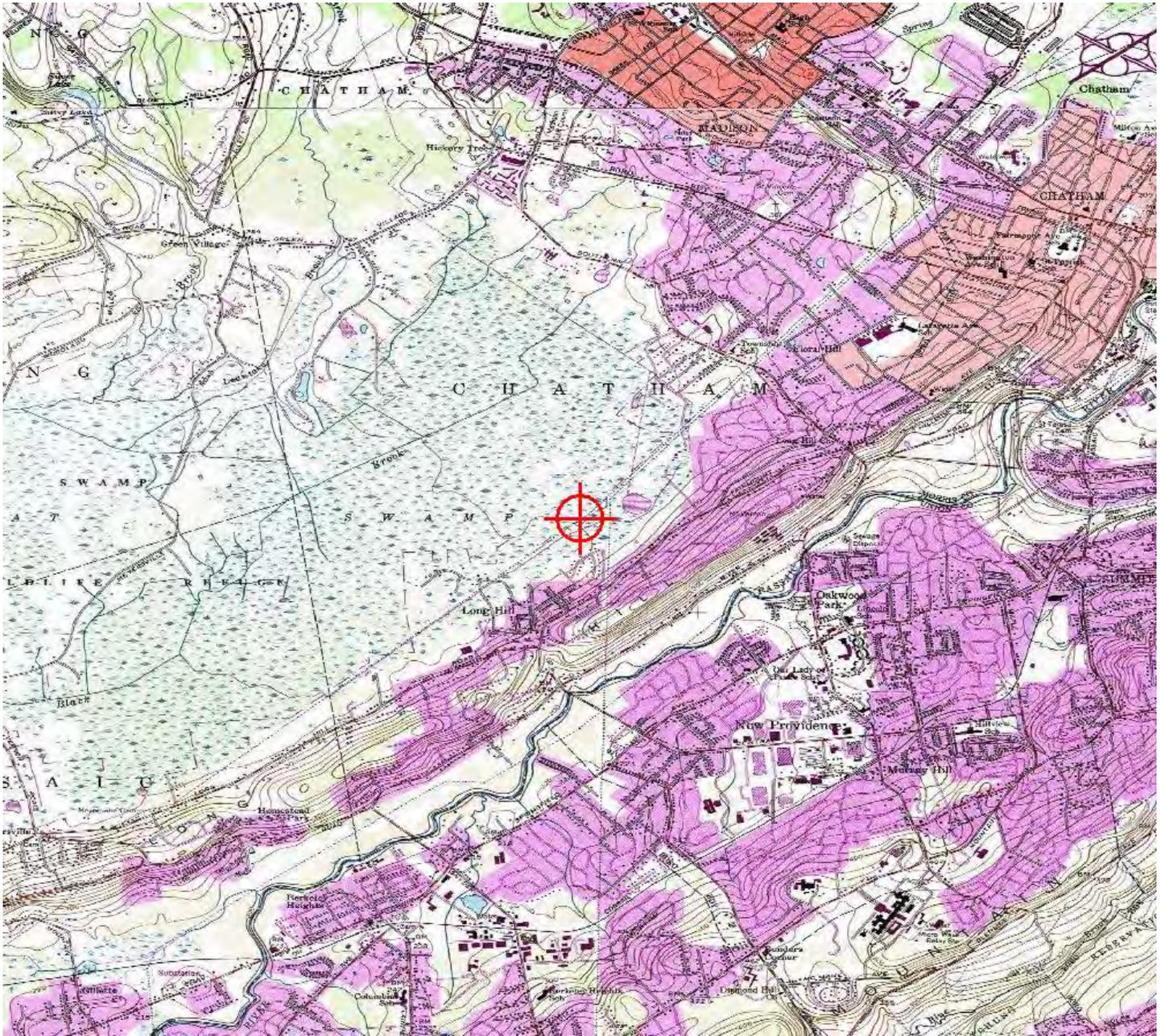
Additional information for ASN 2019-AEA-7408-OE

Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7408-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7409-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 10/1-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-42-49.59N NAD 83 |
| Longitude: | 74-25-45.38W |
| Heights: | 238 feet site elevation (SE) 153 feet above ground level (AGL) 391 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7409-OE.

Signature Control No: 409906215-414202507

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

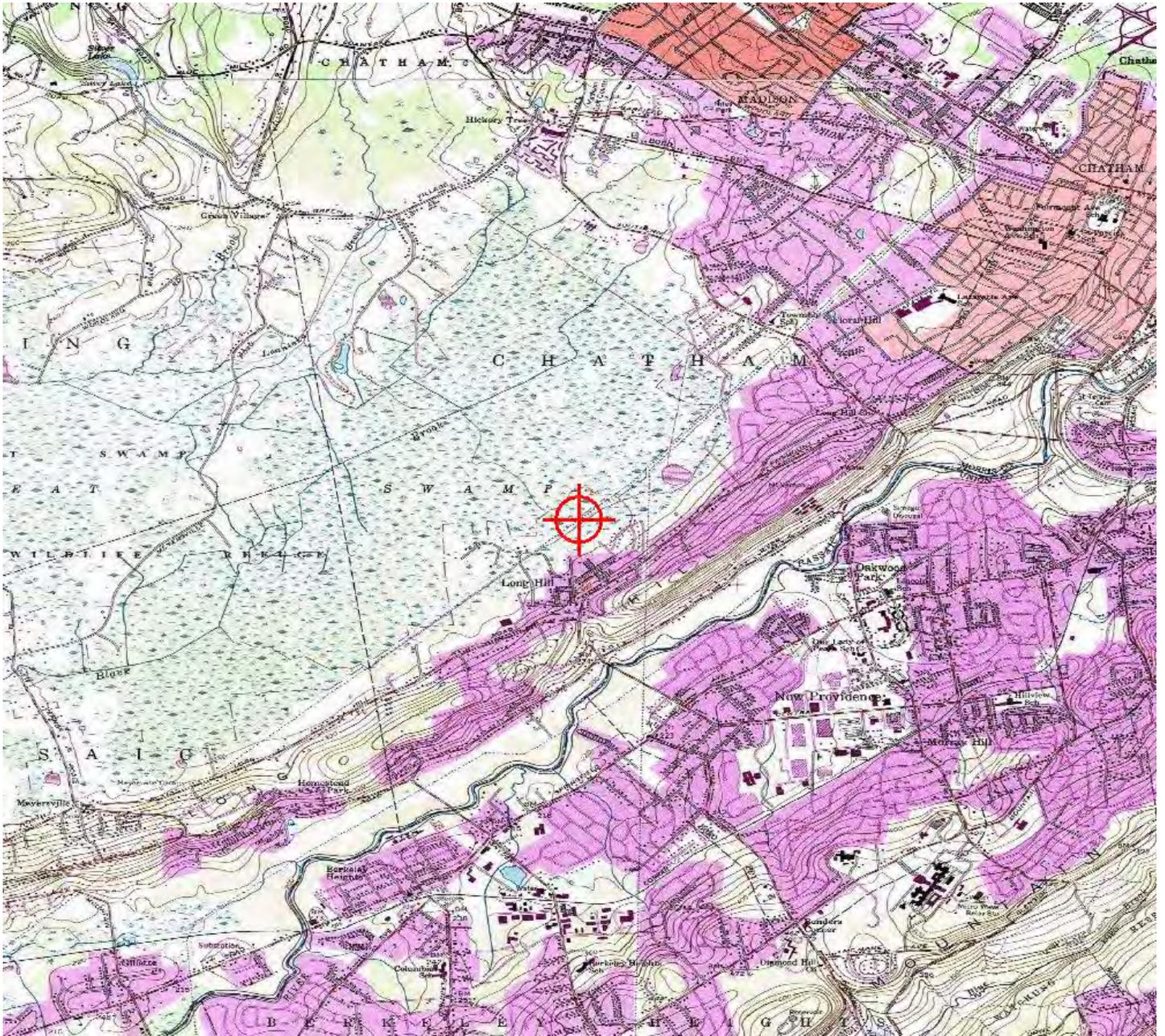
Additional information for ASN 2019-AEA-7409-OE

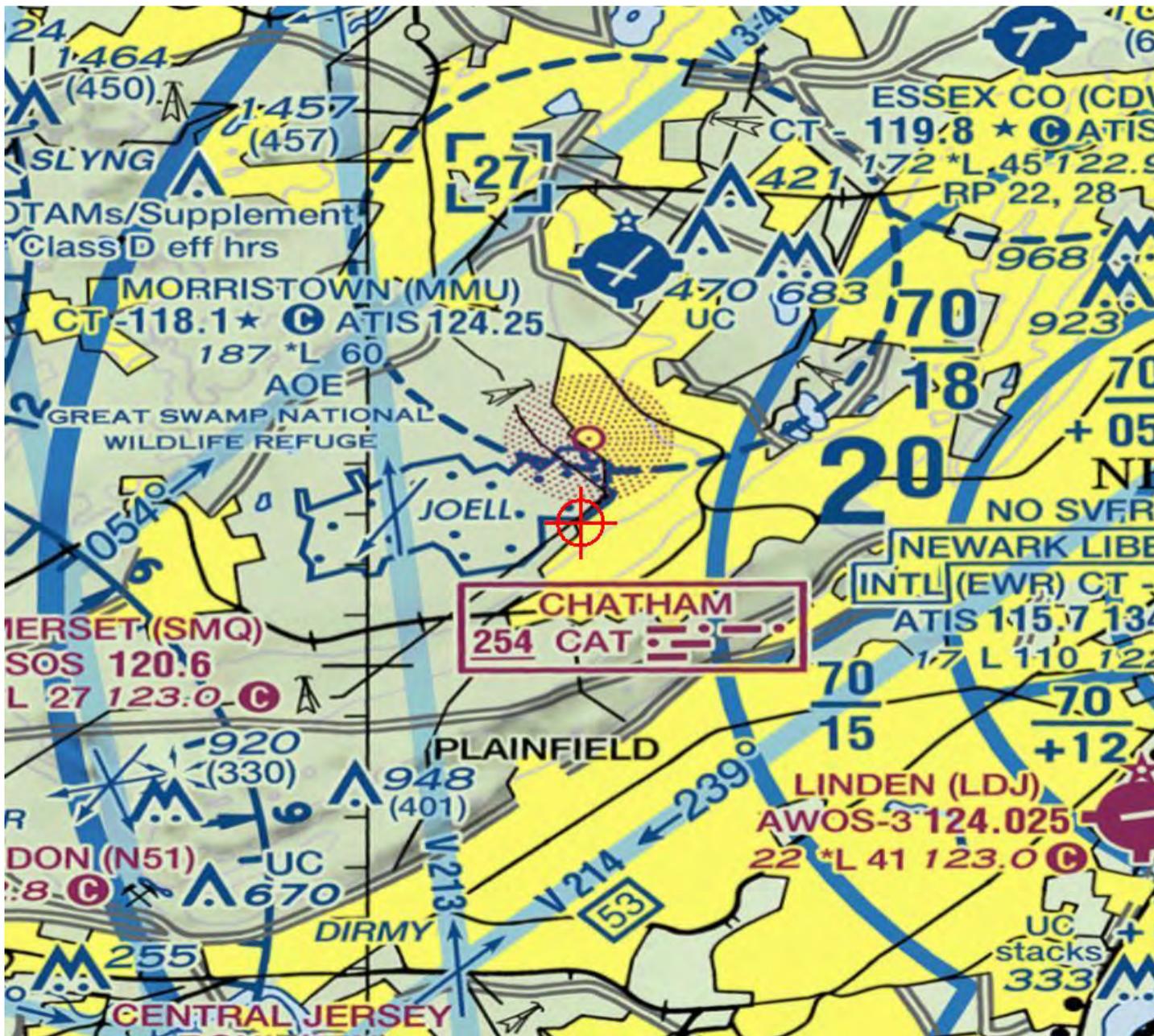
Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7409-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7410-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 10/2-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-42-42.18N NAD 83 |
| Longitude: | 74-25-55.75W |
| Heights: | 237 feet site elevation (SE) 143 feet above ground level (AGL) 380 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7410-OE.

Signature Control No: 409906216-414202509

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

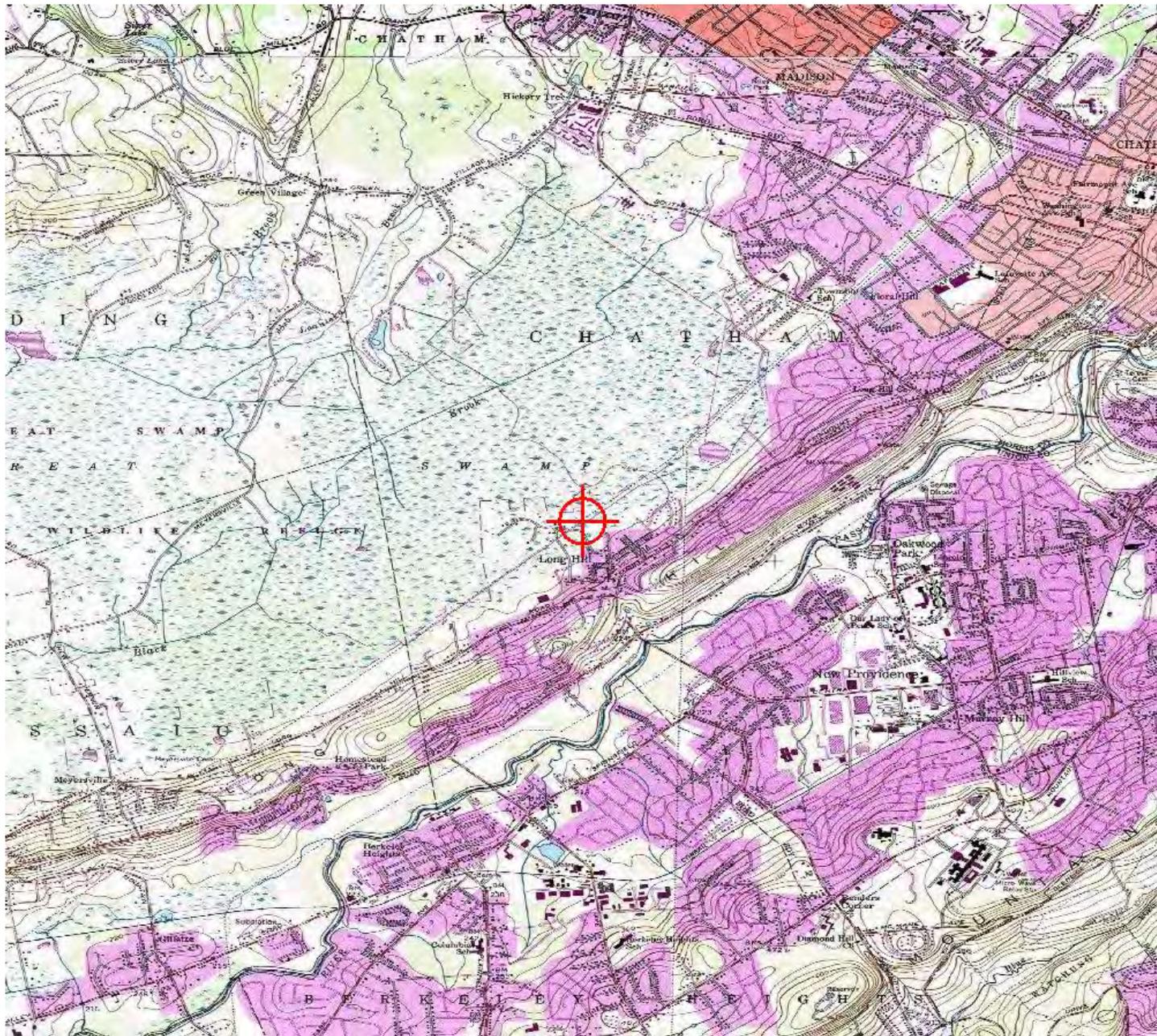
Additional information for ASN 2019-AEA-7410-OE

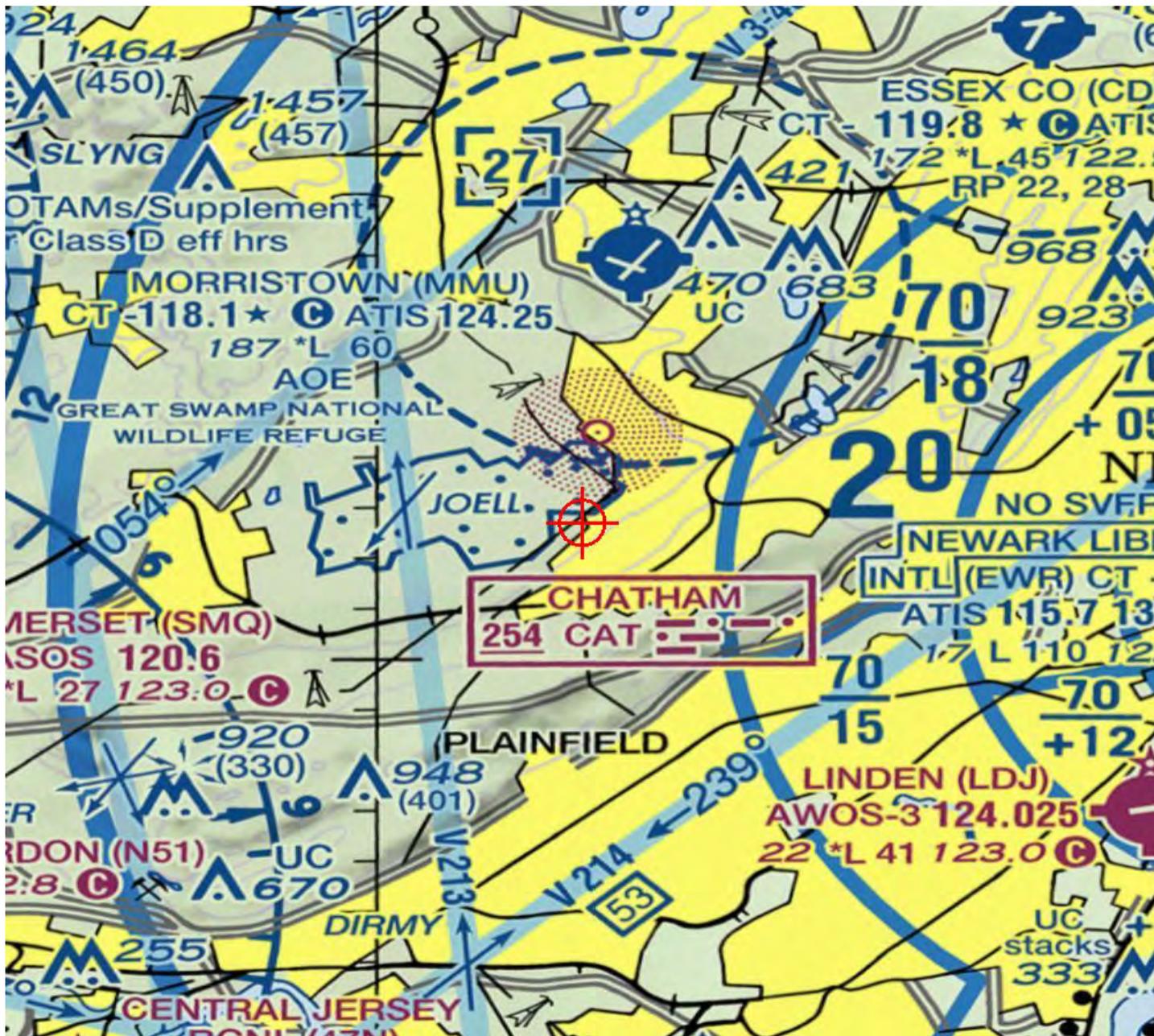
Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7410-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.









Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7411-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 10/3-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-42-34.02N NAD 83 |
| Longitude: | 74-26-07.27W |
| Heights: | 240 feet site elevation (SE) 143 feet above ground level (AGL) 383 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7411-OE.

Signature Control No: 409906217-414202498

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

Additional information for ASN 2019-AEA-7411-OE

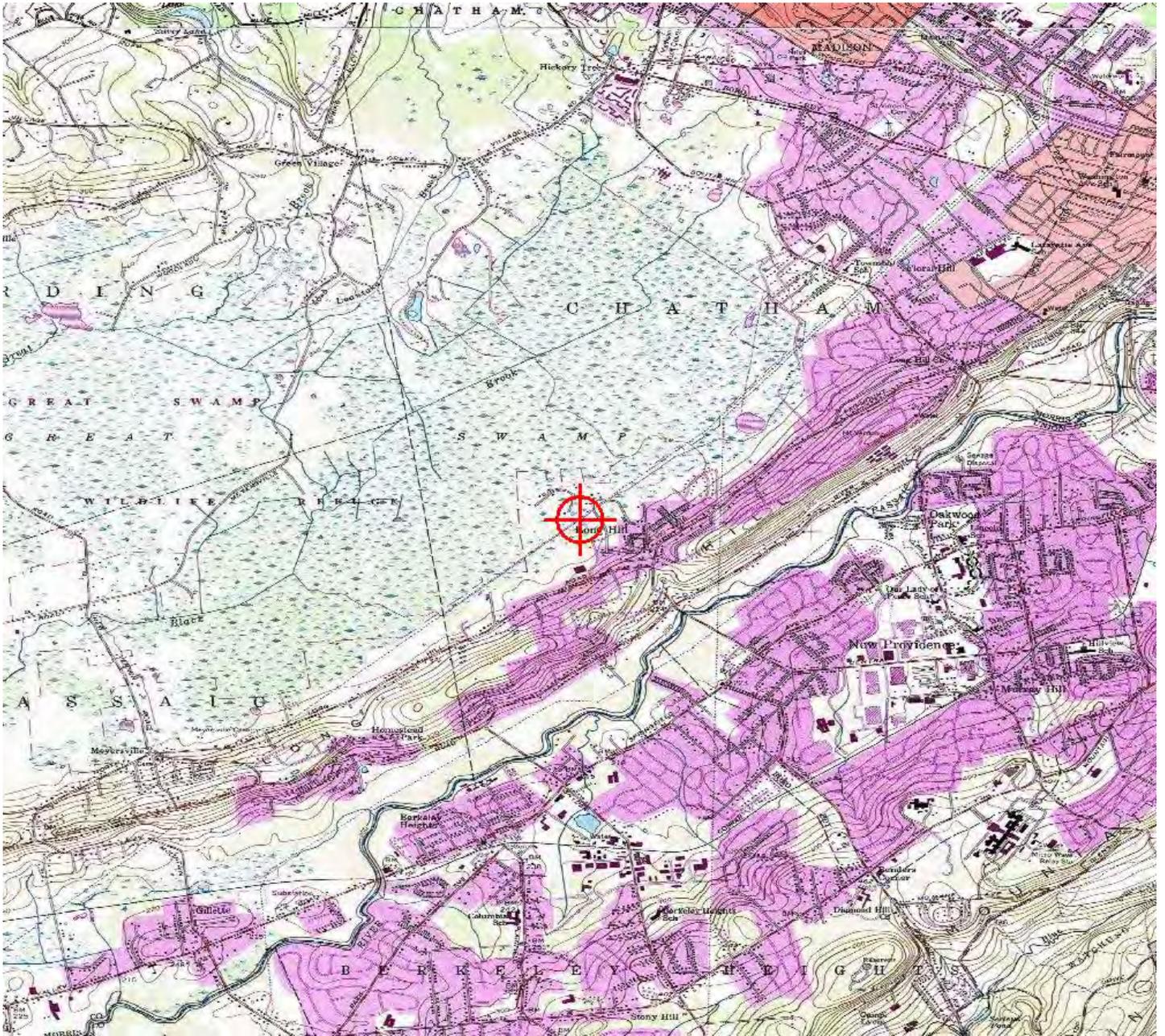
Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

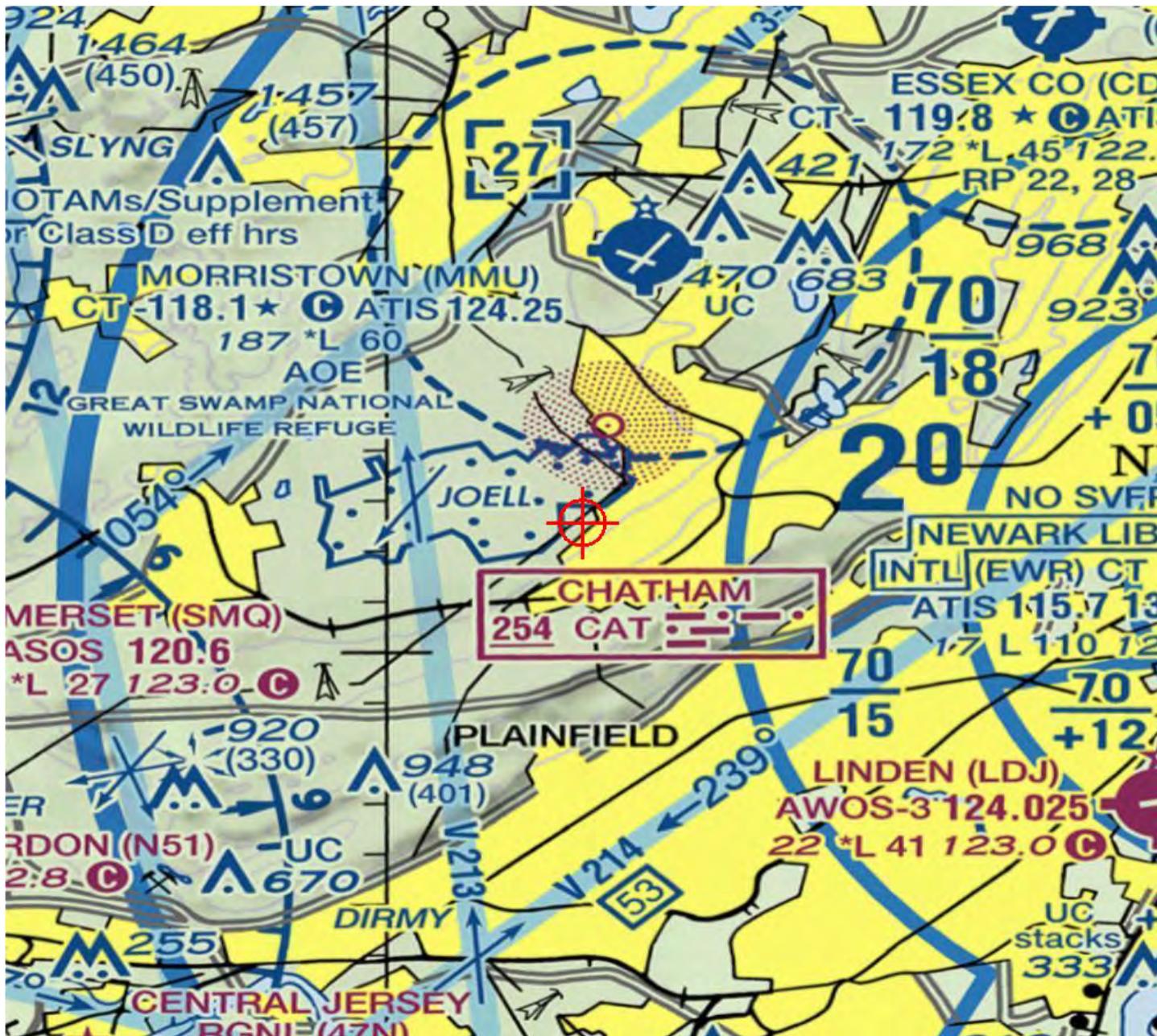
Case Description for ASN 2019-AEA-7411-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.



TOPO Map for ASN 2019-AEA-7411-OE







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Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7412-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 10/4-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-42-25.65N NAD 83 |
| Longitude: | 74-26-19.14W |
| Heights: | 236 feet site elevation (SE) 148 feet above ground level (AGL) 384 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, effective 21 Nov 2007, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

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This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7412-OE.

Signature Control No: 409906218-414202555

(DNE)

Darin Clipper
Specialist

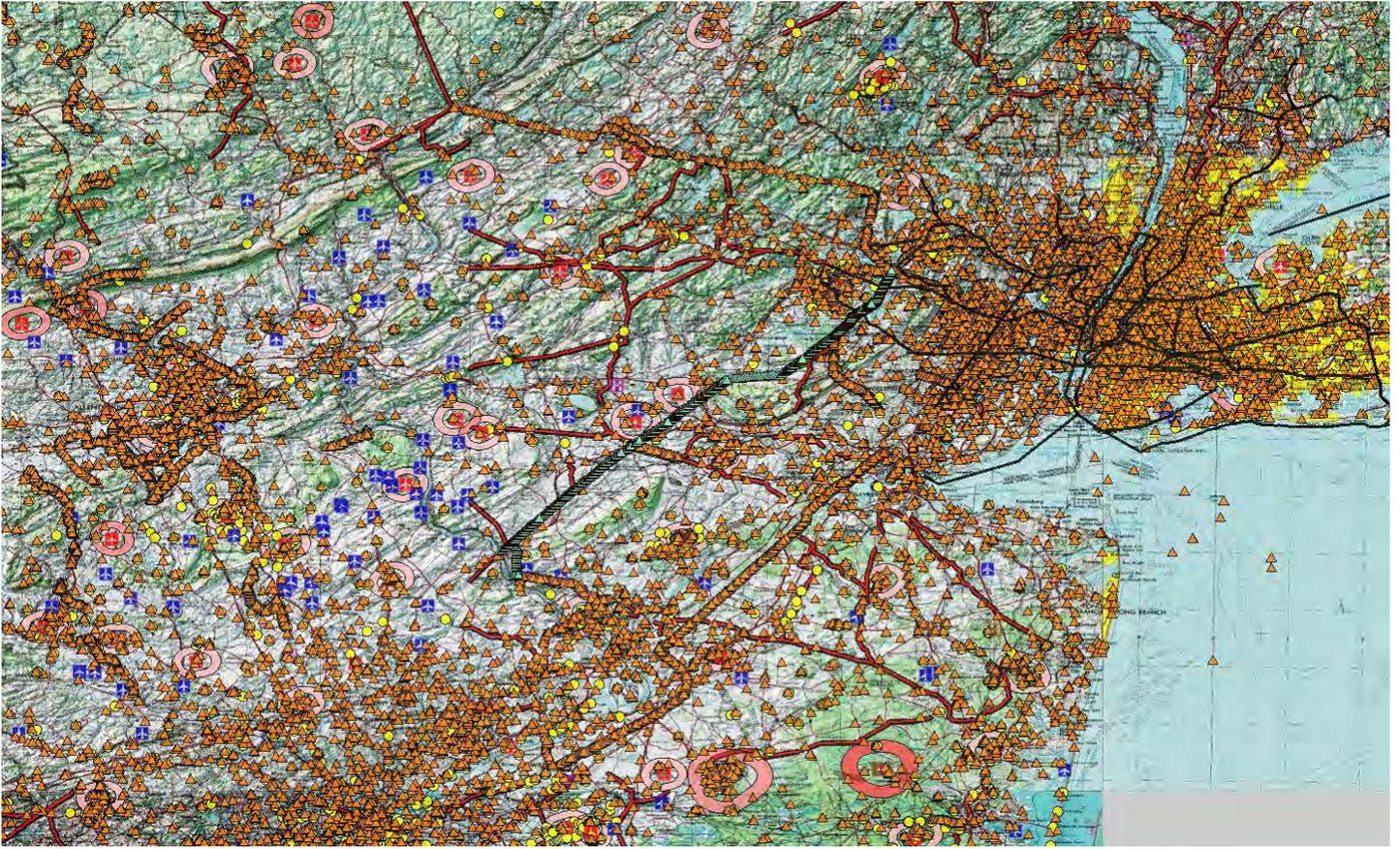
Attachment(s)
Additional Information
Case Description
Map(s)

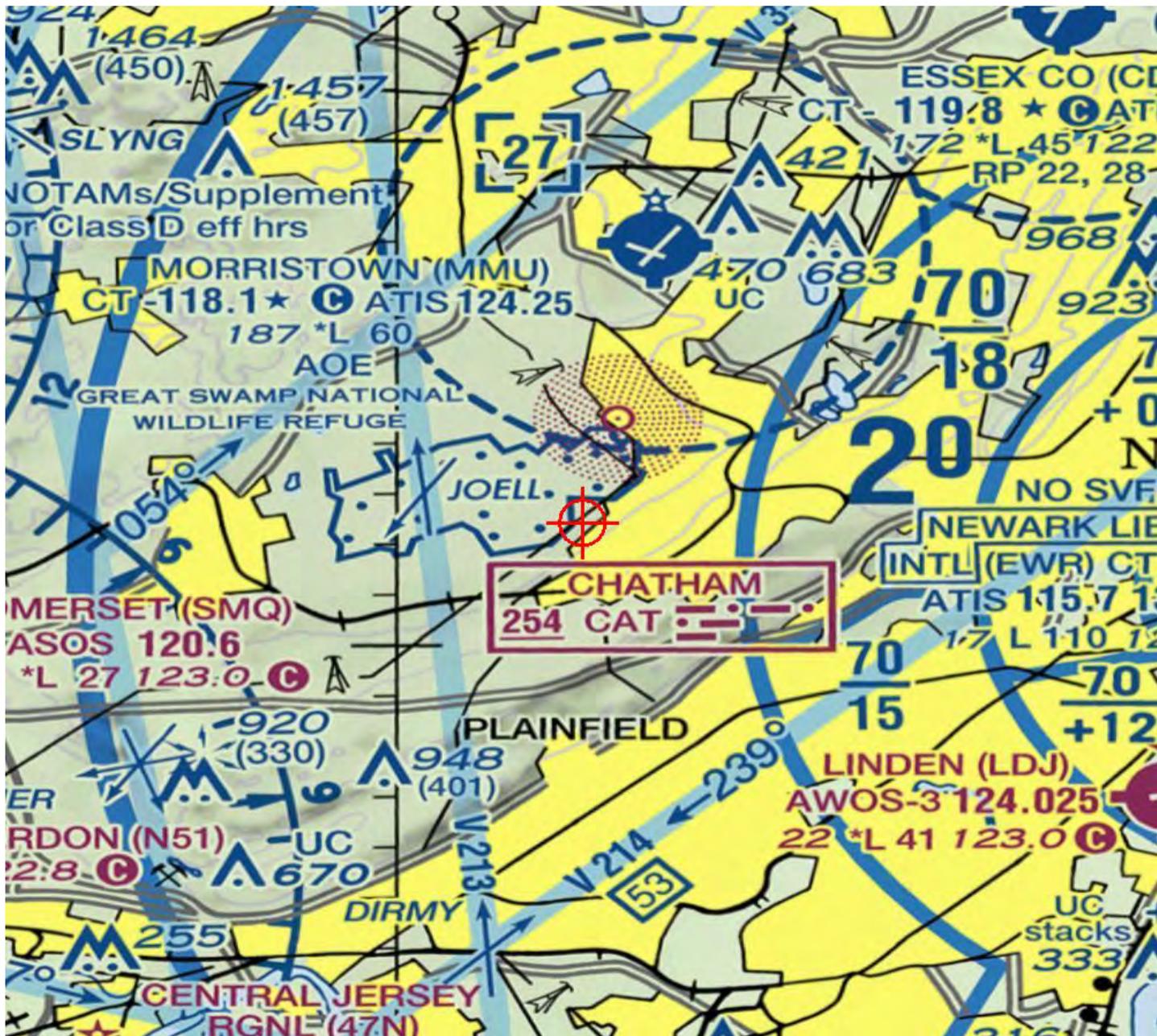
Additional information for ASN 2019-AEA-7412-OE

Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7412-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.







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Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7413-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 11/1-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-42-17.93N NAD 83 |
| Longitude: | 74-26-30.07W |
| Heights: | 235 feet site elevation (SE) 133 feet above ground level (AGL) 368 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

See attachment for additional condition(s) or information.

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

This determination expires on 02/13/2021 unless:

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- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO

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If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7413-OE.

Signature Control No: 409906219-414202556

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

Additional information for ASN 2019-AEA-7413-OE

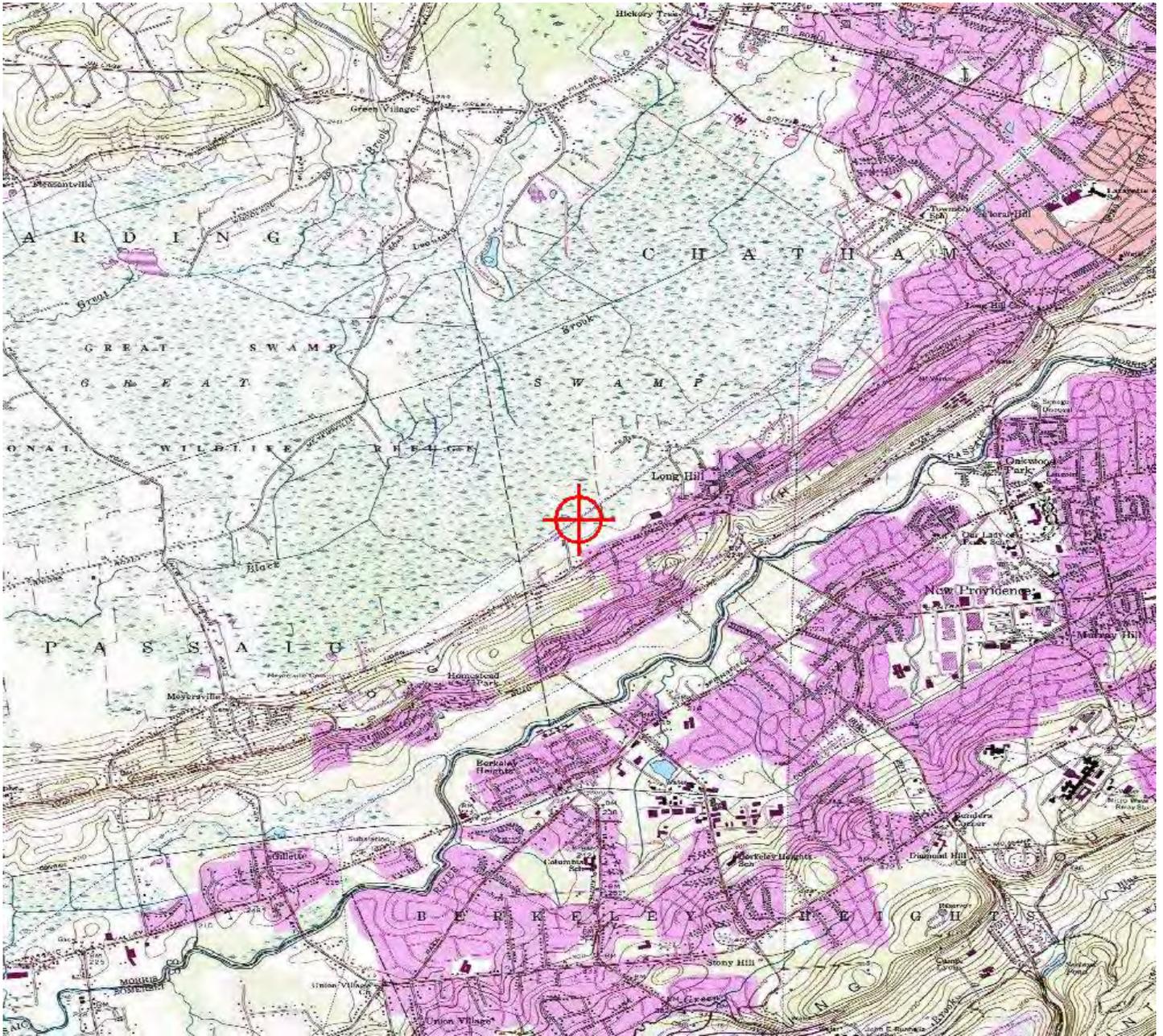
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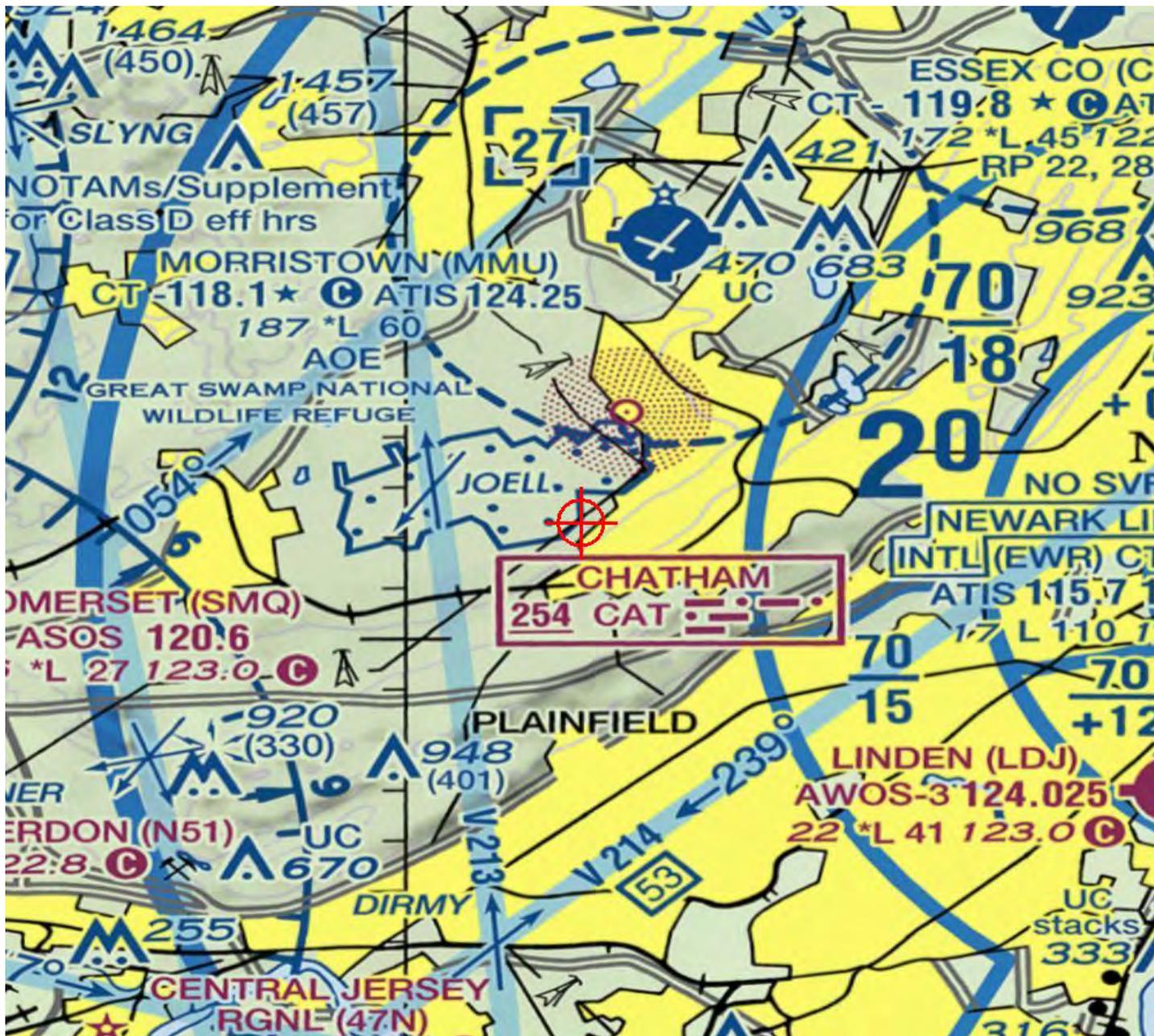
Case Description for ASN 2019-AEA-7413-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.



TOPO Map for ASN 2019-AEA-7413-OE







Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2019-AEA-7414-OE

Issued Date: 08/13/2019

Brandi Bartolomeo
PSE&G
4000 Hadley Road
South Plainfield, NJ 07080

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

| | |
|------------|---|
| Structure: | Transmission Line R-LA 11/2-1 |
| Location: | Roseland Borough, NJ |
| Latitude: | 40-42-10.71N NAD 83 |
| Longitude: | 74-26-40.30W |
| Heights: | 236 feet site elevation (SE) 133 feet above ground level (AGL) 369 feet above mean sea level (AMSL) |

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

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Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 L Change 2.

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If we can be of further assistance, please contact our office at (404) 305-6531, or darin.clipper@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2019-AEA-7414-OE.

Signature Control No: 409906220-414202558

(DNE)

Darin Clipper
Specialist

Attachment(s)
Additional Information
Case Description
Map(s)

Additional information for ASN 2019-AEA-7414-OE

Notice to the FAA is required when a transmission line is more than 200 feet above the surface. It is recommended that any portion of a transmission line from one towered structure to another which may exceed an overall height of 200 ft. above the surface or exceeds an obstruction standard contained in Title 14 CFR Part 77 should be marked with 36" lighted spherical markers in accordance with FAA Advisory Circular 70/7460-1L, Change 2, Obstruction Marking and Lighting. Some examples of surfaces to be reviewed are canyon floors, rivers, lakes, ravines, etc.

Case Description for ASN 2019-AEA-7414-OE

Segment 1- Transmission structures span from the Roseland Switching Station on Block 33, Lot 4 in Roseland Borough (closest airports are MMU and CDW (Runway 4) at Structure R-LA 1/1-1). ROW runs in a southwesterly direction towards the Long Hill/Warren Township municipal boundary (Structure R-LA 16/1-1). The ROW continues in a westerly direction, parallel with the NJ Route 78 until reaching Structure R-LA 20/2-1 in Bernards Township before proceeding in a southwesterly direction again. The ROW continues in a southwesterly direction through the Readington Substation (Structure R-LA 25/6-1) to the Branchburg Switching Station (Structure R-LA 30/5A-1), nearest airport is N51 Runway 04/22. The Route splits at Structures R-LA 45/4-1A and R-LA 45/4-1B where a segment continues in a southwesterly direction and ends in Lambertville City (Structure R-LA 47/1-1). The other segment continues in a southerly direction to Structures WA-PV 4/6-1A and WA-PV 4/6-1B then runs in a westerly direction and ends adjacent to the Pleasant Valley Switching Station location on Block 57, Lot 3 (Structure WA-PV 5/1-1), nearest airport TTN, Runway 16.



TOPO Map for ASN 2019-AEA-7414-OE

